

**VILLAGE OF DOWNERS GROVE
REPORT FOR THE VILLAGE COUNCIL MEETING
FEBRUARY 11, 2014 AGENDA**

| SUBJECT: | TYPE: | SUBMITTED BY: |
|---|--|--|
| Special Use for a drive-through restaurant at 2535 Ogden Avenue | ✓ Resolution Ordinance Motion Discussion Only | Tom Dabareiner, AICP Community Development Director |

SYNOPSIS

A special use ordinance has been prepared for a drive-through restaurant at 2535 Ogden Avenue.

STRATEGIC PLAN ALIGNMENT

The goals for 2011-2018 include *Strong and Diverse Local Economy*.

FISCAL IMPACT

N/A

RECOMMENDATION

Approval on the March 4, 2014 active agenda. The Plan Commission found that the proposal is an appropriate use in the corridor, compatible with the Comprehensive Plan and meets all standards for approval of a Special Use found in Section 28.2902.

BACKGROUND

The petitioner is seeking a special use to allow a 4,395 square foot drive-through restaurant at 2525 Ogden Avenue. The 0.89 acre property is located on the south side of Ogden Avenue approximately 60 feet west of Cross Street and is currently improved with a vacant single family home and two vacant accessory structures. The property is zoned B-3, General Services and Highway Business. A drive-through use is an allowable Special Use in the B-3 district per Section 28.609 of the Zoning Ordinance. The restaurant and drive-through will be a 24-hour operation.

The site is proposed to have a single access point with one inbound lane and two outbound lanes to Ogden Avenue, as approved by the Illinois Department of Transportation (IDOT). A one-way counter-clockwise traffic pattern will be established around the building. Side-by-side drive-through ordering lanes will be provided to the southwest of the building with order pick-up occurring on the east side of the building. The petitioner is proposing to provide 37 parking and 13 stacking spaces where 36 parking and 8 stacking spaces are required.

Compliance with the Zoning Ordinance

The proposal meets all bulk requirements of the B-3 zoning district. Notable is that they will provide twice the required green space, including more than is required in the front yard. The amount of stacking is also substantial and exceeds requirements to help avoid any unwelcome traffic back-ups both on and off-site.

Parking and Traffic

The proposed use will not have any negative impact on the existing traffic patterns in the area. The petitioner has submitted a traffic impact study that describes, analyzes and evaluates the impact of the proposed development on the existing traffic conditions in the area.

The study examined IDOT’s average daily traffic volume on Ogden Avenue; conducted their own traffic counts on a weekday and weekend in September 2013; and examined the Institute of Transportation Engineers Trip Generation Handbook. The study found that the proposed restaurant will have a minimal impact on the existing traffic volume on Ogden Avenue.

The petitioner also undertook a gap analysis to determine if adequate gaps or interruptions existed within the traffic flow along Ogden Avenue to allow for traffic to enter and exit the site. The study found adequate gaps did exist to allow for efficient access to and from the site.

The study also examined the projected future conditions in 2019 and found that the proposed drive-through restaurant will not adversely impact the Ogden Avenue and Cross Street intersection.

Engineering and Public Improvements

The petitioner is not required to provide on-site stormwater detention because they are not adding more than 25,000 square feet of net new impervious area to the site. However, the petitioner will provide a volume control basin in the southwest corner of the property to meet the Post Construction Best Management Practice (PCBMPs) requirements. The proposal will meet all Stormwater Ordinance requirements.

Due to the significant grade changes from north to south, the southwest corner of the site is approximately 12 feet lower than the north property line, the petitioner will be constructing retaining walls on the east, west and south sides of the property. These masonry block retaining walls vary in height between two and nine feet and will be screened by extensive landscaping around the exterior of the site. A six-foot solid wood fence will be provided along the south retaining wall while a four foot decorative fence will be located along the east and west property lines.

Public Safety

The building will be equipped with an automatic suppression system and an automatic and manual fire alarm system. The site has been designed so that the Village’s largest fire apparatus can maneuver around the site.

Compliance with the Comprehensive Plan

The proposed drive-through restaurant is consistent with and meets the goals of the Comprehensive Plan, which includes redevelopment of sites with uses consistent with the Corridor Commercial designation and improved design.

Public Comment

During the Plan Commission meeting, one resident expressed the following concerns:

| Resident Concern | How the Concern is Addressed |
|---|--|
| Noise associated with the operation of a drive-through restaurant | <ul style="list-style-type: none"> • The drive-through speaker volume is variable based on ambient noise. If there is less ambient noise, the speaker volume will be lower. • No overnight deliveries or trash pick-up |

| | |
|---|---|
| Light pollution | <ul style="list-style-type: none"> • The petitioner provided a photometric plan which meets the Village’s light standards. |
| Increased traffic on Ogden Avenue and adjacent street network | <ul style="list-style-type: none"> • The petitioner’s traffic study identified no significant impact on Ogden Avenue or the adjacent street network. • The proposed development has no direct connection to Cross Street or Drendel Avenue. All traffic will enter and exit the site from Ogden Avenue. |

ATTACHMENTS

Ordinance

Aerial Map

Staff Report with attachments dated January 27, 2014

Draft Minutes of the Plan Commission Hearing dated January 27, 2014

ORDINANCE NO. _____

AN ORDINANCE AUTHORIZING A SPECIAL USE FOR 2535 OGDEN AVENUE TO PERMIT CONSTRUCTION OF A RESTAURANT WITH DRIVE-THROUGH

WHEREAS, the following described property, to wit:

Lots 2, 3, 4 and 5 in Block 2 in Arthur T. McIntosh and Company's Belmont Golf Addition, being a subdivision in the southwest quarter of Section 1 and in the northwest quarter of Section 12, Township 38 North, Range 10, East of the Third Principal Meridian, according to the plat thereof recorded September 14, 1925 as Document 199614 in DuPage County, Illinois.

Commonly known as 2535 Ogden Avenue, Downers Grove, IL
(PINs 08-01-305-006; -007; -008 and -009)

(hereinafter referred to as the "Property") is presently zoned in the "*B-3, General Services and Highway Business District*" under the Comprehensive Zoning Ordinance of the Village of Downers Grove; and

WHEREAS, the owner of the Property has filed with the Plan Commission, a written petition conforming to the requirements of the Zoning Ordinance, requesting that a Special Use per Section 28.609 of the Zoning Ordinance be granted to allow a construction of a restaurant with drive-through; and,

WHEREAS, such petition was referred to the Plan Commission of the Village of Downers Grove, and said Plan Commission has given the required public notice, has conducted a public hearing on January 27, 2014, respecting said petition and has made its findings and recommendations, all in accordance with the statutes of the State of Illinois and the ordinances of the Village of Downers Grove; and,

WHEREAS, the Plan Commission has recommended approval of the Special Use, subject to certain conditions; and,

WHEREAS, the Village Council finds that the evidence presented in support of said petition, as stated in the aforesaid findings and recommendations of the Plan Commission, is such as to establish the following:

1. The proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.
2. The proposed use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements in the vicinity.
3. The proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located.
4. The proposed use is one of the special uses specifically listed for the district in which it is to be located and, if approved with restrictions as set forth in this ordinance, will comply with the provisions of the Downers Grove Zoning Ordinance regulating this Special Use.

NOW, THEREFORE, BE IT ORDAINED by the Council of the Village of Downers Grove, in DuPage County, Illinois, as follows:

SECTION 1. That Special Use of the Property is hereby granted to allow construction of a restaurant with drive-through at 2535 Ogden Avenue within the B-3 zoning district.

SECTION 2. This approval is subject to the following conditions:

1. The proposed Special Use request for a restaurant with a drive-through use shall substantially conform to the Preliminary Engineering Plans for McDonald's Downers Grove, prepared by Watermark Engineering Resources Ltd. dated October 28, 2013, last revised December 5, 2013, the Freestanding Sign Exhibit plan and Wall Signage Exhibit plan prepared by Watermark Engineering Resources Ltd dated October 28, 2013, last revised December 5, 2013, proposed building elevation plans, prepared by Core States Group, dated March 21, 2013, proposed building floor plan prepared by Core States Group, dated March 21, 2013, Pervious/Impervious Exhibit, Truck Circulation plan, Fire Truck Circulation plan and Trash Enclosure Exhibit dated December 5, 2013 attached to the Staff Report dated January 27, 2014 except as such plans may be modified to conform to Village codes, ordinances, and policies.
2. The property consists of four lots of record. The final plat of subdivision for lot consolidation of the property into one new lot must be approved and recorded prior to issuance of the Building Permit.
3. The approval from IDOT, DuPage County Health Department, Downers Grove Sanitary District as well as a copy of the paid receipt for the DuPage County Impact fees will have to be submitted prior to issuance of the building permit.
4. The property must meet all requirements of the Sign Ordinance.
5. The speaker volume on the drive-through window must be reduced during overnight hours.
6. The building shall be equipped with an automatic suppression system and an automatic and manual fire alarm system.

SECTION 3. The above conditions are hereby made part of the terms under which the Special Use is granted. Violation of any or all of such conditions shall be deemed a violation of the Village of Downers Grove Zoning Ordinance, the penalty for which may include, but is not limited to, a fine and or revocation of the Special Use granted herein.

SECTION 4. That all ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

Mayor

Passed:

Published:

Attest: _____

Village Clerk



**VILLAGE OF DOWNERS GROVE
REPORT FOR THE PLAN COMMISSION
JANUARY 27, 2014 AGENDA**

| SUBJECT: | TYPE: | SUBMITTED BY: |
|-------------------------------|--|---|
| PC-45-13 2535 Ogden Street | Special Use for a Restaurant with drive-through use | Stanley J. Popovich, AICP Senior Planner |

REQUEST

The petitioner is requesting approval of a Special Use to construct a new restaurant (McDonald's) with a drive-through use at 2535 Ogden Avenue.

NOTICE

The application has been filed in conformance with applicable procedural and public notice requirements.

GENERAL INFORMATION

OWNER: Scherston Real Estate Investments, LLC.
4685 Winfield Road
Warrenville, IL 60555

APPLICANT: McDonald's USA, LLC
4320 Winfield Road, Suite 400
Warrenville, IL 60555

PROPERTY INFORMATION

EXISTING ZONING: B-3, General Services and Highway Business
EXISTING LAND USE: Vacant
PROPERTY SIZE: 38,815 square feet (0.89 acres)
PIN: 08-01-305-006, -007, -008, -009

SURROUNDING ZONING AND LAND USES

| | ZONING | FUTURE LAND USE |
|---------------|--|------------------------|
| North: | B-3, General Services and Highway Business | Corridor Commercial |
| South: | B-3, General Services and Highway Business | Corridor Commercial |
| East: | B-3, General Services and Highway Business | Corridor Commercial |
| West: | B-3, General Services and Highway Business | Corridor Commercial |

ANALYSIS

SUBMITTALS

This report is based on the following documents, which are on file with the Department of Community Development:

1. Application/Petition for Public Hearing
2. Project Summary
3. Plat of Survey
4. Site Development Plans
5. Architectural Drawings
6. Traffic Impact Study

PROJECT DESCRIPTION

The subject property, commonly known as 2535 Ogden Avenue, is located on the south side of Ogden Avenue just west of Cross Street and is zoned B-3 General Services and Highway Business. The 38,815-square foot property is improved with a single family home and two accessory structures. The property was formerly occupied by a landscape contractor business, but is currently vacant. The petitioner is proposing to demolish all existing structures and construct a new McDonald's restaurant building with a side-by-side drive-through and a surface parking lot. The petitioner is requesting approval of a Special Use for the drive-through use.

A restaurant with a drive-through use is listed in Section 28.609 of the Zoning Ordinance as an allowed Special Use in the B-3, General Services and Highway Business district. The subject property is comprised of four adjacent lots of record. As such, the petitioner will have to consolidate the property into one new lot. The lot consolidation can be approved administratively and will have to be completed prior to issuance of the building permit.

If the proposal is approved, the existing McDonald's restaurant at 1620 Ogden Avenue would be closed and operations relocated to the new facility at 2535 Ogden Avenue.

Site Layout

The petitioner is proposing to construct a 4,395-square foot restaurant building near the center of the property with a surface parking lot east and west of the building. The petitioner is proposing one full access driveway on Ogden Avenue with one inbound lane and two outbound lanes including a middle combined left/through lane and a right-turn only lane under stop sign control. The Illinois Department of Transportation (IDOT) has approved the location of the proposed curb cut.

The drive-through pick-up window located on the east side of the building is designed to allow counter-clockwise drive-through traffic around the building. Two side-by-side ordering lanes are located southwest of the building. The drive-through lane is designed to accommodate a total of 13 vehicles which meets the Village's requirement for eight stacking spaces. The 37-space parking lot would also include a one-way counter-clockwise circulation pattern. New parking lot landscape screening would be installed around the perimeter of the site. The trash enclosure is located in the southeast corner of the property and a new monument sign would be located in the northeast corner of the site. A six-foot high solid wood fence is proposed along the south side of the property while a four-foot open ornamental fence would be located along the east and west sides for the property.

The proposed restaurant and the drive-through window will be open 24 hours. All services to the building, including deliveries and trash pick-up, will be conducted during normal business hours.

COMPLIANCE WITH THE COMPREHENSIVE PLAN

The proposal is consistent with the Comprehensive Plan. The property is designated as Corridor Commercial in the Comprehensive Plan. The Plan calls for commercial uses that cater to the automobile and are typically organized in linear fashion. The proposed restaurant with a drive-through lane is consistent with the Corridor Commercial designation.

The Comprehensive Plan notes the Ogden Avenue corridor should continue to function in its dual role by serving the daily needs of local residents and providing commercial services to the larger region. The proposed restaurant with a drive-through pick-up window will provide service to both local residents and residents from a wider region traveling along Ogden Avenue.

The Plan also promotes redevelopment of underutilized commercial properties in areas that can provide convenient general commercial needs to the community. The Plan calls for perimeter landscape screening of parking areas, high level of design that blends with surrounding properties, adds value to the adjacent properties and improves access management along Ogden Avenue. The proposed plan will include a high quality masonry design building with landscape screening on the perimeter of the parking lot. The proposed dumpster is also fully enclosed and located in the southeast corner of the property. The proposed curb-cut is located across from the existing curb-cut at InTown Suites hotel and will improve access to the property. The proposed use and the proposed plan are consistent with the Comprehensive Plan.

COMPLIANCE WITH THE ZONING ORDINANCE

The property is zoned B-3, General Services and Highway Business District. The properties on all four sides adjacent to the site are also zoned B-3. The proposed restaurant with a drive-through use is listed as an allowable special use in this district.

The proposal will meet all bulk requirements of the Zoning Ordinance. The characteristics of the proposed development are outlined in the table below:

| 2535 Ogden Ave- McDonald's | Required | Proposed |
|---|---|---|
| <i>Building</i> | | |
| Front Setback | 75 ft. from center-line of Ogden Ave | 85 ft. from center-line of Ogden Ave |
| Side Setback | N/A | 59 ft. east, 89 ft. west |
| Rear | N/A | 52 ft. |
| Height | 60 ft. | 18.75 ft. |
| FAR | 0.75 (29,111 sq. ft.) | 0.11 (4,478 sq. ft.) |
| Green space | 10 % (3,881 sq. ft.) | 20 % (7,858 sq. ft.) |
| % of green space required in front yard | 5% (1,940 sq. ft.) | 6.6 % (2,586 sq. ft.) |
| <i>Parking</i> | | |
| Front Setback | 50 ft. from center-line of Ogden Ave | 57 ft. from center-line of Ogden Ave |
| Side Setback | N/A | 9 ft. east and west |
| Rear | N/A | 34 ft. |
| # of parking spaces required | 36 | 37 |
| # of stacking spaces required | 8 | 13 |

The proposal will meet all requirements for parking lot perimeter screening. The petitioner is also proposing new parking lot lighting. The proposed 21-foot high light poles are less than the maximum permitted height of 30 feet. The photometric plan meets all lighting requirements.

All new signage on the property will meet the Village's Sign Ordinance requirements. One new ten-foot high and 36-square foot monument sign will be installed in the northeast corner of the property. One new wall sign will be installed on the north side of the building while additional site directional and drive-through lane signage will also be installed.

PARKING AND TRAFFIC

The proposed use will not have any negative impact on the existing traffic patterns in the area. The petitioner has submitted a traffic impact study that describes, analyzes and evaluates the impact of the proposed development on the existing traffic conditions in the area.

Based on the traffic count data from the Illinois Department of Transportation (IDOT), the average daily traffic volume on Ogden Avenue adjacent to the site is 27,500 vehicles. The petitioner conducted traffic counts on Tuesday, September 24, 2013 during morning, mid-day and evening peak periods. Additionally, traffic counts were conducted on Saturday September 21, 2013 during mid-day peak periods. Based on the Institute of Transportation Engineers (ITE) Trip Generation Handbook, a fast food restaurant with a drive-through service would generate approximately 200 total trips during weekday morning peak hour, 144 total trips during weekday evening peak hour and 260 total trips during weekday and Saturday mid-day peak hour. However, not all trips generated by a drive-through restaurant are new trips. Significant portions of site-generated trips are pass-by trips especially during morning and evening peak hours. Per the ITE Trip Generation Handbook, the average pass-by trips for a drive-through restaurant are approximately 50% during both morning and evening peak hours. As such, with the reduction for pass-by trips, the site will generate approximately 98 new trips during morning peak hour, 70 new trips during evening peak hour and 128 new trips during weekday mid-day and Saturday mid-day peak hours. As such the proposed restaurant will have a minimal impact on the existing traffic volume on Ogden Avenue.

The petitioner also conducted a gap analysis during peak periods to determine if adequate gaps or interruptions exist in the traffic stream on Ogden Avenue to allow for traffic to enter and exit the site. The study found the least number of gaps (39) will be available during weekday evening peak hour, but the number of gaps counted exceeds the required number of gaps (37) needed for the anticipated number of vehicles exiting the site and making the most difficult left-turn movement. It should be noted that per the Highway Capacity Manual, one gap for a left-turn outbound movement must be a minimum of 7.5 seconds. However, if an observed gap exceeds 7.5 seconds, it could allow for multiple vehicles to exit the site. The site is also designed to accommodate up to 50 feet of space for queuing of exiting vehicles without creating an internal conflict or backup. As such, there is adequate number of gaps in the existing traffic flow to allow for efficient access to the site.

The traffic study also provides analysis of the existing roadway capacity conditions and projected future conditions in year 2019. The study concludes that the McDonald's site generated traffic will have a limited impact on the nearby Ogden Avenue and Cross Street intersection. The intersection will continue to operate at the same level of service with an increase on the overall delay of 1.6 seconds or less.

The drive-through pick-up window located on the east side of the building is designed to allow for counter-clockwise drive-through circulation around the building. The drive-through lane is designed to accommodate a total of 13 vehicles; the dual ordering lanes allow for stacking of six vehicles behind the order boards while additional stacking for seven vehicles is provided between the pick-up window and order boards. The proposed drive-through lane exceeds the Village requirement for 8 stacking spaces.

The proposed 37-space parking lot would also include a one-way counter-clockwise circulation pattern. The 37 proposed parking spaces exceed the 36 spaces required by Code.

ENGINEERING/PUBLIC IMPROVEMENTS

The property is currently improved with a single family home and two accessory structures that formerly housed a landscape contractor business. The petitioner will demolish all existing structures and construct a new 4,395-square foot building with adjacent surface parking lot. An on-site stormwater detention basin is not required. The petitioner will, however, provide natural landscape materials and a volume control basin in the southwest corner of the property to meet the requirements for Post Construction Best Management Practice (PCBMPs) for stormwater management. The proposal will meet all requirements of the Village's and DuPage County stormwater ordinances.

The property has significant topographical changes from north to south. The southwest corner of the property is approximately 12 feet lower than the front of the property along Ogden Avenue. To provide a level surface, the petitioner will construct retaining walls on the east, south and west sides of the property. The proposed masonry block retaining walls will be located five feet from the east, south and west property lines. The height of the retaining wall will range from two feet closest to Ogden Avenue to approximately nine feet in the southwest corner of the property. The walls around the trash enclosure and volume control basin will be concrete. A six foot high solid wood fence will be installed on top of the retaining wall along the south property line while a four-foot high ornamental open design fence will be installed on top of the retaining wall along the east and west sides of the property. A guard rail will also be installed adjacent to the fence.

The proposed site will include a total of 7,858 square feet of landscaped green space (20% of lot area) of which 2,586 square feet (6.6% of lot area) will be located in the front yard. The landscape screening will be installed on all sides of the parking lot and along the outer perimeter of the retaining walls. The proposed landscaped green space and parking lot screening meet all requirements of the Zoning Ordinance.

The petitioner is proposing one full access driveway to Ogden Avenue directly across from the access driveway for the InTown Suites hotel on the north side of Ogden Avenue. The proposed driveway will have one inbound lane and two outbound lanes including a middle combined left/through lane and right-turn only exit lane under stop sign control. IDOT has approved the proposed location of the curb-cut.

PUBLIC SAFETY REQUIREMENTS

The Fire Prevention Division of the Fire Department has reviewed the proposed plans. The Fire Prevention division noted there is adequate space on the property and around the building for emergency response vehicles. The proposed building will be fully sprinkled and equipped with a manual and automatic fire alarm system.

NEIGHBORHOOD COMMENT

Notice was provided to all property owners 250 feet or less from the property line in addition to posting the public hearing sign and publishing a legal notice in the *Downers Grove Suburban Life* newspaper. The petitioner also held two neighborhood meetings on November 26, 2013 and December 17, 2013. The summary of neighborhood concerns and information provided to the residents at the meetings is attached to this report. Staff has not received any comments regarding the proposal at this time.

FINDINGS OF FACT

Special Use

The applicant is requesting a special use approval to construct a new restaurant with a drive-through use. Staff believes the proposed use meets the standards for granting a special use as outlined below:

Section 28.1902 Standards for Approval of Special Uses

The Village Council may authorize a special use by ordinance provided that the proposed Special Use is consistent and in substantial compliance with all Village Council policies and land use plans, including but not limited to the Comprehensive Plan, the Future Land Use Plan and Master Plans and the evidence presented is such as to establish the following:

- (a) *That the proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*

The proposed restaurant with a drive-through use provides a desirable service that contributes to the general welfare of the community. The proposed use is also consistent with the Comprehensive Plan's recommendation for redevelopment of underutilized commercial properties in areas that provide convenient and general commercial needs to both local residents and the residents from a wider region. This site is currently vacant.

The proposal is compatible with surrounding uses and will contribute to the general welfare of the neighborhood and the community. The proposed plan will include a high quality masonry design building with landscape screening and fencing on the perimeter of the parking lot. This standard is met.

- (b) *That such use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements in the vicinity.*

The proposed use will not be detrimental to the health, safety or the general welfare of persons in the vicinity of the site. The proposed use is similar in nature to other automobile oriented uses along Ogden Avenue. The anticipated site-generated traffic will have a minimal impact on the existing traffic pattern along Ogden Avenue and the Ogden Avenue and Cross Street intersection. The traffic study indicates there are adequate gaps in the existing traffic stream on Ogden Avenue to accommodate anticipated traffic entering and exiting the site. The study also concludes there is adequate capacity on Ogden Avenue to handle the additional site-generated trips of the proposed use. This standard is met.

- (c) *That the proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located or will comply with any variation(s) authorized pursuant to Section 28-1802.*

The proposal will meet all bulk requirements for the B-3 zoning district. All new exterior signage will meet Village's Sign Ordinance requirements. The proposal will also comply with Village's and DuPage County stormwater ordinances. This standard is met.

- (d) *That it is one of the special uses specifically listed for the district in which it is to be located.*

The proposed restaurant with a drive-through use is permitted as a Special Use in the B-3 district per Section 28.609 of the Zoning Ordinance. This standard is met.

RECOMMENDATIONS

The proposed restaurant with a drive-through use meets the standards for Special Use, is consistent with the Comprehensive Plan and is compatible with surrounding zoning and land use classifications. Based on the findings listed above, staff recommends the Plan Commission make a positive recommendation to the Village Council regarding PC 45-13 subject to the following condition:

1. The proposed Special Use request for a restaurant with a drive-through use shall substantially conform to the Preliminary Engineering Plans for McDonald's Downers Grove, prepared by Watermark Engineering Resources Ltd. dated October 28, 2013, last revised December 5, 2013, the Freestanding Sign Exhibit plan and Wall Signage Exhibit plan prepared by Watermark Engineering Resources Ltd dated October 28, 2013, last revised December 5, 2013, proposed building elevation plans, prepared by Core States Group, dated March 21, 2013, proposed building floor plan prepared by Core States Group, dated March 21, 2013, Pervious/Impervious Exhibit, Truck Circulation plan, Fire Truck Circulation plan and Trash Enclosure Exhibit dated December 5, 2013 attached to this report except as such plans may be modified to conform to Village codes, ordinances, and policies.
2. The property consists of four lots of record. The final plat of subdivision for lot consolidation of the property into one new lot must be approved and recorded prior to issuance of the Building Permit.
3. The approval from IDOT, DuPage County Health Department, Downers Grove Sanitary District as well as a copy of the paid receipt for the DuPage County Impact fees will have to be submitted prior to issuance of the building permit.
4. The property must meet all requirements of the Sign Ordinance.
5. The speaker volume on the drive-through window must be reduced during overnight hours.
6. The building shall be equipped with an automatic suppression system and an automatic and manual fire alarm system.

Staff Report Approved By:

Tom Dabareiner, AICP
Director of Community Development

TD:sp
-att

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November 1, 2013

Mr. Stan Popovich
Village Planner
Village of Downers Grove
801 Burlington Avenue
Downers Grove, IL 60515

Re: Petition for Lot Consolidation and Special Use ("Application")
McDonald's USA, LLC ("Applicant")
Scherston Real Estate Investments, LLC ("Owner")
2535 Ogden Avenue, Downers Grove, IL ("Property")

Dear Mr. Popovich:

Rathje & Woodward, LLC has been retained by the above referenced Applicant, McDonald's USA, LLC, to represent its interest concerning the above referenced Application. Fee title to the Property is held by the above referenced Owner. Provided the Application is approved by the Village, legal title to the Property will be conveyed to MA/GMX Downers Grove, LLC and Applicant will become the ground lessee of the Property. Applicant is desirous of demolishing the existing structures located on the Property and constructing thereon a state-of-the-art McDonald's restaurant with a drive-thru facility which incorporates a side-by-side order system. The Property is currently zoned under the B-3 General Services and Highway District of the Village's Zoning Ordinance. Pursuant to Sections 28.608(a) and 28.609(a) of the Zoning Ordinance, the B-3 District includes the permitted uses (excepting dwellings) and special uses set forth under the B-2 General Retail Business District of the Zoning Ordinance. Section 28.606(f) of the Zoning Ordinance provides for "drive-in, drive-up and drive-through uses, including but not limited to banks and restaurants" as a special use in the B-2 District, which is therefore also a special use under the B-3 District. Accordingly, Applicant is seeking the approval of a special use for the construction, operation and maintenance of the proposed McDonald's restaurant with said drive-through facilities when located in the B-3 District. In conjunction with said application for special use, Applicant is further requesting the approval by the Village of a final plat of subdivision for the purpose of consolidating the existing four parcels comprising the Property into a single subdivided lot.

Mr. Stan Popovich
Re: 2535 Ogden Avenue
Downers Grove, IL
November 1, 2013
Page Two

In pursuit of the relief requested by Applicant and Owner, Applicant is prepared to present evidence to the Village as a part of the hearing and review process, demonstrating the proposed project's compliance with the applicable standards for approval, to wit:

1. Special Use.

(a) The proposed special use is consistent with the Village's Comprehensive Plan as well as the Ogden Avenue Master Plan. The Property is zoned under the B-3 General Services and Highway District, which is an appropriate zoning classification for the Property based upon the Village's long term planning and allows for the proposed use.

(b) The Applicant currently operates an existing McDonald's restaurant serving the subject trade area located at 1620 E. Ogden Avenue ("Existing Location"). The building and drive-through facilities of the Existing Location are antiquated and fail to provide the level of customer services and benefits to the community which are provided with the state-of-the-art facilities proposed by Applicant for the Property. Due to the limited size of the Existing Location, it is not capable of supporting an on-site redevelopment which would justify the substantial reinvestment by Applicant, as contemplated for the Property. The long standing use by Applicant of the Existing Location has demonstrated the public demand for the service provided and the public convenience realized through the existence of a west side McDonald's restaurant location. Following several years of searching for a new west side location of sufficient size to support the current restaurant and drive-thru standards of Applicant, the Property is the only location which has become available that has the requisite size and configuration to support the project and is properly zoned for the proposed use. The continued provision of Applicant's services in this trade area and the substantial economic investment by Applicant in the project will serve to contribute to the general welfare of the neighborhood and the community at large.

(c) The proposed special use will allow Applicant to move its business from the Existing Location into a new facility which will incorporate the latest building and site design, drive-through technology and public safety improvements. The properties located adjacent to the Property to the east, south, west and north, across Ogden Avenue, are zoned under the B-3 District, consistent with the Property. A Culver's restaurant with a drive-through facility is located on the north side of Ogden Avenue at the northwest corner of Ogden and Cross Street. The proposed side-by-side drive-through order facility has been proven to substantially improve the efficiency of the drive-through system, resulting in a much shorter queue during peak hours of operation. The shorter drive-through queue improves the efficiency

Mr. Stan Popovich
Re: 2535 Ogden Avenue
Downers Grove, IL
November 1, 2013
Page Three

and safety of on-site traffic circulation and eliminates any risk of drive-through back up into the public right-of-way. This also brings about improved sales volume which serves to enhance the tax revenue realized by the community. Accordingly the proposed use will not be detrimental to, but will in fact improve, the health, safety, morals and general welfare of persons residing or working within the vicinity and will not be injurious to property, property value or improvements in the vicinity.

(d) Applicant is not requesting any variations under the Village's Zoning Ordinance. The proposed use will comply with the regulations specified in the Zoning Ordinance for the B-3 District.

(e) The proposed special use is an allowed special use within the B-3 District pursuant to the B-3 District's incorporation of special uses within the B-2 district, pursuant to the Sections of the Village's Zoning Ordinance hereinabove identified.

2. Plat of Subdivision/Consolidation. Applicant's request for approval of a final plat of subdivision is made for the purpose of facilitating the consolidation of the four (4) existing parcels comprising the Property into a single subdivided lot. The requested consolidation is consistent with the standards and objectives of the Village as set forth in the ordinances and policies of the Village. The proposed plat of subdivision will conform with all of the applicable standards of the Village's Subdivision Ordinance, including Section 20-507 entitled "Lot Consolidations".

In support of the foregoing, Applicant and Owner herewith submit the following materials:

1. Petition for Plan Commission;
2. Rathje & Woodward, LLC Check #114876 in the amount of \$1101.00 in payment of the applicable Application fees;
3. Legal Description of the Property;
4. Proof of Ownership;
5. Owner Authorization;
6. Certification of Public Notice Information including mailing labels;
7. Four (4) copies of the KLOA Traffic Memorandum dated October 25, 2013 ;

Mr. Stan Popovich
Re: 2535 Ogden Avenue
Downers Grove, IL
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Page Four

8. Six (6) groupings of plan sets, each containing the following documents:
 - (a) Preliminary Engineering Plans, consisting of the following sheets:
 - (i) Cover Sheet
 - (ii) Demolition Plan, dated October 28, 2013;

 - (iii) Geometric (Site) Plan, dated October 28, 2013;
 - (iv) Grading Plan, dated October 28, 2013;
 - (v) Accessible Route Grades & Details, dated October 28, 2013;
 - (vi) Utility Plan, dated October 28, 2013;
 - (vii) Project Details, dated October 28, 2013;

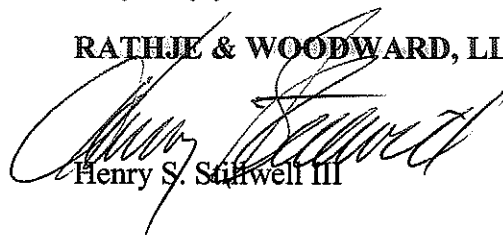
 - (viii) Project Details, dated October 18, 2013;
 - (ix) Project Specifications, dated October 28, 2013;
 - (x) ALTA/ACSM Land Title and Topographic Survey, dated September 23, 2013;
 - (xi) Landscape Plans, dated October 28, 2013;
 - (xii) Point-by-Point Foot Candle Plot, dated October 28, 2013;
 - (xiii) Drive-Thru Pavement Plan, Sheet DT-1, dated October 28, 2013
 - (xiv) Drive-Thru Equipment Plan, Sheet DT-2, dated October 28, 2013;
 - (xv) Drive-Thru Details, Sheet DT-3, dated October 28, 2013;
 - (xvi) Drive-Thru Details, Sheet DT-4, dated October 28, 2013;
 - (b) Wall Signage Exhibit, dated October 28, 2013;
 - (c) Freestanding Signage Exhibit, dated October 28, 2013;
 - (d) Floor Plans, dated March 21, 2013;
 - (e) Exterior Elevations, Sheet A2.0, dated March 21, 2013;
 - (f) Exterior Elevations, Sheet A2.1, dated March 21, 2013;
 - (g) Final Plat of Subdivision, dated October 25, 2013;
 - (h) ALTA/ACSM Land Title & Topographic Survey consisting of two (2) sheets, dated September 23, 2013
9. One (1) Disk of TIFFs of Submittal Documents

Mr. Stan Popovich
Re: 2535 Ogden Avenue
Downers Grove, IL
November 1, 2013
Page Five

Please accept this letter and all of the enclosures submitted herewith for filing and initiate all appropriate procedures in order to facilitate the scheduling of applicable public hearings and meetings. Should you require any additional materials or information concerning the Application, please do not hesitate to contact the undersigned. Please advise the undersigned of all meetings and hearings as the same are from time to time scheduled. Your attention to this matter will be very much appreciated.

Very truly yours,

RATHJE & WOODWARD, LLC



Henry S. Stillwell III

HSS:emd

Enclosures

cc: Rick Dolan (via email w/o enclosures)
Mary Meyer (via email w/o enclosures)
Dan Olson (via email w/o enclosures)
Jeff Miller (via email w/o enclosures)



Via E-Mail Delivery – spopovich@downers.us

December 3, 2013

Mr. Stan Popovich
Senior Planner
Department of Community Development
Village of Downers Grove
801 Burlington Avenue
Downers Grove, Illinois 60515

Re: Proposed McDonald's Restaurant Relocation Site at 2535 Ogden Avenue in Downers Grove, IL

Dear Stan:

Our project team voluntarily hosted a meeting with the residents surrounding the subject proposed development site in the multi-purpose room at the Downers Grove Recreation/Fitness Center located at 4500 Belmont Road between 6:00 pm and 8:00 pm on Tuesday, November 26, 2013. The purpose of the meeting was to introduce the proposed project and to address any concerns raised by the neighbors.

All property owners within 250 feet of the subject site were sent federal express notifications on Monday, November 11th regarding the neighborhood meeting. Five property owners attended the meeting, including:

- Matt Wenc who resides at 4424 Cross Street;
- John Hajek who resides at 4500 Cross Street;
- Martha Richardson who resides at 4416 Cross Street;
- Audrey Pareighis who resides at 4411 Cross Street; and,
- Keji Akin who owns the commercial business Balance at 2525 Ogden Avenue.

Mr. Hajek and Mrs. Richardson appeared most concerned about the proposed development.

Also from our project entitlement team, the following team members attended the meeting:

- Dennis Somers from McDonald's USA, LLC;
- Rick Dolan from McDonald's USA, LLC; and,
- Dan Olson from Watermark Engineering Resources, Ltd.



Letter to Mr. Stan Popovich

Regarding Proposed McDonald's Restaurant Relocation at 2535 Ogden Avenue in Downers Grove, IL

December 3, 2013

Page 2

The general concerns as expressed by the residents are noted below along with an explanation of how our project team plans on addressing said concerns:

1. **Cooking odors and smells** - McDonald's employs industry-standard clean cooking procedures and current state-of-the-art exhaust systems, which ensures that no cooking odors will leave the premises and that smoke exhaust will not pollute the environment;
2. **Neighborhood lighting glare** - the proposed development includes low-impact LED building and parking lot lighting levels, light pole fixture positioning with 90 degree down lighting, no parking lot pole lighting behind the restaurant facility (closest to the neighborhood), no light illumination that will spill over on to the adjacent residential properties and will meet the Village code and a six (6) foot high cedar board-on-board fence;
3. **Proposed 24-hour drive-thru business** - McDonald's must have the option of having 24-hour service within the dining facility and the drive-through facility, but such determination will be based on the profitability of such operations and will also be evaluated as demand changes over time;
4. **Customer garbage littering** - McDonald's operator will have staff that frequently monitors customer trash in and around the grounds of the premises to ensure that the premises and its surrounds are maintained in a clean manner and free of debris that might otherwise end up strewn into the neighborhood;
5. **Trash pick-up times and frequency** - McDonald's current trash pick-up policies include an average trash pick-up of approximately two pick-up times per week, typically during normal working hour time periods which is not very frequent;
6. **Trash containment** - a planned trash corral is to be located at the southeast corner abutting the proposed retaining wall at the south and east property boundaries of the subject site which will be self-contained within a brick structure surrounded by new shrubs, grasses and perennials and will not interfere with vehicular circulation around the parking lot;
7. **Semi-trailer truck delivery times and on-site containment** - the parking lot will be large enough to contain on-site semi-trailer truck traffic, which will likely take place after 7:00 am, and overnight truck idling and/or parking will not be permitted;



Letter to Mr. Stan Popovich

Regarding Proposed McDonald's Restaurant Relocation at 2535 Ogden Avenue in Downers Grove, IL

December 3, 2013

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8. **Rodent and animal garbage picking** - McDonald's store policies and procedures emphasize clean restaurant operations and the self-contained and protected trash corral area should keep the trash area rodent-free;
9. **Restaurant operation noise** - neighborhood noise filters include high efficient roof-top mounted HVAC units shielded by building parapets, the proposed retaining walls up to ten (10) feet high at the eastern, western and southern property boundaries, abundant landscape screening along the subject lot's property boundaries and the six (6) foot cedar board-on-board fence;
10. **Building appearance and aesthetics** - the upgraded architectural appearance of the proposed building surpasses typical building standards, including predominant brick veneer with tasteful modern accents and minimal building signage;
11. **Drive-thru speaker box noise** - the drive-through speaker box locations will be surrounded by bushes and perennial plantings located along the southwest side of the proposed building structure and the modern speaker boxes modulate their volume based upon the ambient noise (noise becomes minimal with further distance from the boxes);
12. **Perception regarding diminution of residential property values** – the entitlement team will solicit input from area professionals such as realtors and/or property appraisers to address this concern, but past experience suggests that well-planned and family-friendly commercial businesses attract additional employment opportunities and sales tax revenues to a neighborhood, which tends to enhance residential property values instead of diminishing them;
13. **Stormwater drainage runoff** - the project's engineering and stormwater drainage must not only adhere to today's strict Village standards and requirements, but must also meet the requirements of the DuPage County Stormwater Management System to ensure a well-planned drainage system on and exiting outflow from the subject site;
14. **Perception of adequacy of current McDonald's restaurant location** - the current restaurant's location at 1620 Ogden Avenue is not large enough to accommodate a modern day McDonald's restaurant with a side-by-side drive-through facility and is also difficult to navigate due to the steep grade differential between the restaurant's parking lot and its location fronting Ogden Avenue;



Letter to Mr. Stan Popovich

Regarding Proposed McDonald's Restaurant Relocation at 2535 Ogden Avenue in Downers Grove, IL

December 3, 2013

Page 4

15. **"More suitable" restaurant relocation alternative site** - GMX conducted an exhaustive suitable site selection search for McDonald's commencing in January 2012 that took the better part of 18 months to complete (please see the enclosed *Suitable Site Selection Analysis* prepared by GMX Real Estate Group, LLC); and,
16. **Vehicular traffic congestion to/from the site**- KLOA's traffic study concluded that vehicular traffic will function with proper traffic queuing to/from the subject site (without any direct connection or impact to the neighborhood fronting Cross Street) and adequate internal parking lot circulation and the Illinois Department of Transportation (IDOT) has preliminarily approved the Ogden Avenue curb cut configuration and geometry for the proposed development.

McDonald's has a good neighbor policy that permeates McDonald's culture and is promoted at every level of its corporate hierarchy. This is why we took the initiative to meet with the neighbors and address their concerns. In conclusion, we will have a follow-up meeting with the neighbors again at the Downers Grove Recreation/Fitness Center between 6:00 pm and 8:00 pm on Tuesday, December 17, 2013, which gives us adequate time to address the neighbors' concerns before the planned Village Plan Commission meeting on January 6, 2014.

In the meantime, please do not hesitate to contact me or any member of the entitlement team if you would like to further discuss this matter.

Sincerely,

GMX REAL ESTATE GROUP, LLC

A handwritten signature in black ink, appearing to be "A. Goodman", written over a horizontal line.

Andrew S. Goodman, its Co-Manager

As Development Agent for MA/GMX Downers Grove, LLC

Enclosures:

- Suitable Site Selection Analysis prepared by GMX Real Estate Group, LLC
- KLOA Traffic Comparison between 1620 Ogden Avenue and 2535 Ogden Avenue



Letter to Mr. Stan Popovich

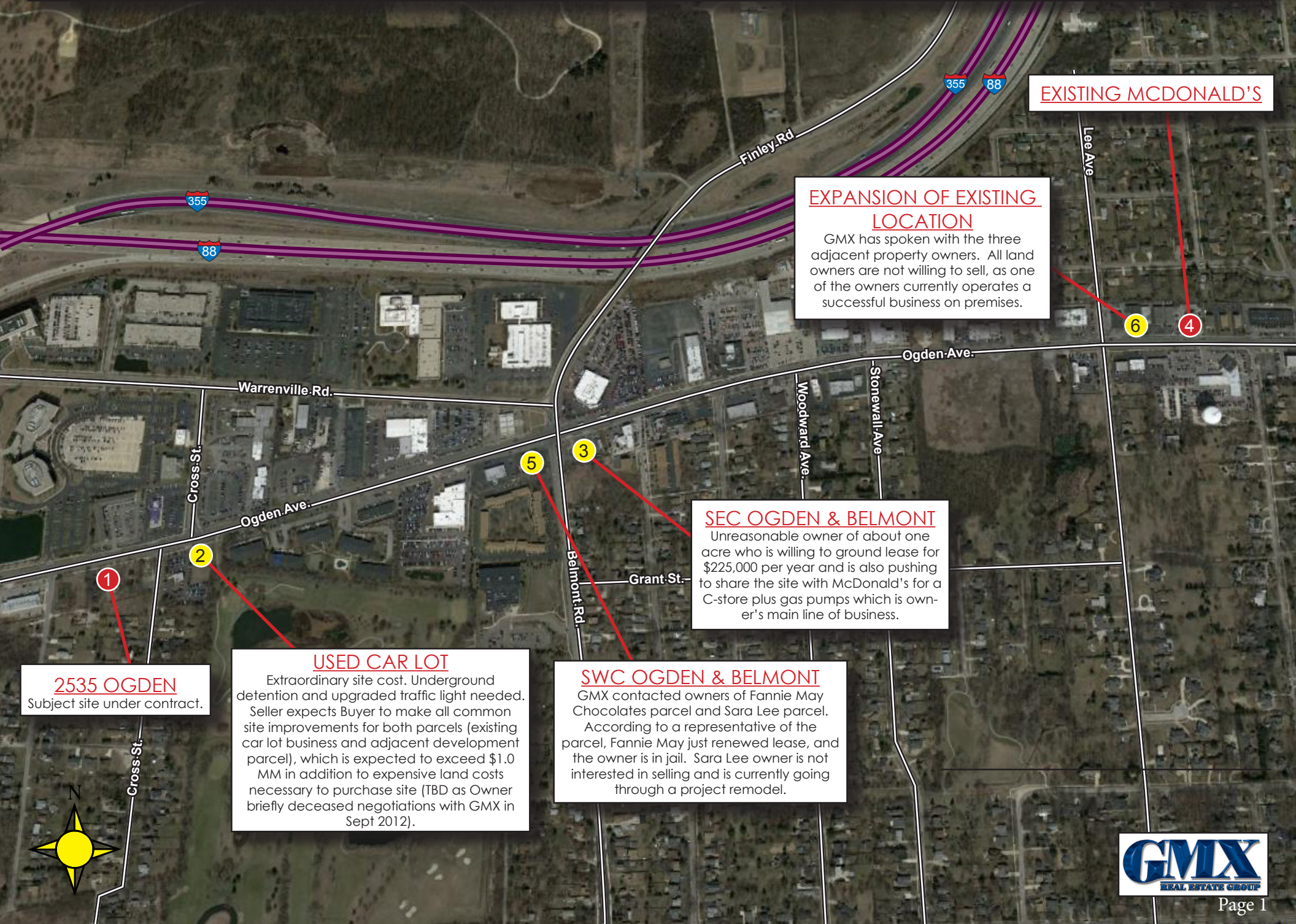
Regarding Proposed McDonald's Restaurant Relocation at 2535 Ogden Avenue in Downers Grove, IL

December 3, 2013

Page 5

cc: Rick Dolan, McDonald's USA, LLC
Mary Meyer, McDonald's USA, LLC
Dennis Somers, McDonald's USA, LLC
Michael Cassa, Downers Grove Economic Development Corporation
John Ginsberg, GMX Real Estate Group, LLC
Luay Aboona, KLOA, Inc.
Matt Bailey, Watermark Engineering Resources, Ltd.
Dan Olson, Watermark Engineering Resources, Ltd.
Hank Stillwell III, Rathje & Woodward LLC

TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL



EXISTING MCDONALD'S

EXPANSION OF EXISTING LOCATION
 GMX has spoken with the three adjacent property owners. All land owners are not willing to sell, as one of the owners currently operates a successful business on premises.

SEC OGDEN & BELMONT
 Unreasonable owner of about one acre who is willing to ground lease for \$225,000 per year and is also pushing to share the site with McDonald's for a C-store plus gas pumps which is owner's main line of business.

SWC OGDEN & BELMONT
 GMX contacted owners of Fannie May Chocolates parcel and Sara Lee parcel. According to a representative of the parcel, Fannie May just renewed lease, and the owner is in jail. Sara Lee owner is not interested in selling and is currently going through a project remodel.

USED CAR LOT
 Extraordinary site cost. Underground detention and upgraded traffic light needed. Seller expects Buyer to make all common site improvements for both parcels (existing car lot business and adjacent development parcel), which is expected to exceed \$1.0 MM in addition to expensive land costs necessary to purchase site (TBD as Owner briefly deceased negotiations with GMX in Sept 2012).

2535 OGDEN
 Subject site under contract.



TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL



Ogden Ave.

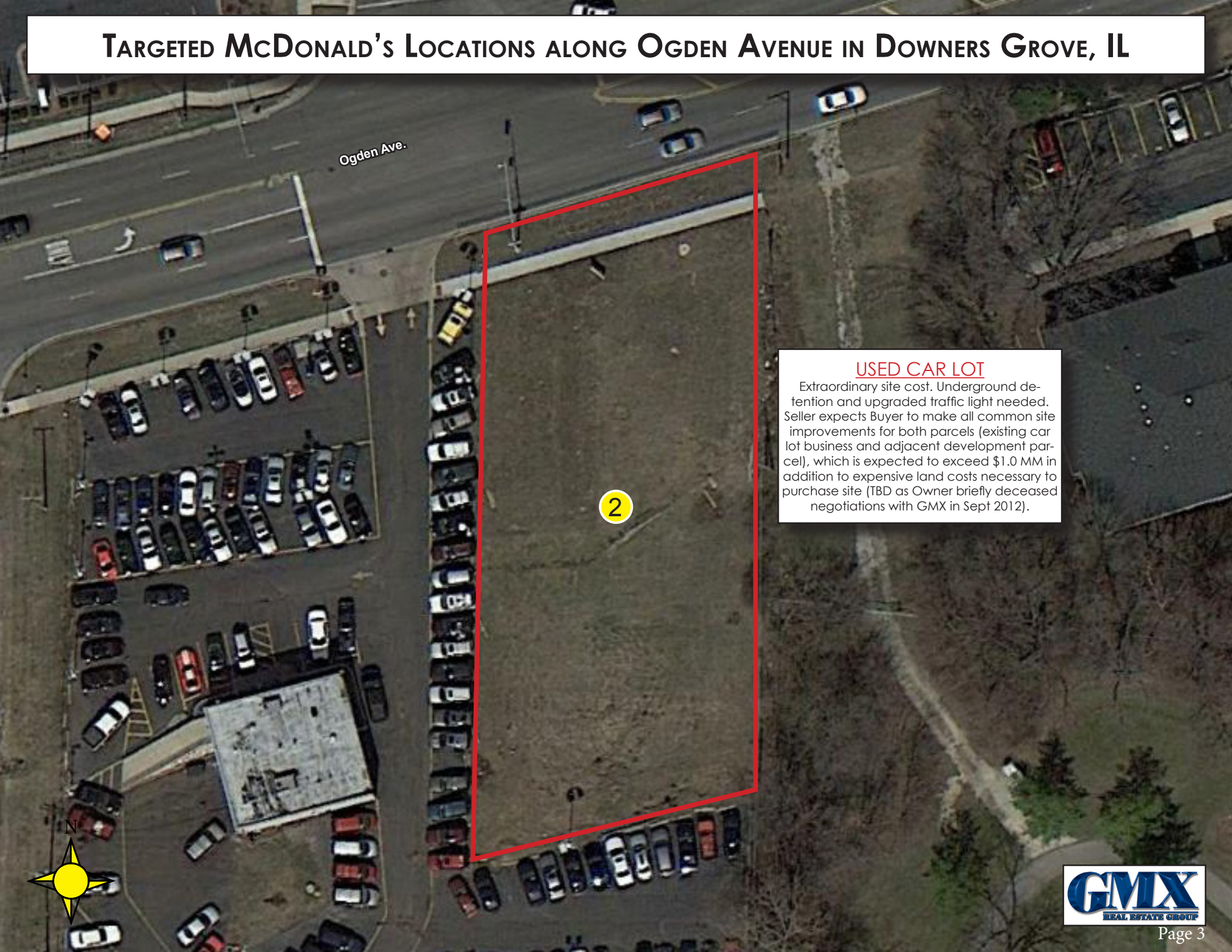
2535 OGDEN
Subject site under contract.

1

Cross St.



TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL



Ogden Ave.

2

USED CAR LOT
Extraordinary site cost. Underground detention and upgraded traffic light needed. Seller expects Buyer to make all common site improvements for both parcels (existing car lot business and adjacent development parcel), which is expected to exceed \$1.0 MM in addition to expensive land costs necessary to purchase site (TBD as Owner briefly deceased negotiations with GMX in Sept 2012).



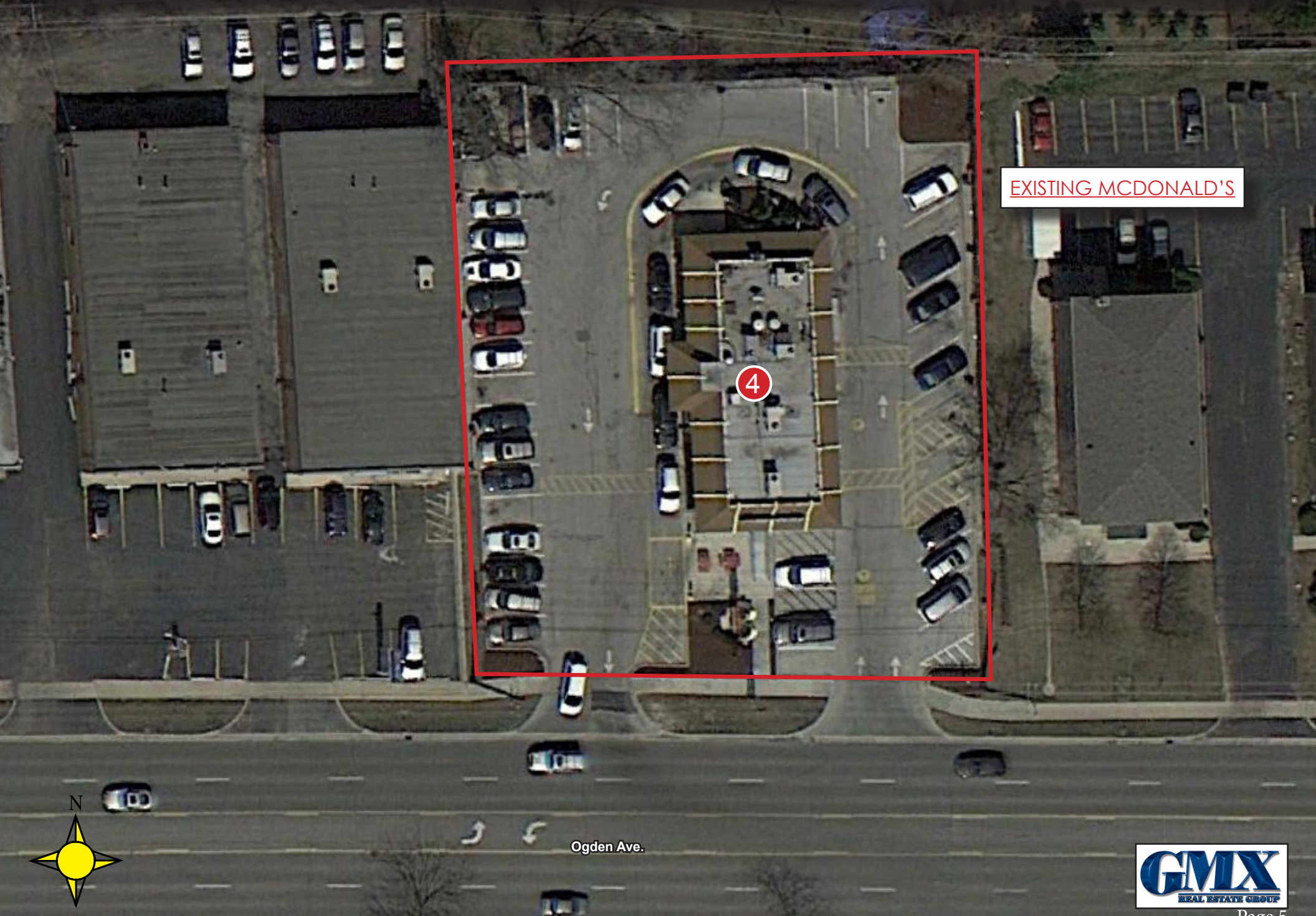
TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL

SEC OGDEN & BELMONT

Unreasonable owner of about one acre who is willing to ground lease for \$225,000 per year and is also pushing to share the site with McDonald's for a C-store plus gas pumps which is owner's main line of business.



TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL



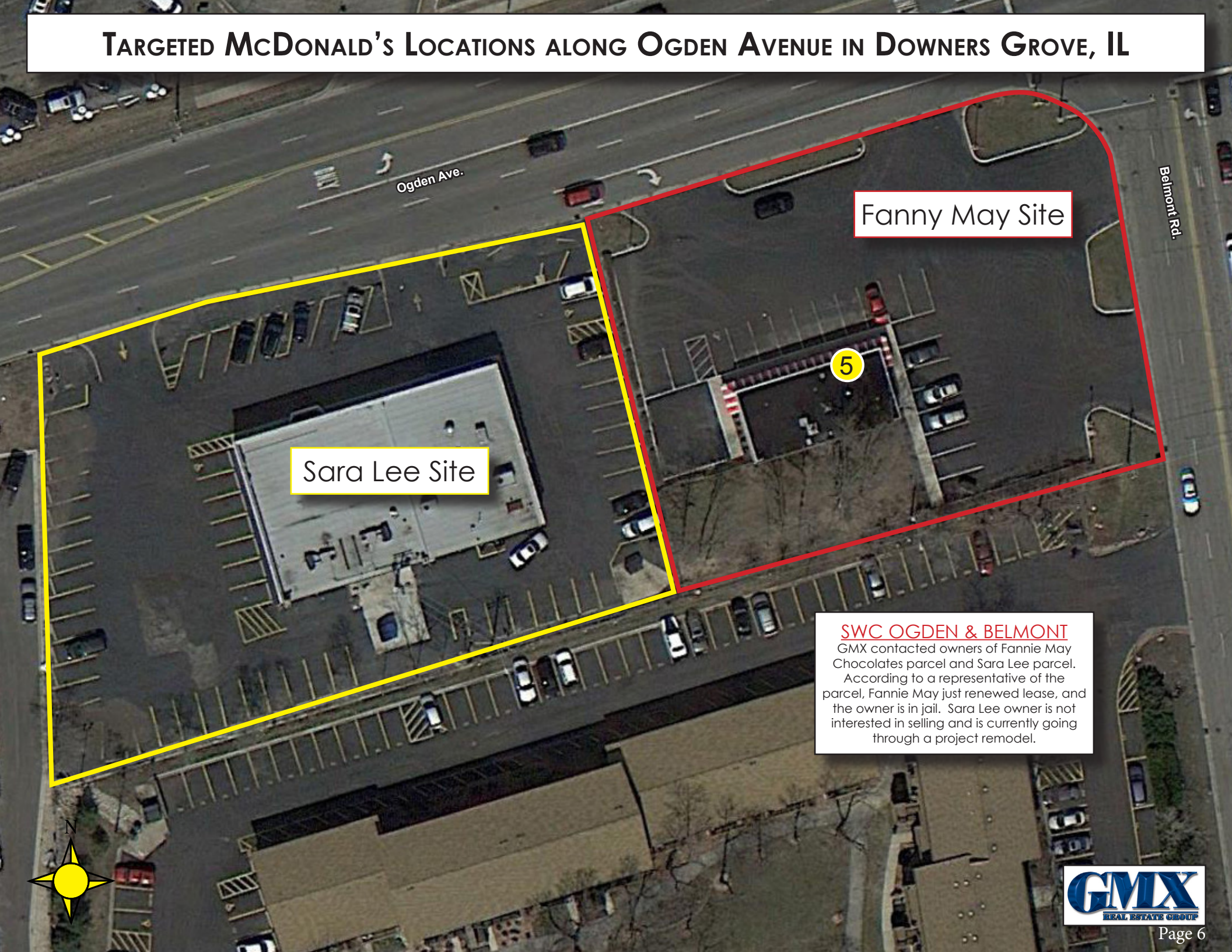
EXISTING MCDONALD'S

4

Ogden Ave.



TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL



Sara Lee Site

Fanny May Site

5

SWC OGDEN & BELMONT
GMX contacted owners of Fannie May Chocolates parcel and Sara Lee parcel. According to a representative of the parcel, Fannie May just renewed lease, and the owner is in jail. Sara Lee owner is not interested in selling and is currently going through a project remodel.



TARGETED MCDONALD'S LOCATIONS ALONG OGDEN AVENUE IN DOWNERS GROVE, IL

EXPANSION OF EXISTING LOCATION
GMX has spoken with the three adjacent property owners. All land owners are not willing to sell, as one of the owners currently operates a successful business on premises.



Ogden Ave.



MEMORANDUM TO: Dennis Somers
McDonald's Corporation

FROM: Javier Millan
Senior Consultant

Luay R. Aboona, PE
Principal

DATE: August 1, 2013

SUBJECT: Preliminary Traffic Evaluation
Proposed McDonald's Relocation
Downers Grove, Illinois

This memorandum summarizes a preliminary traffic evaluation conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for the proposed relocation of an existing McDonald's restaurant located at 1620 Ogden Avenue to 2535 Ogden Avenue in Downers Grove, Illinois.

The 2535 Ogden Avenue site is currently occupied by a vacant home and a detached garage. The plans call for razing the existing building and build a 4,388 square foot McDonald's restaurant with a dual ordering drive-through lane. Access will be provided on Ogden Avenue opposite an existing access drive serving the In Town Suites hotel.

The purpose of this study is to evaluate and consider the pros and cons of the proposed site as compared to the existing location. The locations will be compared to address the following:

- Adequacy of the access
- Ability to accommodate site traffic
- Proximity to signalized intersections
- Adequacy of sight lines and grade separation from street level
- On-site stacking and circulation
- Drive-through efficiency
- Traffic backups and ease of parking lot maneuverability

Table 1 presents a summary comparing the existing McDonald's location to the proposed McDonald's location. Further detail and analysis is provided in the evaluation section of this memorandum.

Table 1
McDONALD’S RESTAURANT LOCATION COMPARISON

| Evaluation Item | Advantage | |
|---|---------------------|---------------------|
| | Existing Restaurant | Proposed Restaurant |
| Accessibility and Ability to Accommodate Site Traffic | -- | X |
| Proximity to Signalized Intersection | -- | X |
| Adequacy of Sight Lines | -- | -- |
| Drive-Through Stacking and Circulation | -- | X |
| Traffic Backups | -- | -- |
| Internal Parking Lot Circulation and Traffic Flow | -- | X |
| Grade Separation/Distance to/from Ogden Avenue | -- | X |
| Ease of Parking Lot Maneuverability | -- | X |

Based on the summary results shown on Table 1, the proposed McDonald’s location scores considerably more favorably to that of the existing restaurant location based on the eight key traffic items under examination. Based on the results, the proposed restaurant location has an advantage over the existing location on six of the eight traffic items, as further discussed below.

Evaluation of Existing McDonald’s Restaurant (1620 Ogden Avenue)

Accessibility and Ability to Accommodate Site Traffic

The existing McDonald’s restaurant has two access drives (one inbound and one outbound) on Ogden Avenue. The inbound only access drive is offset to the west from the Bill Kay Nissan dealer by approximately 25 feet. The outbound only access drive provides two outbound lanes and is located approximately 80 feet west of the inbound only access drive.

The existence of a two-way left-turn lane on Ogden Avenue allows for vehicles that desire to turn left into the McDonald’s restaurant to do so without blocking through traffic. Conversely, the two-way left-turn lane affords exiting traffic the opportunity to perform a two-part left-turn maneuver.

Based on our field observations, the provision of two outbound lanes allows exiting traffic desiring to turn right onto Ogden Avenue to do so without queuing behind a vehicle that is waiting to perform a left-turn out maneuver.

Proximity to Signalized Intersections

The closest traffic signals to the existing McDonald's restaurant are at Saratoga Avenue to the east (approximately 2,900 feet) and at Finley Road/Belmont Road to the west (approximately 3,600 feet).

Based on our field observations, the long distances between the existing restaurant and the adjacent traffic signals (1/2 mile or more) reduces the availability of gaps in the through traffic stream as the arrival patterns of vehicles become random rather than platooned. This makes it difficult for exiting traffic to find adequate two-way gaps to perform a single left-turn out maneuver. Furthermore, given the long distances between the existing restaurant and the adjacent traffic signals, the prevailing speed of through traffic is the one posted or higher thus making it harder for vehicles to exit the restaurant. As such and based on our observations, the majority of the left-turn out maneuvers were forced to use the two-way left-turn lane to perform a two-part left-turn maneuver.

Adequacy of Sight Lines and Grade Separation from Street Level

Based on field inspection, the McDonald's restaurant is located at approximately the crest of a vertical curve on Ogden Avenue and as such it affords adequate sight lines for exiting vehicles to see oncoming traffic and vice-versa. However, there is a large site grade separation from Ogden Avenue to the McDonald's building that must be maneuvered through a challenging and vertically sloped parking field.

Drive-Through Stacking and On-Site Circulation

The existing site provides approximately four stacking spaces from the pick-up window and approximately six stacking spaces from the ordering board with the potential for an additional three spaces before reaching Ogden Avenue for a total stacking distance of nine vehicles from the ordering board. Although our observations during breakfast and dinner time periods did not show backups exceeding the provided stacking, our experience with similarly designed drive-through facilities at similarly located McDonalds has shown that the provided stacking is exceeded occasionally causing traffic to potentially back-up on Ogden Avenue. In addition, the following circulation issues were observed:

- The site does not have an exclusive by-pass lane for the drive-through facility given that if one vehicle parks on the parking spaces on the north side of the site, the "by-pass" lane will be blocked and cannot be used.
- Similarly, if a vehicle is parked in the north side, vehicles parked on the east side of the restaurant that want to exit will be blocked and will have to wait for the drive-through traffic to clear in order to circulate around the building and exit.
- There are six parking spaces within close proximity to the throat of the inbound only access drive. This creates potential conflicts with inbound traffic and vehicle backing out of these parking spaces.

Traffic Backups and Ease of Parking Lot/Maneuverability

No apparent backups were observed at the McDonald's access drive or on Ogden Avenue. However, it was observed that customers had difficulty maneuvering the tight parking lot and steep parking lot grades.

Evaluation of Proposed Site (2535 Ogden Avenue)

Accessibility and Ability to Accommodate Site Traffic

Based on a review of the proposed plan, the McDonald's restaurant will provide one access drive opposite the In Town Suite hotel access drive. The access drive will provide one inbound lane and two outbound lanes striped for an exclusive left-turn lane and an exclusive right-turn lane with outbound movements under stop sign control.

Similar to the existing site, the existence of a two-way left-turn lane on Ogden Avenue will allow for vehicles that desire to turn left into the McDonald's restaurant to do so without blocking through traffic. Conversely, the two-way left-turn lane affords exiting traffic the opportunity to perform a two-part left-turn maneuver.

The provision of two outbound lanes will allow exiting traffic desiring to turn right onto Ogden Avenue to do so without queuing behind a vehicle that is waiting to perform a left-turn out maneuver.

Proximity to Signalized Intersections

The closest traffic signals to the proposed site are the north leg of Cross Street to the east (approximately 400 feet) and the I-355 northbound on/off ramps (approximately 2,030 feet).

Based on our field observations, the short distances between traffic signals and the proposed access drive (0.38 miles or less) platoons traffic thus creating significantly more gaps in the through traffic stream than the existing site. It should be noted that eastbound traffic was observed to queue past the proposed access drive periodically during the P.M. peak hour. However, these queues always cleared with the green phase and numerous two-way gaps in the through traffic stream were observed. Therefore, left-turning traffic out of the proposed McDonald's restaurant will be able to exit with relative ease given the numerous two-way gaps and the two-way left-turn lane on Ogden Avenue. The maximum time a left-turning vehicle will have to wait for the queues to clear and gaps become available will be less than one minute.

Adequacy of Sight Lines and Grade Separation from Street Level

Based on field inspection, the proposed site has adequate sight distance to the east and west affording good sight lines for exiting vehicles to see oncoming traffic and vice-versa. The site also will sit at roughly grade level with Ogden Avenue, making site access easier and without parking lot grade separation challenges.

Drive-Through Stacking and On-Site Circulation

Based on a review of the site plan (dated June 6, 2013) prepared by Watermark Engineering Resources, Ltd., the dual ordering board facility has been designed to provide a storage length of six vehicles from the ordering boards with the possibility of accommodating an additional four vehicles before reaching Ogden Avenue thus providing a total of storage length of ten vehicles from the ordering boards. In addition stacking for approximately six vehicles from the pick-up window will be provided. Furthermore, seven to eight cars can stack between the pick-up window and Ogden Avenue. Based on KLOA, Inc. observations of the other dual ordering board McDonald restaurants and given that the proposed dual ordering board will improve the efficiency of the drive-through facility, speed up service, increase the available stacking from the ordering boards and the pick-up window, adequate stacking distance will be provided for the vehicles that will use the double ordering board ensuring traffic will not extend to Ogden Avenue.

As proposed, the site plan has been designed to provide maximum ingress/egress flexibility for the site with minimal pedestrian/vehicle conflicts via one-way traffic flow and one full ingress/egress access drive. Drive-through traffic will enter from Ogden Avenue and circulate counterclockwise around the restaurant. An exclusive by-pass lane will be provided on the south side of the site which will allow vehicles that do not want to wait in line at the drive-through facility or vehicles that are exiting the parking stalls to circulate and exit the site. This design will be a significant improvement over the existing McDonald's restaurant.

Traffic Backups and Ease of Parking Lot Maneuverability

As previously indicated, eastbound queues were periodically observed to back up past the proposed access drive during the P.M. peak hour. However, it was also observed that these queues cleared with every cycle length and did not preclude vehicles from entering/exiting the In Town Suites access drive opposite the proposed access drive. The proposed site will benefit from a larger parking facility and a near grade level parking lot.

Conclusion

Both sites (existing and proposed) provide adequate sight lines and are able to accommodate site traffic. However, based on a comparison of both sites, the proposed site will provide the following benefits that the existing site does not:

- Access drive will be lined up opposite an existing access drive eliminating offset access drives.
- Increased on site stacking for the drive-through facility (significant improvement over existing restaurant facility).
- Improved efficiency of the drive-through facility and speed up service (significant improvement over existing restaurant facility).
- Better internal circulation and traffic flow by providing an uninterrupted/exclusive by-pass lane (significant improvement over existing restaurant facility).
- Increased availability of two-way gaps in the through traffic stream on Ogden Avenue as a result of its proximity to adjacent traffic signals.
- Smoother grade separation/leveling between the proposed parking lot grade and Ogden Avenue.

MEMORANDUM TO: Dennis Somers
McDonald's Corporation

FROM: Javier Millan
Senior Consultant

Luay R. Aboona, PE
Principal

DATE: October 25, 2013

SUBJECT: McDonald's Restaurant
Downers Grove, Illinois

This memorandum presents the findings and recommendations of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for the proposed McDonald's restaurant to be located at 2635 Ogden Avenue in Downers Grove, Illinois. The proposed restaurant will replace an existing facility located at 1620 Ogden Avenue. The plans call for developing a 4,388 square-foot drive-through restaurant with dual ordering boards and 38 parking spaces. The drive-through pick-up window will be located on the east side of the building with dual ordering boards located on the southwest side. The site will be designed to allow vehicles to internally circulate counterclockwise around the building. Access to the restaurant is proposed to be provided on Ogden Avenue opposite the InTown Suites hotel access drive.

The purpose of this evaluation was to access the following.

- Impact of site traffic on area roadways
- Adequacy of the proposed access drive
- Proposed drive-through demand and stacking adequacy
- On-site circulation and efficiency

The following sections of this memorandum present the following.

- Existing site conditions
- A description of the proposed McDonald's restaurant
- Directional distribution of development-generated traffic
- Vehicle trip generation for the proposed McDonald's restaurant
- Traffic analysis for the weekday morning, midday and evening and Saturday midday peak hours for both the existing and future conditions
- Recommendations with respect to site access and internal circulation for the future conditions
- Evaluation of the proposed dual ordering board drive-through operation

Existing Conditions

In order to project future traffic conditions in the site area, three general components of existing conditions were considered: (1) the geographical location of the site and land uses in the area, (2) the characteristics of the roadways in the site vicinity and (3) traffic volumes on these roads.

Site Location

The site, which is currently occupied by a vacant home and detached garage, is located at 2635 Ogden Avenue in Downers Grove, Illinois. Land uses in the area include Culvers Restaurant and the InTown Suites hotel to the north, the Auto Extreme used car dealership to the east, an insurance agency to the west, and single-family homes to the south. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site area.

Existing Roadway System Characteristics

Ogden Avenue (U.S. Route 34) is an east-west arterial that is under the jurisdiction of the Illinois Department of Transportation (IDOT). Ogden Avenue provides two lanes in each direction with a two-way left turn lane (TWLTL) along the site's frontage. At its signalized intersection with Cross Street/Auto Extreme access drive, Ogden Avenue provides an exclusive left turn lane on its west approach. At its unsignalized intersection with the south leg of Cross Street, (approximately 180 feet west of Cross Street's north leg), westbound left-turn movements are prohibited via signage. Ogden Avenue has a posted speed limit of 40 mph and carries an average daily traffic (ADT) of approximately 27,500 vehicles.

Cross Street is a north-south local roadway. The north of approach of Cross Street, which is opposite the Auto Extreme access drive, provides an exclusive left-turn lane and a combined through/right-turn lane at its signalized intersection with Ogden Avenue. The Auto Extreme access drive provides one inbound lane and one outbound lane. The south leg of Cross Street is offset approximately 180 feet to the west and provides one inbound lane and one outbound lane with outbound movements under stop sign control. Cross Street has a posted speed limit of 25 mph and has a weight limit of six tons on the south approach.

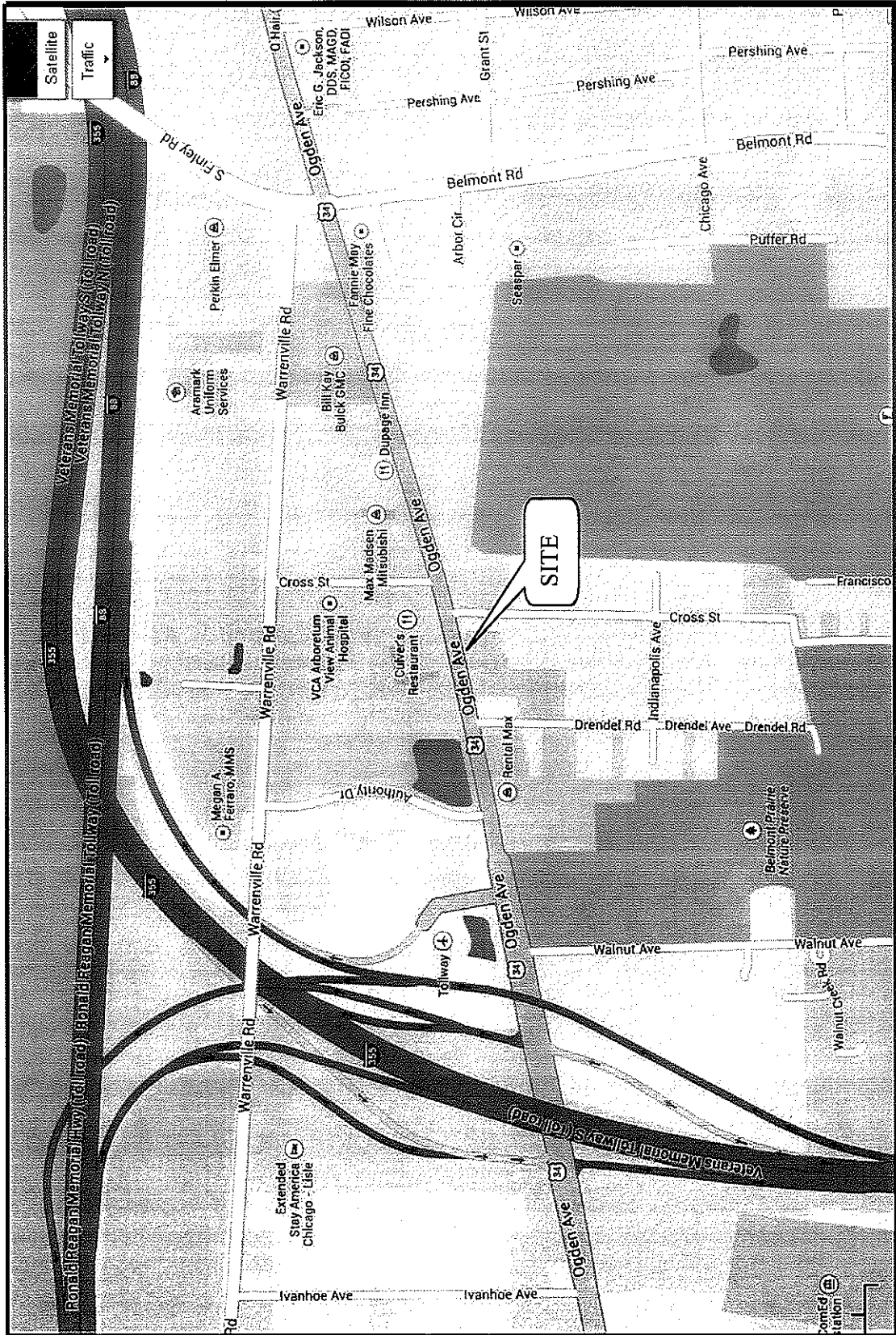
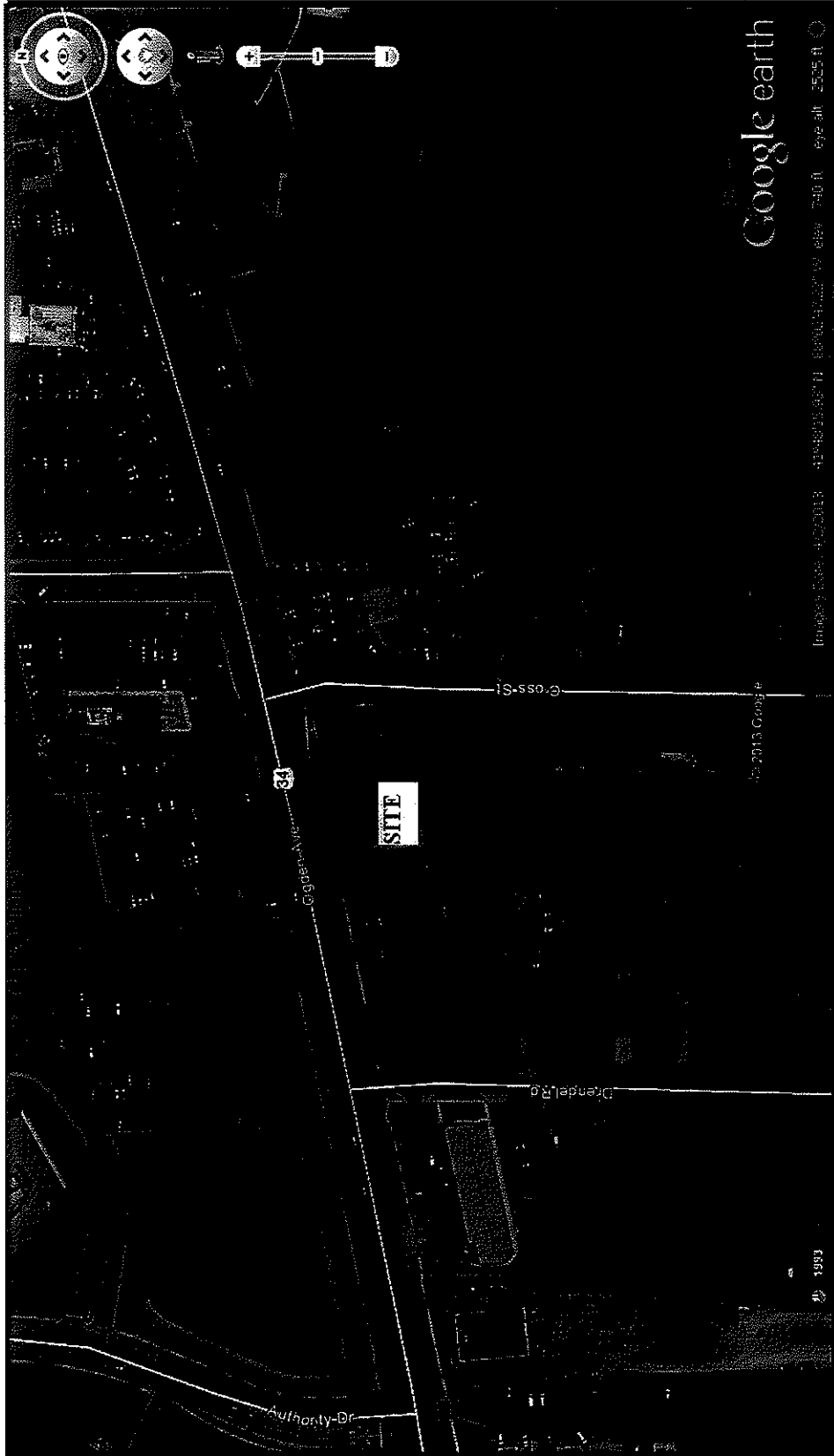


Figure 1

Site Location



Aerial View of Site

Figure 2

Existing Peak Hour Traffic Volumes

Manual vehicle traffic counts were conducted by KLOA, Inc. at the intersections of Ogden Avenue with:

- Cross Street/Auto Extreme access drive
- Cross Street 9south leg)
- Culver's right-in/right-out access drive
- InTown Suites hotel access drive

The counts were conducted on Tuesday, September 24, 2013 during the weekday morning (7:00 to 9:00 A.M.), midday (11:30 A.M. to 1:30 P.M.) and evening (4:00 to 6:00 P.M.) peak periods. Midday traffic counts were also conducted on Saturday, September 21, 2013 from 11:30 A.M. to 1:30 P.M.

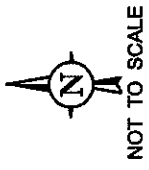
Summaries of the traffic counts indicate that the weekday morning peak hour occurs from 7:30 to 8:30 A.M., the midday from 12:30 A.M. to 1:30 P.M., the evening peak hour from 4:45 to 5:45 P.M. and the Saturday midday peak hour occurs from 12:30 to 1:30 P.M. **Figure 3** illustrates the existing peak hour traffic volumes.

Area Traffic Observations

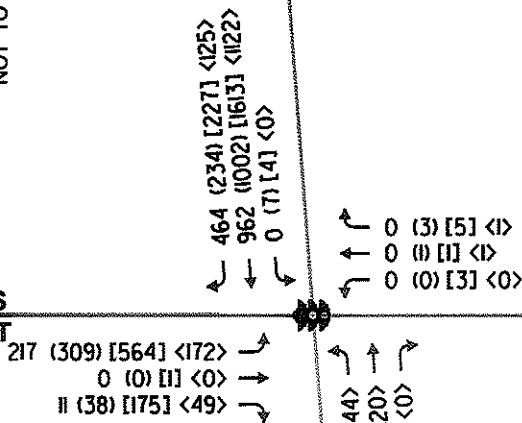
As part of the traffic counts, backups of eastbound traffic flow on Ogden Avenue were observed particularly any backups at its signalized intersection with Cross Street. These observations indicated the following.

- During the weekday morning and midday traffic counts, there were no observed backups in the eastbound direction that extended to the InTown Suites Access drive.
- During the weekday evening traffic counts, eastbound backups extended beyond the InTown Suites access drive periodically. However, the queues cleared with every cycle.
- During the Saturday midday traffic counts, eastbound backups also extended to or beyond the InTown Suites access drive periodically. However, these queues cleared with every cycle.

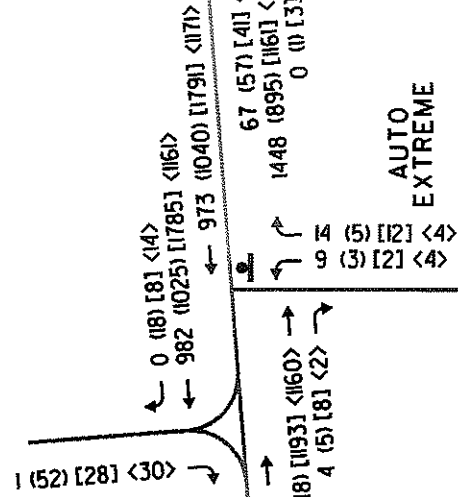
Furthermore, observations of outbound movements from the InTown Suites indicated that minimal queuing occurred with a maximum queue of two vehicles.



CROSS STREET



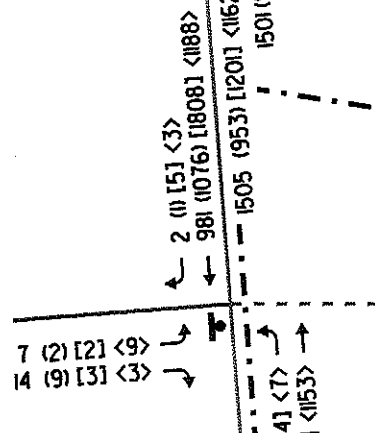
CULVERS



AUTO EXTREME

CROSS STREET

IN TOWN SUITES



SITE

LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - MIDDAY PEAK HOUR (12:30-1:30 PM)
- [00] - PM PEAK HOUR (4:45-5:45 PM)
- <00> - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)

KLOAN
 Job No: 13-124
 Figure: 3

TITLE:
Existing Traffic Volumes

PROJECT:
**Proposed McDonald's
 Drive Through Restaurant
 Downers Grove, Illinois**

Development Traffic Characteristics

Proposed Plans

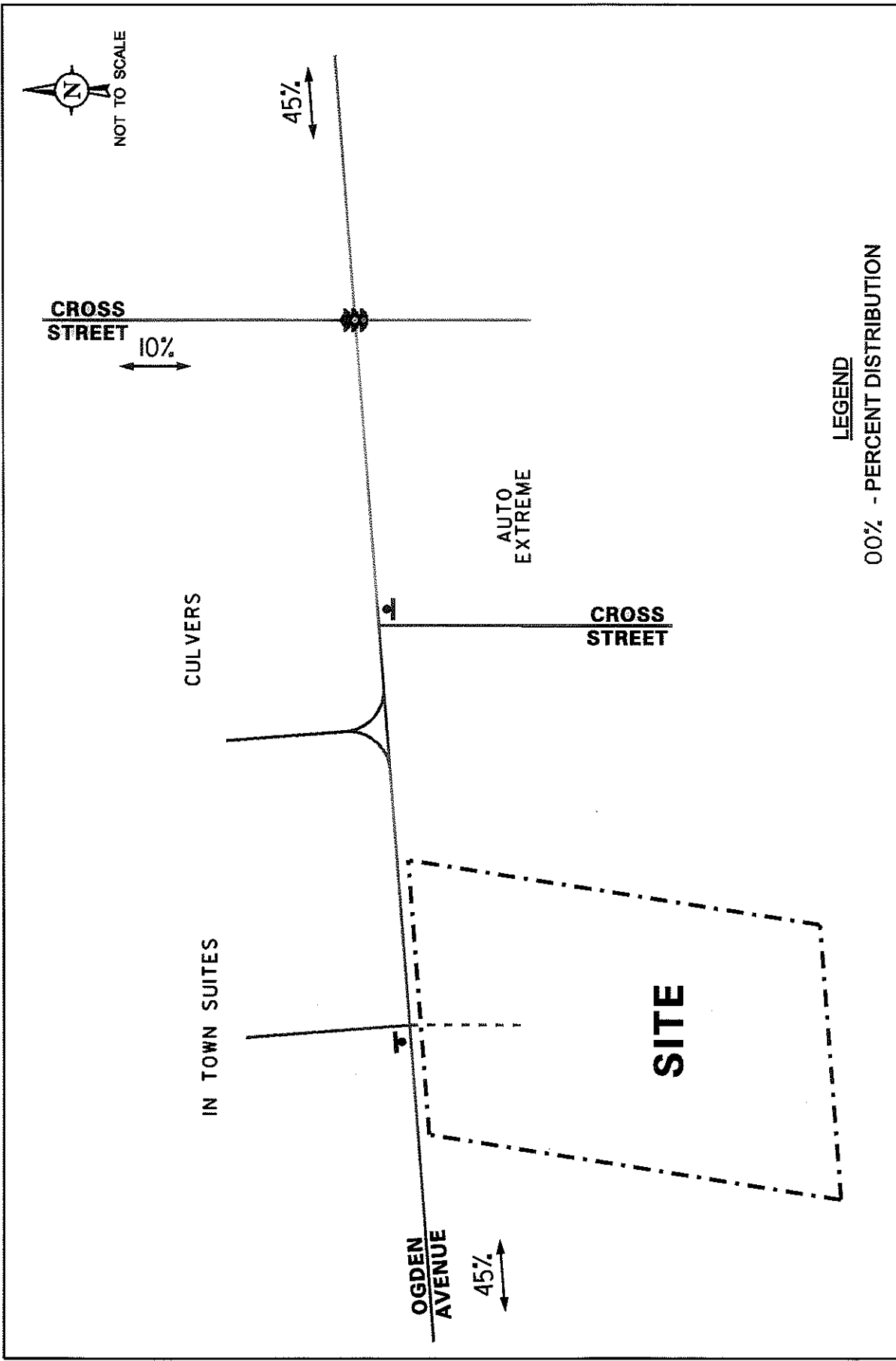
McDonald's is proposing to relocate its existing store to 2635 Ogden Avenue and build a 4,388 square-foot drive through restaurant with dual ordering boards to provide for a more efficient drive-through service, on-site circulation and access. This site will provide 38 parking spaces. The drive-through pick-up window will be located on the east side of the building with dual ordering boards located on the southwest side. The site will be designed to allow vehicles to internally circulate counterclockwise around the building. Access to restaurant is proposed to be provided on Ogden Avenue via a full ingress/egress access drive on Ogden Avenue opposite the InTown suites hotel access drive. The access drive will provide one inbound lane and two outbound lanes striped for a combined left/through lane and an exclusive right-turn lane with outbound movements under stop sign control. Inbound left-turn movements will be accommodated via the existing TWLTL on Ogden Avenue.

Directional Distribution

The directional distribution of the site-generated trips on the external roadway system is a function of several variables including the operational characteristics of the roadway system, and the ease with which the drivers can travel over various sections of the roadway system, with the least amount of peak hour congestion. The directions from which the customers of the development will approach and depart the site were estimated based on the existing travel patterns, as determined from the traffic counts. **Table 1** and **Figure 4** show the estimated directional distribution of traffic.

Table 1
DIRECTIONAL DISTRIBUTION

| Route | Percent |
|---------------------------------------|------------|
| To and from the east on Ogden Avenue | 45% |
| To and from the west on Ogden Avenue | 45% |
| To and from the north on Cross Street | <u>10%</u> |
| Total | 100% |



PROJECT: Proposed McDonald's Drive Through Restaurant Downers Grove, Illinois

TITLE: Estimated Directional Distribution

PROJECT: Proposed McDonald's Drive Through Restaurant Downers Grove, Illinois

Estimated Site Traffic Generation

The volume of traffic generated by a development is based on the type of land use and the size of the development. The estimates of peak hour vehicle trips to be generated by the proposed development was based on vehicle trip rates contained in the Trip Generation manual, 9th Edition, published by the Institute of Transportation Engineers (ITE). The "Fast-Food Restaurant with Drive-Through Window" (Land-Use Code 934) was used for the McDonald's restaurant. It should be noted that ITE Saturday midday rates were utilized to estimate the weekday midday trip generation given that ITE does not have trip generation rates for a fast food restaurant with a drive-through facility during the midday peak hour.

Surveys conducted by the Institute of Transportation Engineers (ITE) have shown that a considerable number of trips made to drive-through restaurants are diverted from existing passing traffic. This is particularly true during the weekday morning and evening peak hours when traffic is diverted from the home-to-work and work-to-home trips. Such diverted trips are referred to as pass-by traffic. These surveys indicate that approximately 50 percent of the peak hour trips generated by a McDonald's are diverted from existing traffic on the adjacent roads. **Table 2** shows the projected new trips to be generated by the proposed McDonald's relocation and the pass-by traffic.

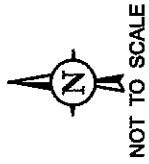
Table 3
PROJECTED SITE-GENERATED TRAFFIC VOLUMES

| ITE Land- Use Code | Land Use | Weekday Morning Peak Hour | | Weekday Midday Peak Hour* | | Weekday Evening Peak Hour | | Saturday Midday Peak Hour | |
|-----------------------|------------------------|---------------------------------|------------|---------------------------------|------------|---------------------------------|------------|---------------------------------|------------|
| | | In | Out | In | Out | In | Out | In | Out |
| 934 | McDonald's Restaurant | 102 | 98 | 132 | 128 | 74 | 70 | 132 | 128 |
| | Pass-By Trip Reduction | <u>-51</u> | <u>-51</u> | <u>-66</u> | <u>-66</u> | <u>-37</u> | <u>-37</u> | <u>-66</u> | <u>-66</u> |
| | Total New Trips | 51 | 47 | 66 | 62 | 37 | 33 | 66 | 62 |

*Trip rates based on ITE Saturday midday rates.

Traffic Assignment

The new trips to be generated by the proposed McDonald's restaurant relocation were assigned to the area roadway based on the directional distribution analysis (Table 1 and Figure 4) and the proposed access configuration. **Figure 5** illustrates the new trips site traffic assignment while **Figure 6** illustrates the pass-by traffic assignment.



CROSS STREET

CULVERS

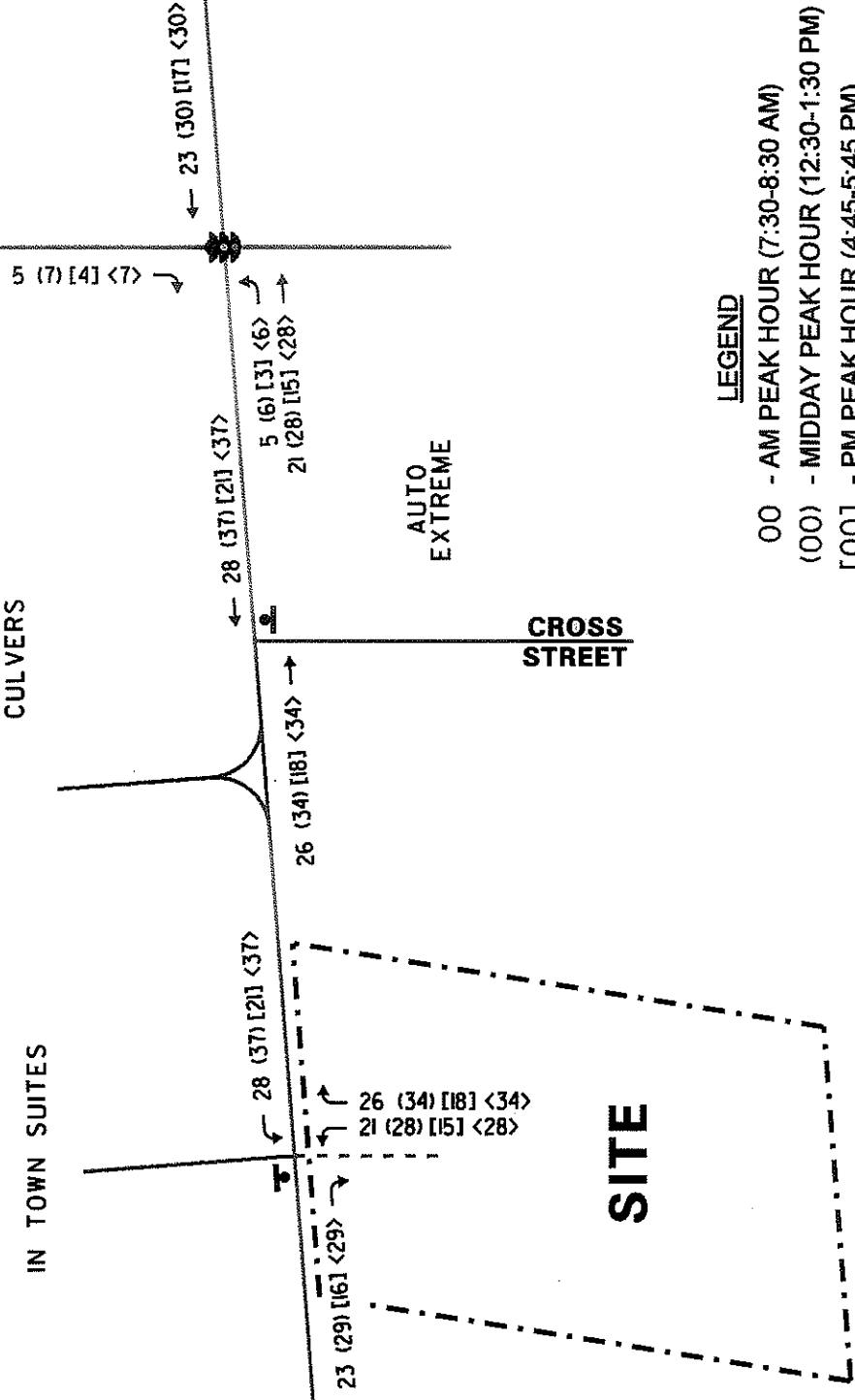
IN TOWN SUITES

OGDEN AVENUE

CROSS STREET

AUTO EXTREME

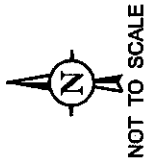
SITE



LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - MIDDAY PEAK HOUR (12:30-1:30 PM)
- [00] - PM PEAK HOUR (4:45-5:45 PM)
- <00> - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)

| | |
|---|--|
| <p>PROJECT: Proposed McDonald's Drive Through Restaurant Downers Grove, Illinois</p> | <p>TITLE: Estimated New Site Traffic Assignment</p> |
| <p>KLOAN Job No: 13-124 Figure: 5</p> | |



CROSS STREET

CULVERS

**AUTO
EXTREME**

CROSS STREET

IN TOWN SUITES

**OGDEN
AVENUE**

SITE

-20 (-35) [-22] <-33>
+20 (+35) [+22] <+33>

+31 (+31) [+15] <+33>
+20 (+35) [+22] <+33>

-31 (-31) [-15] <-33>
+31 (+31) [+15] <+33>

LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - MIDDAY PEAK HOUR (12:30-1:30 PM)
- [00] - PM PEAK HOUR (4:45-5:45 PM)
- <00> - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)



Job No: 13-124

Figure: 6

TITLE:

PROJECT:
Proposed McDonald's
Drive Through Restaurant
Downers Grove, Illinois

Estimated Pass-by Site Traffic Assignment

Projected Traffic Volumes

The estimated weekday morning, midday, evening, and Saturday midday peak hour traffic volumes that will be generated by the proposed development and the pass-by volumes were added to the existing traffic volumes. **Figure 7** illustrates the existing traffic volumes plus site traffic while **Figure 8** illustrates the Year 2019 future traffic volumes. Included in the future traffic volumes is the background growth on traffic assumed at 0.5 percent per year for six years. This is consistent with IDOT policies requiring the analysis of buildout year plus five.

Gap Analysis

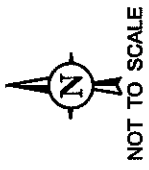
In order to determine the ease with which a vehicle can turn out of the driveway, a gap analysis was conducted to determine if adequate gaps or interruptions exist in the traffic stream on Ogden Avenue to allow for site traffic to enter and exit the proposed McDonald's restaurant access drive.

The gap study was conducted on Tuesday September 24 and October 8, 2013 during the weekday morning (7:00 to 9:00 A.M.), midday (11:30 A.M. to 1:30 P.M.) and evening (4:00 to 6:00 P.M.) peak periods and on Saturday September 21, 2013 during the midday (11:30 A.M. to 1:30 P.M.) peak period. The study measured the number and length of gaps in the eastbound direction on Ogden Avenue (allowing left turns in and right turns out) and in both directions on Ogden Avenue (allowing left turns out). The results of the gap study are summarized in **Table 3**.

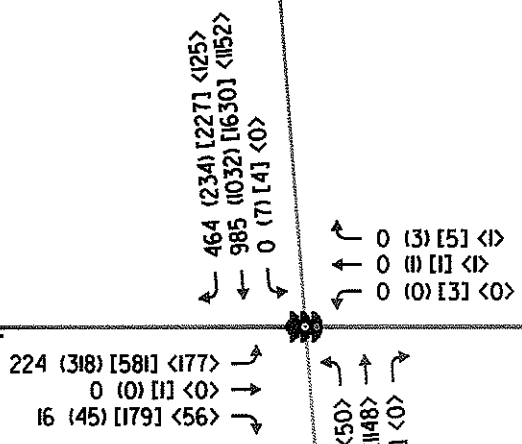
Table 3
GAP RESULTS SUMMARY

| Time Period | Left-Turn In | | Left-Turn Out | |
|------------------|----------------|---------------|----------------|---------------|
| | Available Gaps | Required Gaps | Available Gaps | Required Gaps |
| Weekday | | | | |
| A.M. Peak Hour | 237 | 48 | 55 | 41 |
| Midday Peak Hour | 471 | 72 | 182 | 63 |
| P.M. Peak Hour | 325 | 43 | 39 | 37 |
| Saturday | | | | |
| Midday Peak Hour | 381 | 70 | 74 | 61 |

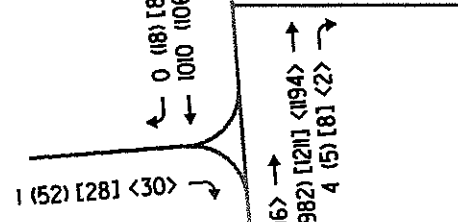
As can be seen from Table 3, there are adequate gaps needed to allow both the left turn in and out movements. During the critical A.M. peak hour, there are 35 percent more gaps than needed. Similarly, three times the amount of gaps needed during the weekday midday peak hour are available in the Ogden Avenue traffic stream.



CROSS STREET



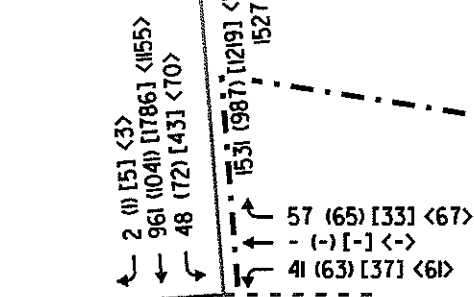
CULVERS



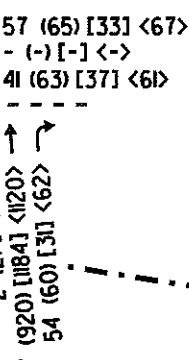
CROSS STREET

AUTO EXTREME

IN TOWN SUITES



OGDEN AVENUE



SITE

LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - MIDDAY PEAK HOUR (12:30-1:30 PM)
- [00] - PM PEAK HOUR (4:45-5:45 PM)
- <00> - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)

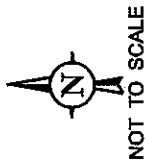
TITLE:

PROJECT:
Proposed McDonald's
Drive Through Restaurant
Downers Grove, Illinois

Existing Plus Site Traffic Volumes



Figure: 7



CROSS STREET

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 1014 (1062) [1678] <1185>
 0 (7) [4] <0>

224 (318) [581] <177>
 0 (0) [1] <0>
 16 (46) [184] <57>

1030 (108) [1859] <1232>
 1030 (108) [1865] <1242>

74 (65) [45] <51>
 1512 (950) [1211] <1182>
 0 (1) [3] <0>

14 (5) [12] <4>
 9 (3) [2] <4>

0 (3) [5] <0>
 0 (0) [1] <0>
 0 (0) [3] <0>

CULVERS

0 (18) [8] <14>
 1039 (1093) [1859] <1232>

1 (52) [28] <30>

1030 (108) [1859] <1232>
 1030 (108) [1865] <1242>

74 (65) [45] <51>
 1512 (950) [1211] <1182>
 0 (1) [3] <0>

14 (5) [12] <4>
 9 (3) [2] <4>

AUTO EXTREME

CROSS STREET

IN TOWN SUITES

2 (1) [5] <3>
 990 (1072) [1839] <1189>
 48 (72) [43] <70>

1572 (1011) [1247] <1229>
 4 (5) [8] <2>

57 (65) [33] <67>
 (-) [-] <->
 41 (63) [37] <61>

OGDEN AVENUE

7 (2) [2] <9>
 (-) [-] <->
 14 (9) [3] <3>

2 (2) [4] <7>
 1512 (949) [120] <155>
 54 (60) [31] <62>

SITE

LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - MIDDAY PEAK HOUR (12:30-1:30 PM)
- [00] - PM PEAK HOUR (4:45-5:45 PM)
- <00> - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)



PROJECT: **Proposed McDonald's Drive Through Restaurant Downers Grove, Illinois**

TITLE: **Projected Total Traffic Volumes**

It should be noted that the gap analysis assumed that vehicles will make the left-turn out in one single maneuver. Given that Ogden Avenue is separated by a TWLTL, it will allow exiting vehicles the opportunity to make a two part left-turn maneuver and as such more vehicles than what the gap study indicated will be able to exit the site.

Traffic Analysis

Capacity analyses using the Synchro/SimTraffic 8 capacity analysis software were conducted at the following intersections to determine the impact of added traffic at these locations.

- Ogden Avenue with Cross Street/Auto Extreme access drive
- Ogden Avenue with Cross Street/Culver's right-in/right-out access drive
- Ogden Avenue with InTown suites access drive/proposed access drive

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter grade from A to F based on the average control delay experienced by vehicles passing through the intersection. Level of Service A is the highest grade (best traffic flow and least delay), Level of Service E represents saturated or at capacity conditions and Level of Service F is the lowest grade (oversaturated conditions, extensive delays). The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for unsignalized intersections is shown in the Appendix. The results of the capacity analysis for existing, existing plus site and future conditions are summarized in **Tables 4 through 6**.

Table 4
CAPACITY ANALYSIS—EXISTING CONDITIONS

| Location | Weekday | | | | | | Saturday | |
|---|-----------|-------|-------------|-------|-----------|-------|-------------|-------|
| | A.M. Peak | | Midday Peak | | P.M. Peak | | Midday Peak | |
| | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay |
| Ogden Avenue and Cross Street/Auto Extreme ¹ | B | 17.8 | C | 22.9 | E | 74.7 | B | 19.0 |
| Ogden Avenue and Cross Street/Culvers R-I/R-O | | | | | | | | |
| • North Approach | A | 9.3 | B | 10.2 | B | 12.7 | A | 10.0 |
| • South Approach | D | 26.8 | B | 14.9 | C | 15.7 | C | 19.3 |
| Ogden Avenue and InTown Suites access drive | B | 12.0 | B | 10.6 | C | 16.7 | B | 14.9 |

1 - Signalized Intersection
LOS - Level of Service
Delay is measured in seconds.

Table 5
CAPACITY ANALYSIS—EXISTING TRAFFIC PLUS SITE

| Location | Weekday | | | | | | Saturday | |
|---|-----------|-------|-------------|-------|-----------|-------|-------------|-------|
| | A.M. Peak | | Midday Peak | | P.M. Peak | | Midday Peak | |
| | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay |
| Ogden Avenue and Cross Street/Auto Extreme ¹ | B | 17.9 | C | 23.5 | E | 76.3 | B | 19.5 |
| Ogden Avenue and Cross Street/Culvers R-I/R-O | | | | | | | | |
| • North Approach | A | 9.3 | B | 10.3 | B | 12.7 | B | 10.1 |
| • South Approach | D | 27.7 | C | 15.3 | C | 15.9 | B | 19.9 |
| Ogden Avenue and InTown Suites/McDonald's drive | | | | | | | | |
| • North Approach | C | 15.2 | B | 11.8 | C | 20.9 | C | 21.9 |
| • South Approach | D | 34.7 | C | 19.3 | D | 25.1 | D | 25.3 |

¹ - Signalized Intersections
LOS - Level of Service
Delay is measured in seconds.

Table 6
CAPACITY ANALYSIS—YEAR 2019 FUTURE CONDITIONS

| Location | Weekday | | | | | | Saturday | |
|---|-----------|-------|-------------|-------|-----------|-------|-------------|-------|
| | A.M. Peak | | Midday Peak | | P.M. Peak | | Midday Peak | |
| | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay |
| Ogden Avenue and Cross Street/Auto Extreme ¹ | B | 19.0 | C | 24.8 | F | 84.9 | C | 21.2 |
| Ogden Avenue and Cross Street/Culvers R-I/R-O | | | | | | | | |
| • North Approach | A | 9.4 | B | 10.5 | B | 12.7 | B | 10.3 |
| • South Approach | D | 29.2 | C | 15.7 | C | 16.3 | C | 20.6 |
| Ogden Avenue and InTown Suites/McDonald's drive | | | | | | | | |
| • North Approach | C | 15.6 | B | 11.9 | C | 22.4 | C | 22.1 |
| • South Approach | E | 38.0 | C | 20.0 | D | 25.8 | D | 26.8 |

1 - Signalized Intersections
LOS - Level of Service
Delay is measured in seconds.

Traffic Evaluation

Ogden Avenue and Cross Street/Auto Extreme Access Drive

The results of the capacity analyses indicate that the intersection of Ogden Avenue with Cross Street/Auto Extreme access drive is operating at acceptable levels of service except during the weekday evening peak hour. This is due to heavy southbound to eastbound left-turn movement and the short amount of green time allocated to this movement. As previously indicated and based on our observations, eastbound traffic on Ogden Avenue periodically backed up to or beyond the InTown suites access drive during the weekday evening and Saturday midday peak hours. However, these queues cleared with every cycle length as verified by the results of the capacity analyses which show that this movement operates at a level of service B or better.

Under the existing traffic plus site conditions, the intersection will continue to operate at the same levels of service with an increase in the overall delay of 1.6 seconds or less. This indicates that the McDonald's site generated traffic will have a limited impact on the intersection's operations.

Under the Year 2019 future traffic conditions, including the background growth of 3.5 percent, the intersection will continue to operate at acceptable levels of service except during the weekday evening peak hour where the overall level of service drops from an "E" to an "F". This is due to the assumed background growth and the long delays the southbound to eastbound left-turn movement will continue to experience. Further inspection of the capacity analyses indicate that the eastbound movements on Ogden Avenue will continue to operate at a level of service B or better during all studied peak hours. It should be noted that the proposed McDonald's restaurant will not have a significant impact on this intersection as 50 percent of the traffic generated by the McDonald's restaurant is already in the traffic stream (pass-by traffic) and given that the development will not be adding any traffic to the southbound to eastbound left turn movement. In fact, the new site traffic volumes will amount to only one percent of the future traffic volumes thus having a very limited impact on this intersection.

Ogden Avenue and Cross Street/Culvers Right-In/Right-Out Access Drive

The results of the capacity analysis indicate that the outbound movements from the south leg of Cross Street as well as the right-turn movement out of the Culvers right-in/right-out access drive currently operate at acceptable levels of service and will continue to do so in the future indicating that the proposed development will have a limited impact on traffic conditions at this intersection.

Ogden Avenue and InTown Suites/McDonald's Access Drive

The intersection of Ogden Avenue with the InTown Suites access drive operates at acceptable levels of service under all four study periods. Under the existing plus site conditions, the north and south approaches will continue to operate at acceptable levels of service.

Under Year 2019 future conditions, the outbound movements from the InTown suites access drive will continue to operate at acceptable levels of service. The McDonald's access drive will operate at acceptable levels of service except during the morning peak hour where a level of service E will be experienced by the exiting movements. However, as discussed earlier, the conducted gap studies indicated that there are adequate gaps in Ogden Avenue's traffic stream to allow for left turns in and out to occur. These additional movements will be accommodated by the available gaps which will allow the left turning movements to occur efficiently and without significant increase in average delays. Further inspection of the capacity analyses indicate that queuing of exiting vehicles will not be significant (50 feet or less) which can be accommodated within the site without creating internal conflicts or backups. Similarly, left turning movements into the site will operate at a level of service C or better with projected queues of 25 feet. As such, the McDonald's site traffic will have a limited impact on the operation of the intersection and it will continue operating in a similar manner in the future with the additional traffic that will be generated by the McDonald's restaurant.

On-Site Circulation and Design

As proposed, customer traffic will enter the site from Ogden Avenue and circulate counterclockwise around the restaurant with vehicles exiting the site on Ogden Avenue thus limiting the number of curb cuts on Ogden Avenue to one.

Based on a review of the site plan (dated October 18, 2013) prepared by Watermark Engineering Resources, Ltd., the dual ordering board facility has been designed to provide a storage length of six vehicles from the ordering boards with the possibility of accommodating an additional four vehicles on-site before reaching Ogden Avenue thus providing a total storage length of ten vehicles from the ordering boards. In addition, stacking for approximately seven vehicles from the pick-up window will be provided for a total stacking of 13 vehicles. Based on KLOA, Inc. observations of the other dual ordering board McDonald restaurants and given that the proposed dual ordering board will improve the efficiency of the drive-through facility, speed up service, increase the available stacking from the ordering boards and the pick-up window, stacking for six to eight vehicles was found to be sufficient. Therefore, adequate stacking is being provided for the vehicles that will use the double ordering board ensuring traffic will not extend to Ogden Avenue.

The site plan has been designed to provide maximum ingress/egress flexibility for the site with minimal pedestrian/vehicle conflicts via one-way traffic flow and one full ingress/egress access drive. As previously indicated, drive-through traffic will enter from Ogden Avenue and circulate counterclockwise around the restaurant. An exclusive by-pass lane will be provided on the south side of the site which will allow vehicles that do not want to wait in line at the drive-through facility or vehicles that are exiting the parking stalls to circulate and exit the site.

Conclusion

Based on the preceding evaluation, the following are our findings and conclusions.

- The proposed McDonald's restaurant will not generate a significant amount of new trips
- The additional traffic that will be generated by the proposed McDonald's restaurant can be accommodated by the area roadway system.
- The proposed access system will be adequate in accommodating site traffic.
- The results of the gap study have shown that there are adequate gaps in the through traffic stream along Ogden Avenue to allow projected McDonald's traffic to enter and exit the site efficiently.
- The proposed dual ordering board will enhance the efficiency of the drive-through facility, speed up the service and provide adequate stacking.

Appendix

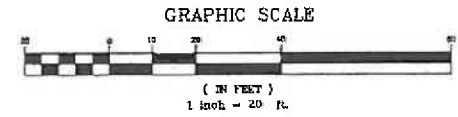
LEVEL OF SERVICE CRITERIA

| Signalized Intersections | | |
|---------------------------------|--|---|
| Level of Service | Interpretation | Average Control Delay (seconds per vehicle) |
| A | Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping. | ≤10 |
| B | Good progression, with more vehicles stopping than for Level of Service A. | >10 - 20 |
| C | Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping. | >20 - 35 |
| D | The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable. | >35 - 55 |
| E | Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent. | >55 - 80 |
| F | The volume-to-capacity ratio is very high, progression is very poor and the cycle length is long. Most cycles fail to clear the queue. | >80.0 |

| Unsignalized Intersections | |
|-----------------------------------|-------------------------------|
| Level of Service | Average Total Delay (SEC/VEH) |
| A | 0 - 10 |
| B | > 10 - 15 |
| C | > 15 - 25 |
| D | > 25 - 35 |
| E | > 35 - 50 |
| F | > 50 |

Source: *Highway Capacity Manual*, 2010.

ALTA/ACSM LAND TITLE AND TOPOGRAPHIC SURVEY



LEGAL DESCRIPTION

LOTS 2, 3, 4, AND 5 IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S BELMONT GOLF ADDITION, BEING A SUBDIVISION IN THE SOUTHWEST 1/4 OF SECTION 1 AND IN THE NORTHWEST 1/4 OF SECTION 12, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED SEPTEMBER 14, 1925 AS DOCUMENT 198614, IN DUPAGE COUNTY, ILLINOIS.

FOLLOWING IS A NARRATIVE DESCRIPTION OF THE PROPERTY BASED ON MEASURED VALUES:

LOTS 2, 3, 4, AND 5 IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S BELMONT GOLF ADDITION, BEING A SUBDIVISION IN THE SOUTHWEST QUARTER OF SECTION 1 AND IN THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED SEPTEMBER 14, 1925 AS DOCUMENT 198614, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 2; THENCE SOUTH 00 DEGREES 54 MINUTES 33 SECONDS WEST ALONG THE EAST LINE OF SAID LOT 2, A DISTANCE OF 198.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 2; THENCE SOUTH 77 DEGREES 04 MINUTES 13 SECONDS WEST ALONG THE SOUTHERLY LINES OF SAID LOTS 2 AND 3, A DISTANCE OF 89.57 FEET TO THE SOUTHWEST CORNER OF SAID LOT 3; BEING ALSO THE SOUTHWEST CORNER OF SAID LOT 4; THENCE SOUTH 78 DEGREES 23 MINUTES 02 SECONDS WEST ALONG THE SOUTHERLY LINES OF SAID LOTS 4 AND 5, A DISTANCE OF 99.84 FEET TO THE SOUTHWEST CORNER OF SAID LOT 5; THENCE NORTH 00 DEGREES 47 MINUTES 37 SECONDS EAST ALONG THE WEST LINE OF SAID LOT 5, A DISTANCE OF 200.10 FEET TO THE NORTHWEST CORNER OF SAID LOT 5; THENCE NORTH 78 DEGREES 24 MINUTES 21 SECONDS EAST ALONG THE NORTHERLY LINES OF LOTS 5 AND 4, A DISTANCE OF 97.52 FEET TO A POINT OF CURVATURE; THENCE NORTHEASTERLY ALONG THE NORTHERLY LINES OF SAID LOTS 4, 3 AND 2, BEING A CURVE CONCAVE TO THE NORTH HAVING A RADIUS OF 5778.70 FEET, A CHORD BEARING OF NORTH 77 DEGREES 54 MINUTES 02 SECONDS EAST, A CHORD LENGTH OF 101.95, A DELTA ANGLE OF 01 DEGREE 00 MINUTES 38 SECONDS, AN ARC LENGTH OF 101.95 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

CORRIDORS 1 DOWNER'S GROVE A.P. DOCUMENT #R1998-096417



VICINITY MAP NO SCALE

- ### LEGEND
- FOUND 1/8" O.D.I.P. UNLESS OTHERWISE NOTED (FIELD LOCATION) (CONTROL POINT)
 - CONCRETE MONUMENT
 - CROSS IN CONCRETE
 - STORM SEWER
 - STORM SEWER
 - WALK VAULT
 - FIRE HYDRANT
 - FLARED END SECTION
 - UTILITY POLE
 - OVERHEAD TRAFFIC SIGNAL
 - TRAFFIC SIGNAL HANDBOOK
 - OVERHEAD WIRE
 - GAS METER
 - ELECTRIC METER
 - TRANSFORMER PAD
 - TELEPHONE POCASAL
 - ELECTRIC POCASAL
 - TELEPHONE HANDBOOK
 - CABLE TELEVISION POCASAL
 - ELECTRIC HANDBOOK
 - VALVE BOX
 - BRICK
 - SOIL
 - RELAND POLE
 - LIGHT
 - LIGHT POLE
 - MAILBOX
 - GAS METER
 - ELECTRIC MARKER
 - TELEPHONE MARKER
 - WATER MARKER
 - GAS VALVE
 - CONCRETE TREE W/ APPROX. DIAMETER
 - SELENEOUS TREE W/ APPROX. DIAMETER
 - WATER SYSTEM
 - WATER SYSTEM (ONE LINE DRAWING IS APPROXIMATE)
 - ELEVATION
 - CONTROL POINT
 - STIMULOUS PAVEMENT
 - CONCRETE SURFACE
 - GRAVEL SURFACE
 - LANDSCAPE AREA
 - STONE SURFACE
 - VEGETABLE FACILE
 - WOOD FENCE
 - CHAIN LINK FENCE
 - SPECIAL GUARDRAIL
 - DIAGRAMMED TRAFFIC SIGNAL
- ### ABBREVIATIONS
- O.D.I.P. = OUTSIDE DIAMETER FOR IRON PIPE
 TF = TOP OF FOUNDATION
 FF = FINISHED FLOOR
 FES = FLARED END SECTION
 VCP = VITRIFIED CLAY PIPE
 DRP = DUCTILE IRON PIPE
 PVC = POLYVINYL CHLORIDE
 RCP = REINFORCED CONCRETE PIPE
 CMP = CORRUGATED METAL PIPE
 (M) = RECORD BEARING OR DISTANCE
 (W) = MEASURED BEARING OR DISTANCE
 (D) = DEED BEARING OR DISTANCE
 A = ARC LENGTH
 R = RADIUS
 CH = CHORD
 CB = CHORD BEARING
 B.S.L. = BUILDING SETBACK LINE
 U.L. = UTILITY EASEMENT
 C.E. = CENTERLINE
 P.U.C. = PUBLIC UTILITY EASEMENT
 P.O.C. = POINT OF COMMENCEMENT
 P.O.B. = POINT OF BEGINNING
 P.V. & D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT
 B.O. = BACK OF CURB
 B.O.C. = BACK OF CURB
 F.L. = FLOW LINE
 C. = CONCRETE
 P. = PAVEMENT
 G. = GRAVEL
 E.M. = EDGE OF WALK
 T.W. = TOP OF WALL
 S. = TOP OF PIPE
 S.E. = INVERT ELEVATION
 R.W. = RETAINING WALL
 P.L. = PROPERTY LINE
 D.M. = DOWNHILL
 S.F. = SQUARE FEET

SCHEDULE B EXCEPTIONS

RESTRICIONS CONTAINED IN THE CERTIFICATE APPENDED TO THE PLAT OF ARTHUR T. MCINTOSH AND CO'S BELMONT GOLF ADDITION, AFORESAID, AS FOLLOWS:

THERE IS HEREBY ESTABLISHED A BUILDING LINE 15 FEET BACK FROM THE FRONT LINE OF ALL LOTS FACING COHEN AVENUE AND A BUILDING LINE 40 FEET BACK FROM THE FRONT LINE OF ALL OTHER LOTS IN THIS SUBDIVISION AND NO BUILDING NOR ANY PARTY THEREOF SHALL BE ERRECTED OR MAINTAINED BETWEEN SAID BUILDING LINE AND THE STREET.

NOTE: SAID INSTRUMENT CONTAINS NO PROVISION FOR A FORFEITURE OF OR REVERSION OF TITLE IN CASE OF BREACH OF CONDITION.

RESTRICTIONS (BUT OMITTING ANY SUCH COVENANT OR RESTRICTION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS OR NATIONAL ORIGIN UNLESS AND ONLY TO THE EXTENT THAT SAID COVENANT (A) IS EXEMPT UNDER CHAPTER 42 SECTION 30.07 OF THE UNITED STATES CODE OR (B) RELATES TO HANDICAP BUT DOES NOT DISCRIMINATE AGAINST HANDICAPPED PERSONS), CONTAINED IN DEED RECORDED DECEMBER 18, 1935 AS DOCUMENT NUMBER 395055, CONCERNING THE LAND, AND RELATING TO THE LOCATION OF BUILDINGS TO BE ERRECTED ON THE LAND.

TERMS, PROVISIONS AND CONDITIONS CONTAINED IN A PERMANENT EASEMENT AGREEMENT BY AND BETWEEN MICHAEL LOCOCCO AND THE DOWNERS GROVE SANITARY DISTRICT RECORDED SEPTEMBER 18, 2008 AS DOCUMENT R2008-141732.

NOT PLOTTABLE

SCHEDULE B EXCEPTIONS A, B, C, X, Y, AD, Z, AA, AF, AB, AE, AG, AH, J, E, F, G, H, L, K, M, O, S, T, U AND V ARE NOT SURVEY RELATED AND THEREFORE NOT SHOWN.

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEYS AND EXISTING DRAWINGS, MAPS AND RECORDS SUPPLIED TO SURVEYOR BY THE SURVEYOR'S CLIENT. SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN CONFORM TO ALL SUCH UTILITIES IN THE AREA. CLIENT IN SERVICE OF UNDERGROUND UTILITIES SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL SUCH UTILITIES IN THE EXACT LOCATION SHOWN. SURVEYOR HAS PHYSICALLY LOOKED TO VERIFY STRUCTURES, UTILITY AND HAS NOT PHYSICALLY LOCATED THE UNDERGROUND LINES.

ZONING INFORMATION

THE FOLLOWING INFORMATION WAS OBTAINED BY THE SURVEYOR FROM THE VILLAGE OF DOWNER'S GROVE WEBSITE:

THE SITE IS ZONED RS - GENERAL SERVICES AND HIGHWAY BUSINESS DISTRICT.

NO MINIMUM LOT AREA.
 NO MINIMUM LOT WIDTH.
 MAXIMUM BUILDING HEIGHT SHALL BE SIXTY (60) FEET, INCLUDING PARAPETS, TILES AND MECHANICAL FLOORS.
 NO MAXIMUM LOT COVERAGE.
 NO MINIMUM FLOOR AREA.
 THE MAXIMUM FLOOR AREA RATIO OF BUILDINGS ON A ZONING LOT SHALL NOT EXCEED 75%.
 THE MINIMUM FRONT YARD SETBACK SHALL BE NOT LESS THAN TWENTY-FIVE (25) FEET, EXCEPT THAT BUILDINGS OVER TWENTY (20) FEET IN HEIGHT SHALL BE SET BACK ONE (1) ADDITIONAL FOOT FOR EACH TWO (2) FEET OF HEIGHT OVER TWENTY (20) FEET. THE SETBACK REQUIREMENTS SHALL BE OBSERVED ON EACH STREET SIDE OF A CORNER AND/OR MULTI-FRONTAGE LOT, WHERE A ZONING LOT FRONTS COHEN AVENUE AND ALSO FRONTS ON STREET INTERSECTING WITH COHEN AVENUE, THE MINIMUM SETBACK FOR PARKING OR DISPLAY OF MOTOR VEHICLES ALONG THE INTERSECTING STREET SHALL BE NOT LESS THAN EIGHT (8) FEET.
 NO SIDE YARD SETBACK.
 A REAR YARD SHALL BE PROVIDED FOR RESIDENTIAL USES LOCATED ABOVE THE FIRST FLOOR. SUCH YARD TO BE NOT LESS THAN THIRTY (30) FEET IN DEPTH AND TO BEGIN AT A LEVEL NO HIGHER THAN THAT OF THE FINISHED FLOOR OF THE LOWEST RESIDENTIAL UNIT.
 THERE ARE NO STRIPED PARKING SPACES ON THIS SITE.

THE SURVEYOR CONTACTED THE VILLAGE OF DOWNER'S GROVE COMMUNITY DEVELOPMENT DEPARTMENT ON OCTOBER 7, 2013 AND ACCORDING TO CHRIS LABRINO, A PLAT OF SUBDIVISION WILL BE REQUIRED FOR THIS PROPERTY.

IMPROVEMENTS CROSSING PROPERTY LINES

(A) RETAINING WALL CROSSES LINE 0.72' WEST
 (B) FENCE END 0.10' EAST OF LINE

SURVEYOR'S STATEMENT

I, SCOTT C. KRER, A DELAWARE CORPORATION, A DELAWARE LIMITED LIABILITY COMPANY, MCDONALD'S REAL ESTATE COMPANY, A DELAWARE CORPORATION, FRANCHISE RELIANCE INVESTMENT TRUST - A MARYLAND CORPORATION, AND CHICAGO TITLE INSURANCE COMPANY

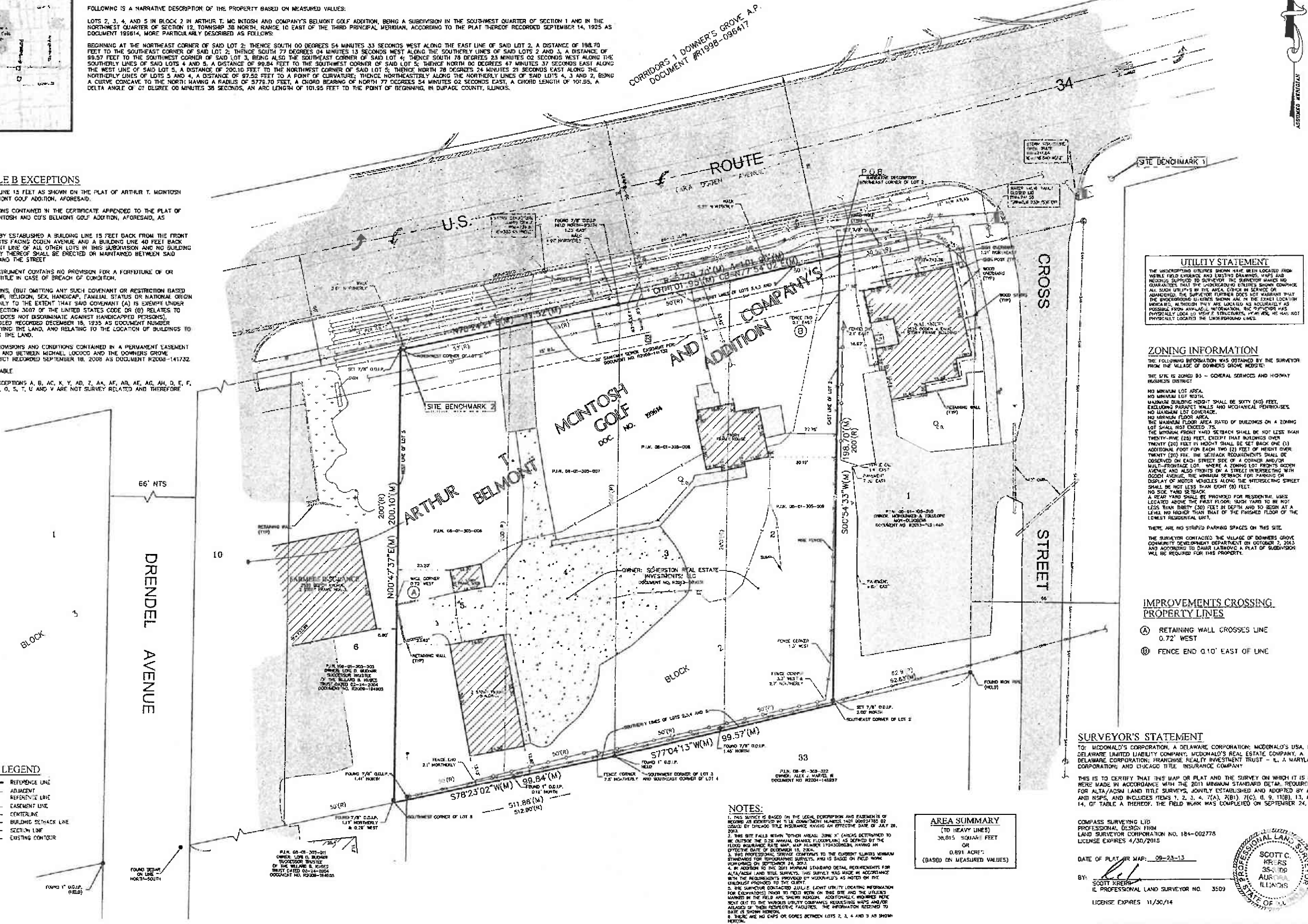
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 7, 8, 11, 12, 13, AND 14, OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON SEPTEMBER 24, 2013.

AREA SUMMARY

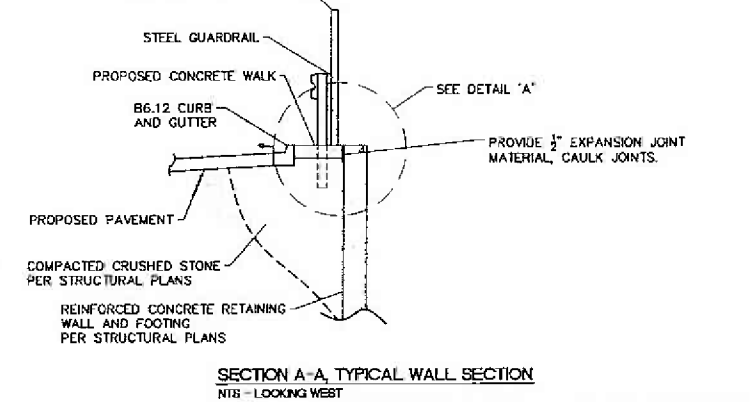
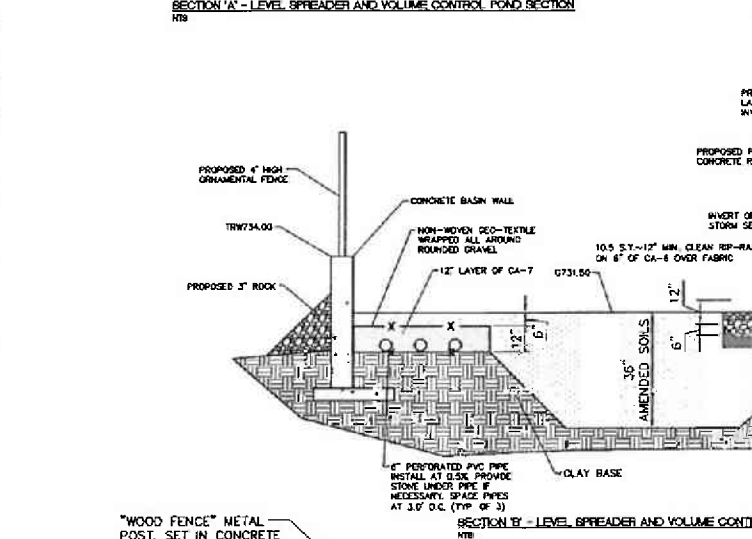
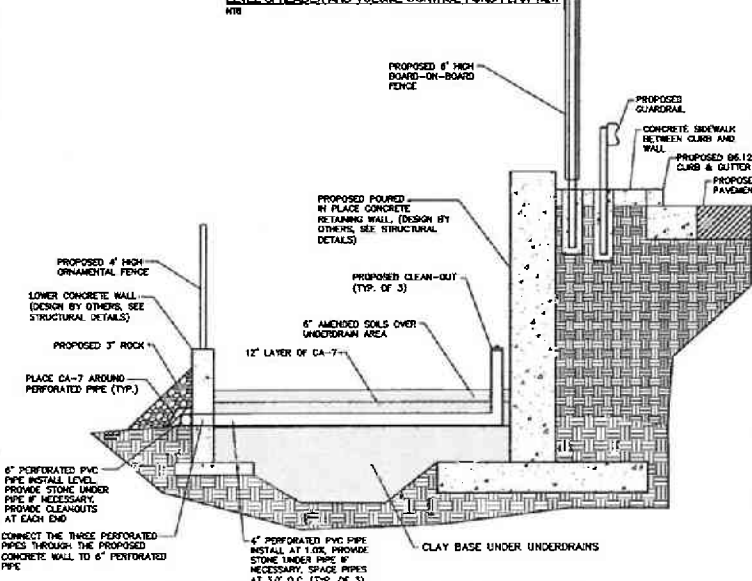
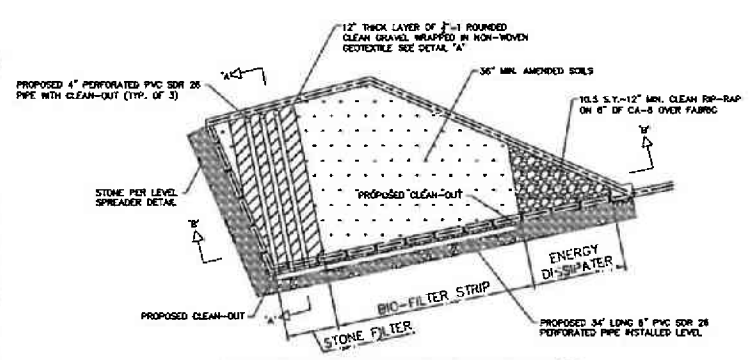
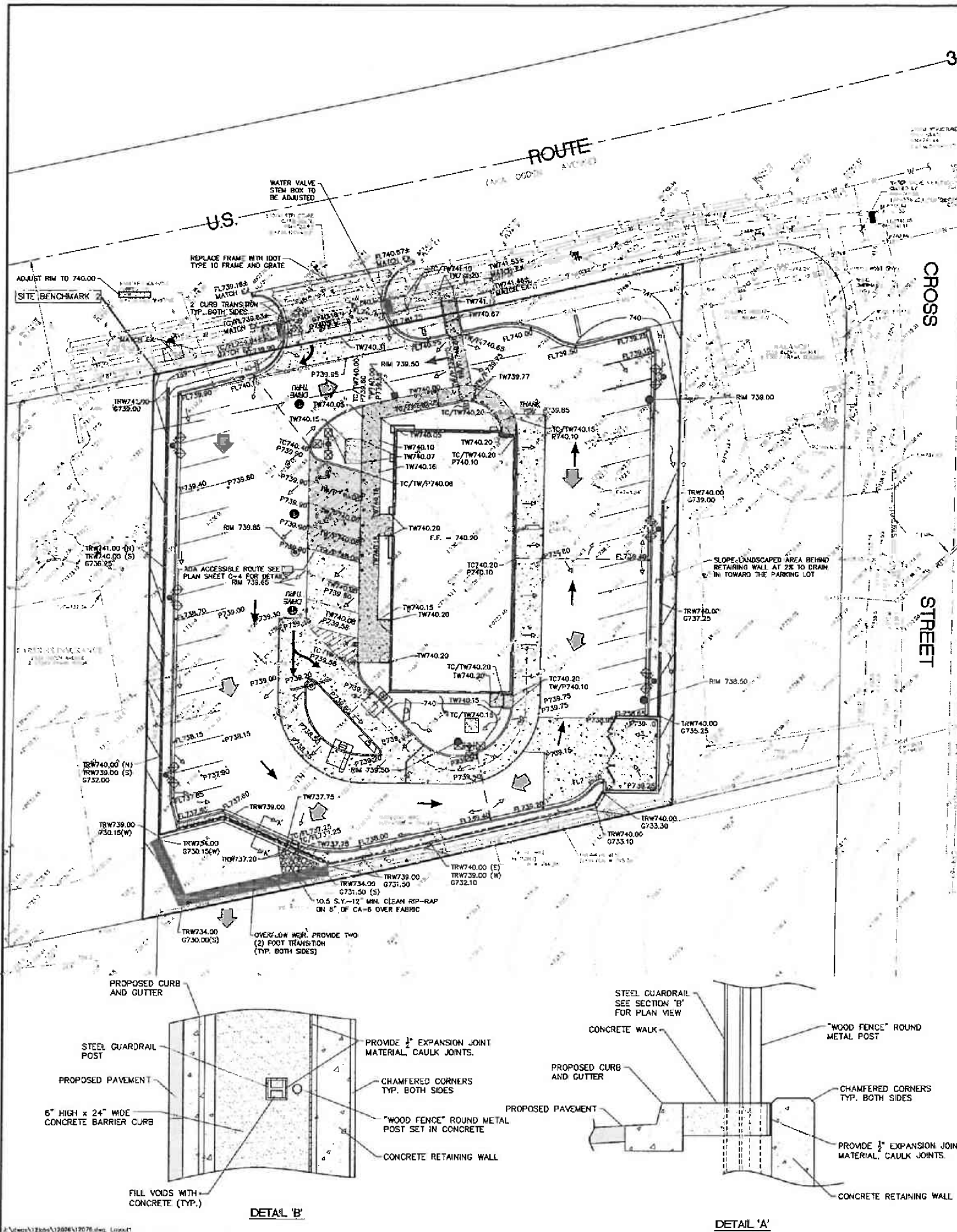
(TO HEAVY LINES)
 36,015 SQUARE FEET
 OR
 0.831 ACRES
 (BASED ON MEASURED VALUES)

NOTES

- THIS SURVEY IS BASED ON THE LEGAL DESCRIPTION AND EASEMENTS OF RECORD AS EXISTING IN THE CONVENTION RECORDS AND RECORDED BY CHICAGO TITLE INSURANCE HAVING AN EFFECTIVE DATE OF JULY 26, 2013.
- THIS SITE FALLS WITHIN TOWNSHIP ZONING ZONE X (ARCAS DETERMINED TO BE OUTSIDE THE 0.25 ANIMAL CHANGES FLOORING) AS SHOWN BY THE FLOOD INSURANCE RATE MAP, MAP NUMBER 17043CON04, HAVING AN EFFECTIVE DATE OF DECEMBER 15, 2014.
- THE PROFESSIONAL SERVICE CONTRACTORS TO THE CLIENT ADVISED MINIMUM STANDARDS FOR TOPOGRAPHIC SURVEYS, AND IS BASED ON FIELD WORK COMPLETED ON SEPTEMBER 24, 2013.
- AN ADDITION TO THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, THIS SURVEY WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS PROVIDED BY MCDONALD'S AS NOTED ON THE EASEMENT PROVIDED TO THE CLIENT.
- THE SURVEYOR CONTACTED JULIE LOFF (OFFICE LOCATING INFORMATION AND EASEMENTS) TOGETHER TO FIELD WITH ON THIS SITE AND THE UTILITIES SHOWN IN THE FIELD ARE SHOWN HEREON. ADDITIONALLY, MCDONALD'S REPRESENTATIVE TO THE SURVEYOR ADVISED THAT THE UTILITIES SHOWN ARE BEING ALIGNED TO THEIR PROPOSED FACILITIES. THE INFORMATION RECEIVED TO DATE IS SHOWN HEREON.
- THERE ARE NO CURBS OR GORES BETWEEN LOTS 2, 3, 4 AND 5 AS SHOWN HEREON.



| | |
|-------------|--|
| PROJECT | McDonald's - LC #12-2354 |
| CLIENT | MCDONALD'S CORPORATION 3301 GINGER WOODS PARKWAY, STE. 300 AURORA, IL 60504 PHONE: (630) 820-1000 FAX: (630) 820-1000 |
| DATE | 08-23-13 |
| CHECKED BY | SCOTT C. KRER |
| DATE | 08-23-13 |
| NO. | 13.0128-01 |
| SCALE | 1" = 20' |
| PAGE | 1 OF 2 |
| PROJECT NO. | 13.0128-01 |



GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE ALTA/ACSM LAND TITLE AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0128.01 DATED 09/23/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

REFERENCE BENCHMARK
 DUPAGE COUNTY BENCHMARK #J03002 (NAVO 88 DATUM)
 DISK ON THE NORTHEAST CORNER OF A BRIDGE FOR WARRENVILLE ROAD OVER THE EAST BRANCH OF THE DUPAGE RIVER, 33.4' NORTH OF THE CENTERLINE OF WARRENVILLE ROAD, 0.5' WEST OF THE EAST END OF THE NORTH BRIDGE HEADWALL. ELEVATION=670.86

DUPAGE COUNTY BENCHMARK #0166 (NAVO 88 DATUM)
 DISK ALONG THE EAST SIDE OF FINLEY ROAD AT THE OVERPASS FOR INTERSTATE 88. STATION IS 110.0' NORTH OF THE CENTERLINE OF A CAR DEALERSHIP ENTRANCE AND 32.0' EAST OF THE CENTERLINE OF FINLEY ROAD (NORTHBOUND). DISK LOCATED ON THE SOUTH END OF THE EAST BRIDGE WALL FOR THE OVERPASS. ELEVATION=771.01

SITE BENCHMARKS
 1. SOUTHEAST BONNET BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER OF THE INTERSECTION OF CROSS STREET AND U.S. ROUTE 34. ELEVATION=743.71
 2. NORTHWEST BONNET BOLT ON FIRE HYDRANT LOCATED AT THE NORTHWEST CORNER OF SITE. ELEVATION=739.75

GRADING PLAN NOTES:
 1. UNLESS OTHERWISE SPECIFIED, TOP OF CURB (IC) AND/OR TOP OF WALK ELEVATIONS ARE 0.5' HIGHER THAN THE ADJACENT FLOW LINE (FL) OR PAVEMENT (P) ELEVATIONS.
 2. IN ALL LOCATIONS WHERE ELEVATIONS ARE SHOWN AS ±, THE ELEVATION HAS BEEN DETERMINED BASED ON INTERPOLATED GRADES FROM THE SURVEY. CONTRACTOR IS TO VERIFY THESE GRADES PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS WITHIN THE PROXIMITY OF THESE INTERPOLATED GRADES AND REPORT THEM TO THE DESIGN ENGINEER FOR VERIFICATION OF PROPOSED SLOPES PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. DESIGN ENGINEER IS NOT RESPONSIBLE FOR SLOPES OF PROPOSED IMPROVEMENTS BASED ON THESE ± GRADES WITHOUT CONFIRMATION OF EXISTING ELEVATIONS AT TIME OF CONSTRUCTION.
 3. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPING.

| | |
|-----------|--|
| DATE | 12/25/13 |
| REVISIONS | PER VILLAGE REVIEW LETTER (DATED 11/18/13) |
| | PER PHONE CALL BY VILLAGE PLANNER (DATED 12/12/13) |
| NO. | 1 |
| | 2 |

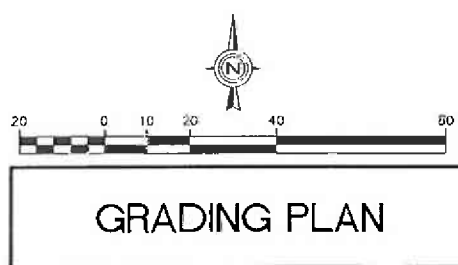
Prepared For:
 McDonald's
 4320 Winfield Road, Suite 400
 Warrenville, IL 60555
 McDonald's - DOWNERS GROVE, IL
 2535 Ogden Avenue
 Downers Grove, Illinois

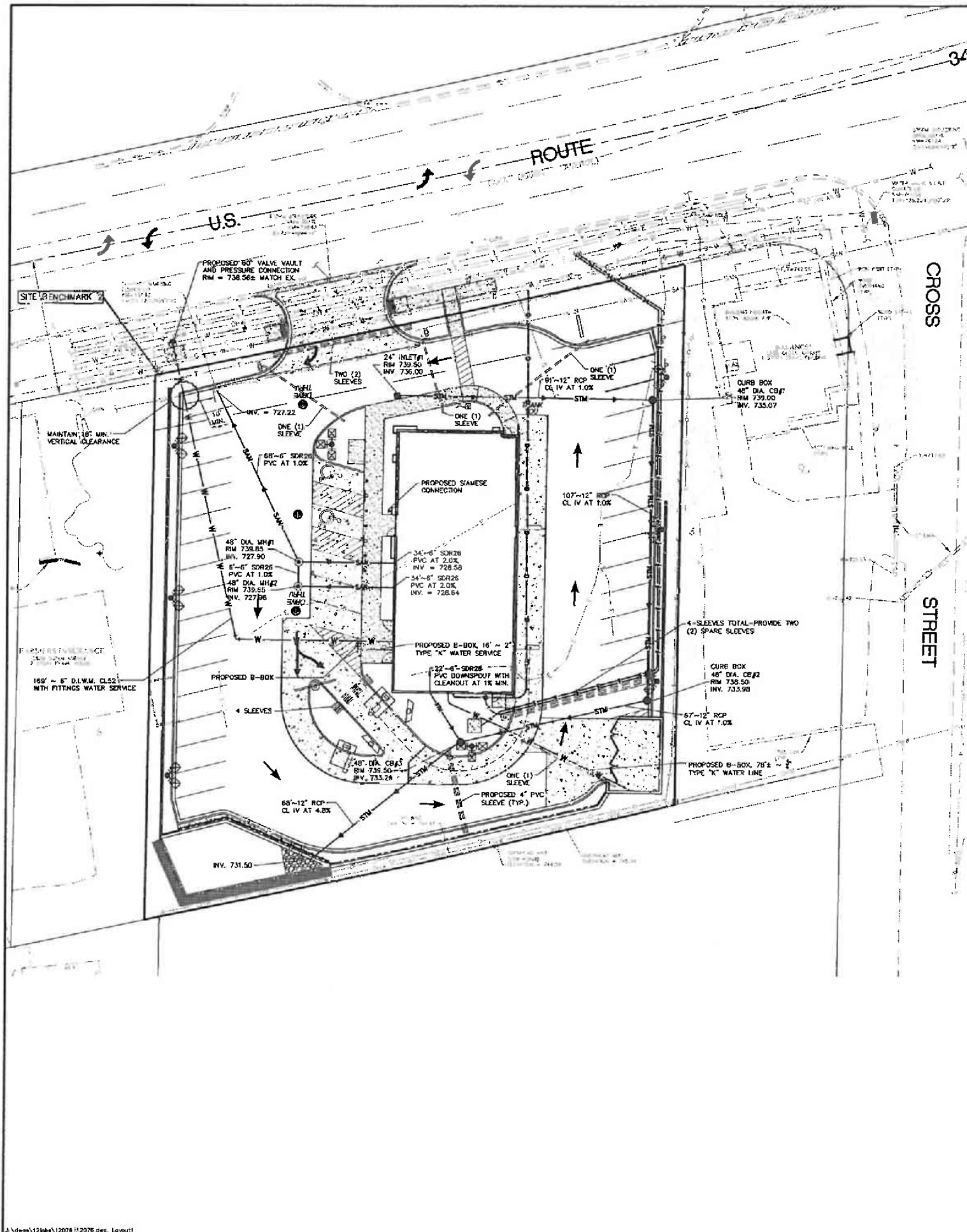
Prepared By:
 Watermark Engineering Resources, Inc.
 2631 Ginger Woods Parkway, Suite 100, Aurora, IL 60502
 phone 630-375-1900 fax 630-375-1900 www.watermark-engineering.com

| |
|-------------------------|
| CHECKED BY: J. MILLER |
| DESIGN BY: M. BAILEY |
| DRAWN BY: M. BAILEY |
| DATE: NOVEMBER 25, 2013 |
| SCALE: 1" = 20' |
| PROJECT NO.: 12-076 |

4 of 9
 LC #12-2554

GRADING PLAN





GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE ALTA/ACSM LAND TITLE AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0128.01 DATED 09/23/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

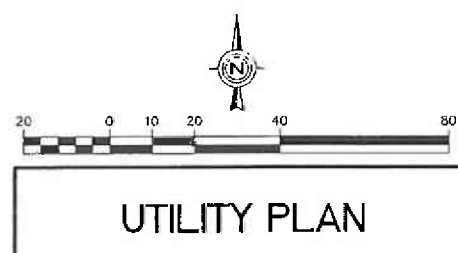
REFERENCE BENCHMARK:
 DUPAGE COUNTY BENCHMARK #J03002 (NAVD 88 DATUM)
 DISK ON THE NORTHEAST CORNER OF A BRIDGE FOR WARRENVILLE ROAD OVER THE EAST BRANCH OF THE DUPAGE RIVER. 33.4' NORTH OF THE CENTERLINE OF WARRENVILLE ROAD, 0.5' WEST OF THE EAST END OF THE NORTH BRIDGE HEADWALL. ELEVATION=670.85

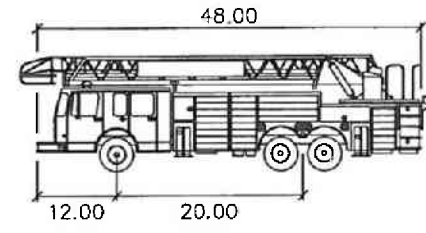
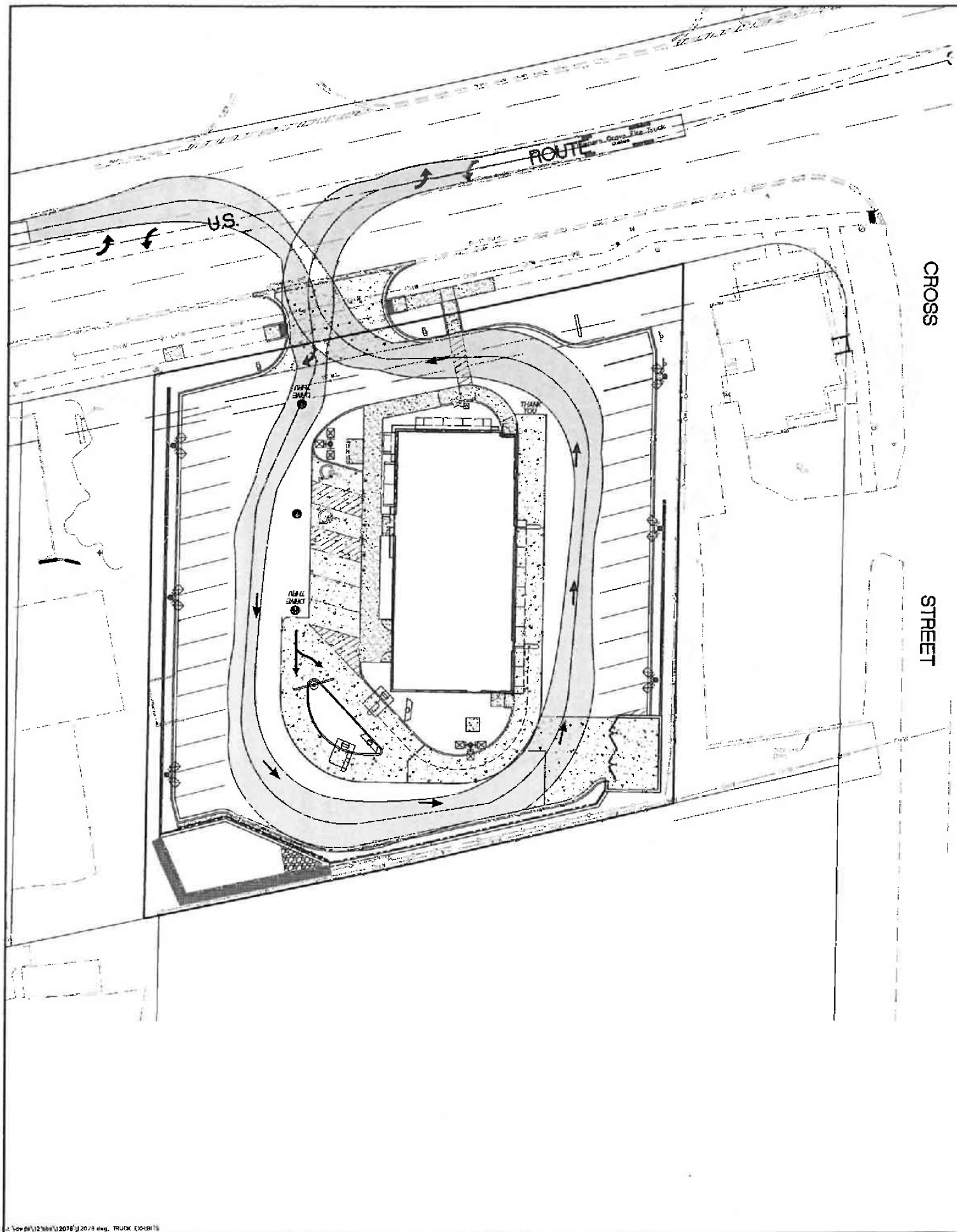
DUPAGE COUNTY BENCHMARK #0166 (NAVD 88 DATUM)
 DISK ALONG THE EAST SIDE OF FINLEY ROAD AT THE OVERPASS FOR INTERSTATE 88. STATION IS 110.0' NORTH OF THE CENTERLINE OF A CAR DEALERSHIP ENTRANCE AND 32.0' EAST OF THE CENTERLINE OF FINLEY ROAD (NORTHBOUND). DISK LOCATED ON THE SOUTH END OF THE EAST BRIDGE WALL FOR THE OVERPASS. ELEVATION=771.01

SITE BENCHMARKS:
 1. SOUTHEAST BONNET BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER OF THE INTERSECTION OF CROSS STREET AND U.S. ROUTE 34. ELEVATION=743.71
 2. NORTHWEST BONNET BOLT ON FIRE HYDRANT LOCATED AT THE NORTHWEST CORNER OF SITE. ELEVATION=739.75

UTILITY PLAN NOTES:
 1. PRIOR TO CONSTRUCTION OF ANY UTILITIES, CONTRACTOR IS TO VERIFY THAT THE PROPOSED UTILITIES SHOWN ON THIS PLAN THAT ENTER THE PROPOSED BUILDING(S) CORRESPOND WITH THE UTILITIES ON THE PLUMBING PLANS AS THEY EXIT THE BUILDING(S). CONTRACTOR TO REPORT IN WRITING ANY DISCREPANCIES IN SIZE, LOCATION, OR INVERT ELEVATION TO THE DESIGN ENGINEER IMMEDIATELY FOR RESOLUTION OF THE CONFLICT IN WRITING.
 2. GENERAL CONTRACTOR TO COORDINATE THE INSTALLATION AND PERMITTING OF THE PUBLIC UTILITIES, SUCH AS GAS, ELECTRIC, TELEPHONE, CABLE AND FIBER OPTICS, WITH THE PUBLIC UTILITY COMPANIES AND ARCHITECT PRIOR TO CONSTRUCTION. THE INSTALLATION OF THE PUBLIC UTILITIES AND NECESSARY SLEEVING TO BE INCLUDED AS PART OF GENERAL CONTRACTOR'S SCOPE OF WORK FOR THIS PROJECT.

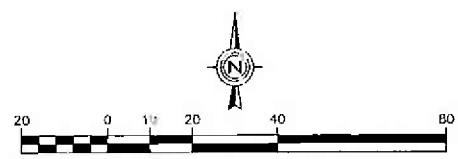
| | |
|---------------------|---|
| DATE | 12.02.13 |
| REVISIONS | |
| NO. | 1 |
| PREPARED FOR: | McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555 |
| PREPARED BY: | Watermark Engineering Resources, Ltd 2631 Ginger Woods Parkway, Suite 100, Aurora, IL 60502 Phone 630-820-9100 www.watermarkengineering.com |
| CHECKED BY: | J. MILLER |
| DESIGN BY: | M. BAILEY |
| DRAWN BY: | K. BAILEY |
| DATE: | NOVEMBER 25, 2013 |
| SCALE: | 1" = 20' |
| PROJECT NO.: | 12-076 |
| UTILITY PLAN | |
| 6 of 9 | |
| LC #12-2554 | |





Downers Grove Fire Truck
feet

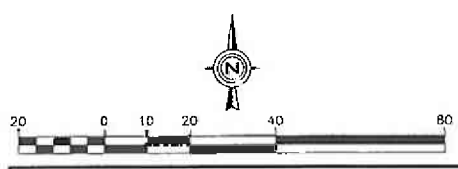
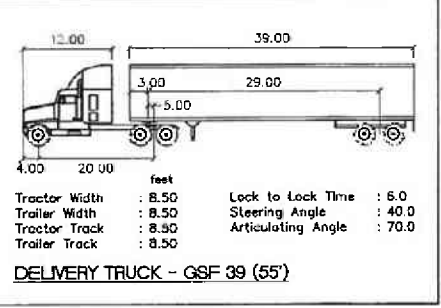
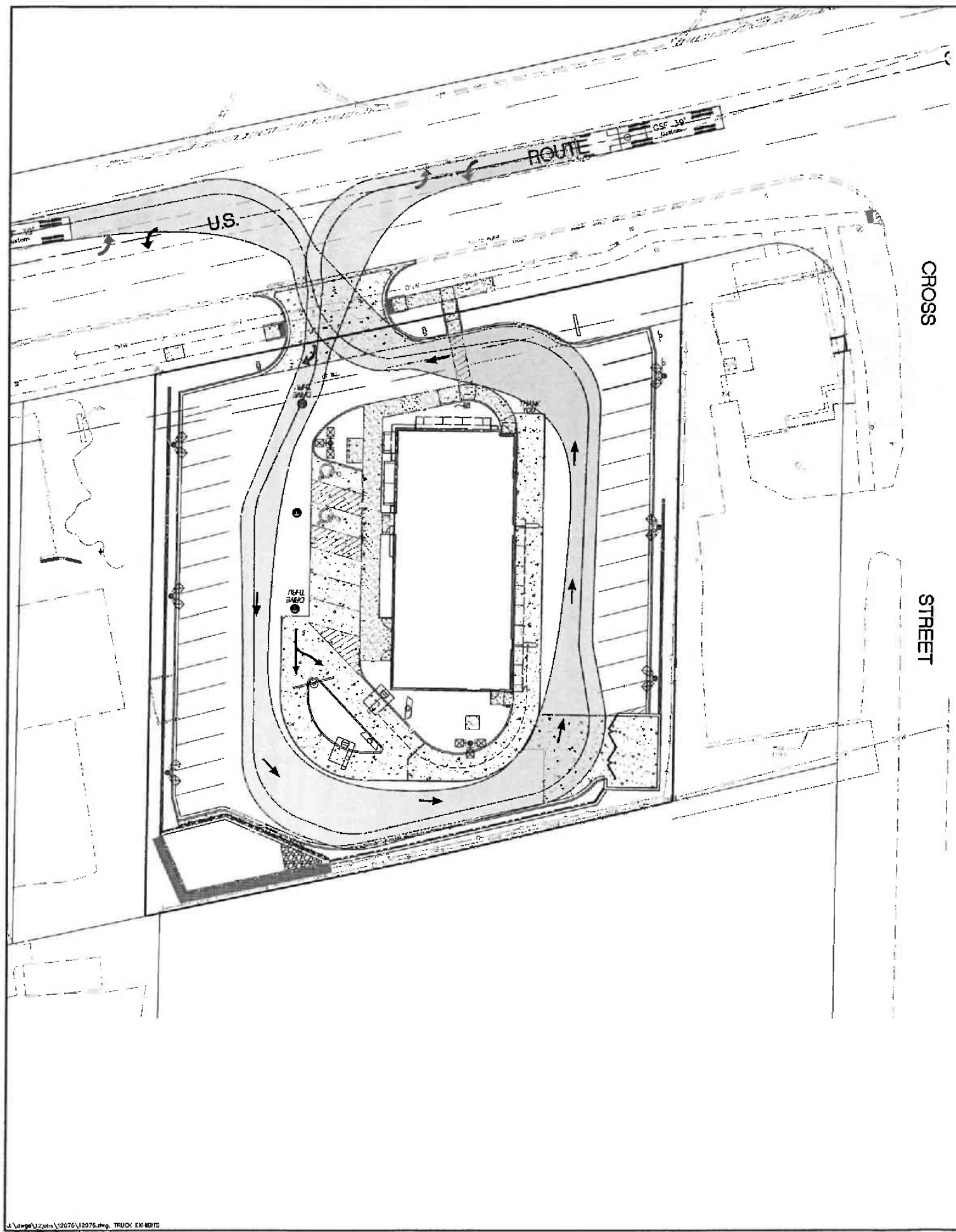
- Width : 8.00
- Track : 7.00
- Lock to Lock Time : 6.0
- Steering Angle : 32.0



**FIRE TRUCK
CIRCULATION PLAN**

| | |
|---|-----------------------------|
| CHECKED BY: J. MILLER DESIGN BY: M. BAILEY DRAWN BY: M. BAILEY DATE: DECEMBER 05, 2013 SCALE: 1" = 20' PROJECT NO.: 12-076 | DATE: |
| | REVISIONS: |
| Prepared For: | NO.: |
| McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555 McDONALD'S - DOWNERS GROVE, IL 2535 Ogden Avenue DOWNERS GROVE, Illinois | Prepared By: |
| | FIRE TRUCK CIRCULATION PLAN |
| 1 OF 1 LC #12-2554 | |

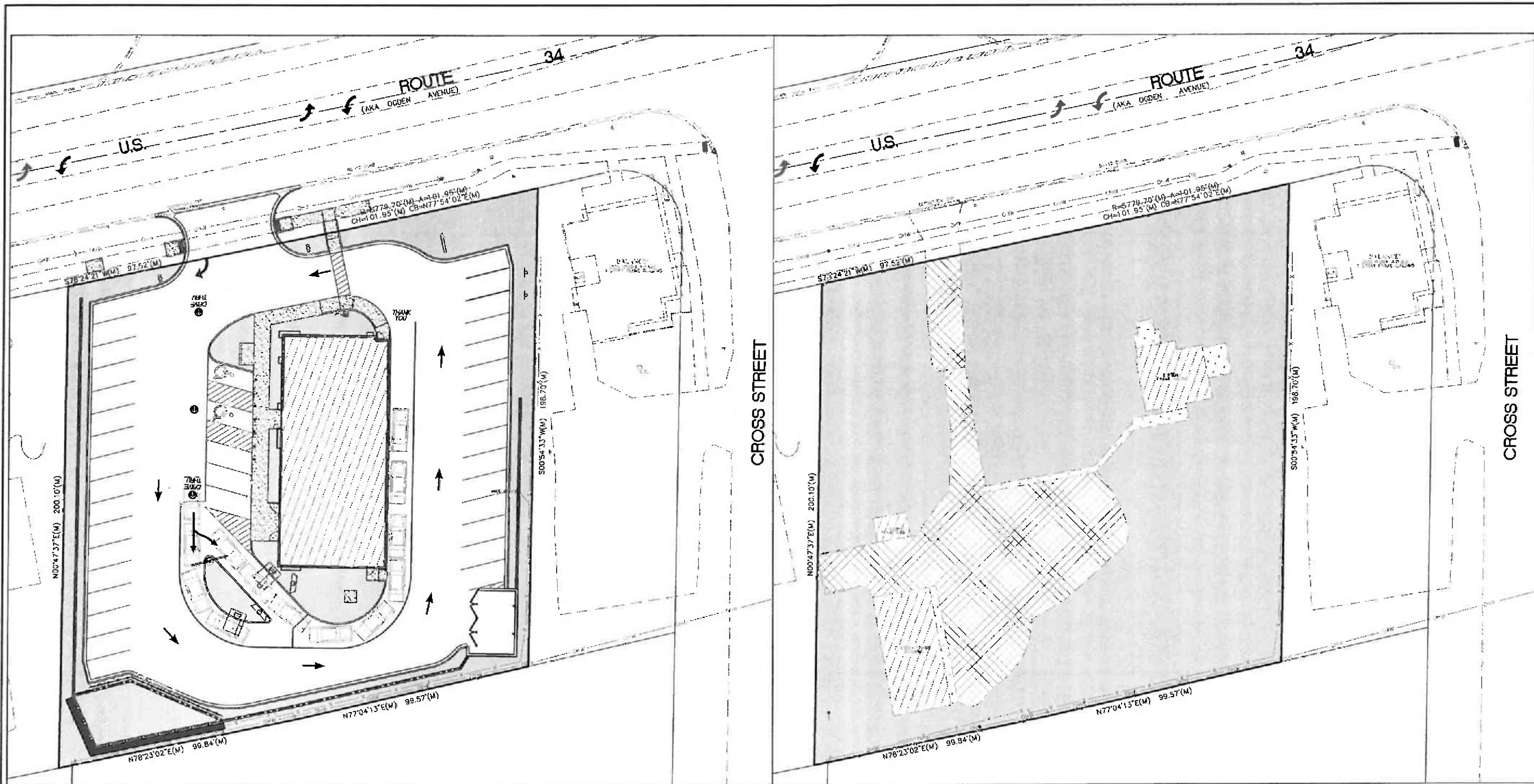
12/05/2013 12:00:00 PM 12-076.dwg, TRUCK CIRCULATION







TRUCK CIRCULATION PLAN

| | | |
|---|--|---|
| CHECKED BY: J. MALLER DESIGN BY: M. BAILEY DRAWN BY: M. BAILEY DATE: DECEMBER 05, 2013 SCALE: 1" = 20' PROJECT NO.: 12-075 | | NO. _____ DATE _____ REVISIONS _____ |
| Prepared For: McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555 McDONALD'S - DOWNERS GROVE, IL 2535 Ogden Avenue Downers Grove, Illinois | | Prepared By: 2631 Singer Woods Parkway, Suite 100, Aurora, IL 60502 phone 630-375-1800 fax 630-238-1900 www.watermark-engineering.com |
| 1 OF 1 LC #12-2554 | | TRUCK CIRCULATION PLAN |

\\vgp\j2\zeta\12076\12076.dwg, TRUCK EXHIBIT







PROPOSED PERVIOUS/IMPERVIOUS PLAN

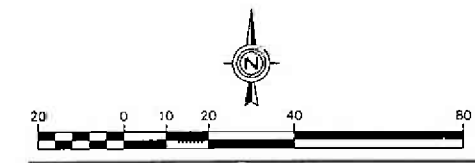
-  PROPOSED PERVIOUS AREA
-  PROPOSED PAVEMENT AREA
-  PROPOSED CONCRETE SIDEWALK AREA
-  PROPOSED BUILDING AREA

| PROPOSED SITE DATA | |
|----------------------------|------------------------------|
| LOT AREA | 38,815 S.F. (0.89 AC.) |
| PR PERVIOUS AREA | 8,987 S.F. (0.21 AC.) (24%) |
| PR IMPERVIOUS AREA | 29,828 S.F. (0.68 AC.) (76%) |
| PR PAVEMENT AREA | 24,110 S.F. |
| PR BUILDING AREA | 4,388 S.F. |
| PR CONCRETE SIDEWALK AREA | 1,330 S.F. |
| PR DISTURBED AREA ON SITE | 38,815 S.F. (0.89 AC.) |
| PR DISTURBED AREA OFF SITE | 1,539 S.F. (0.04 AC.) |

| EXISTING SITE DATA | |
|---------------------------------|------------------------------|
| LOT AREA | 38,815 S.F. (0.89 AC.) |
| EX PERVIOUS AREA | 29,089 S.F. (0.67 AC.) (75%) |
| TOTAL EX IMPERVIOUS AREA | 9,716 S.F. (0.22 AC.) (25%) |
| - ASPHALT/GRAVEL AREA | 6,948 S.F. |
| - BUILDING AREA | 2,115 S.F. |
| - CONCRETE PATIOS AND SIDEWALKS | 653 S.F. |

EXISTING PERVIOUS/IMPERVIOUS PLAN

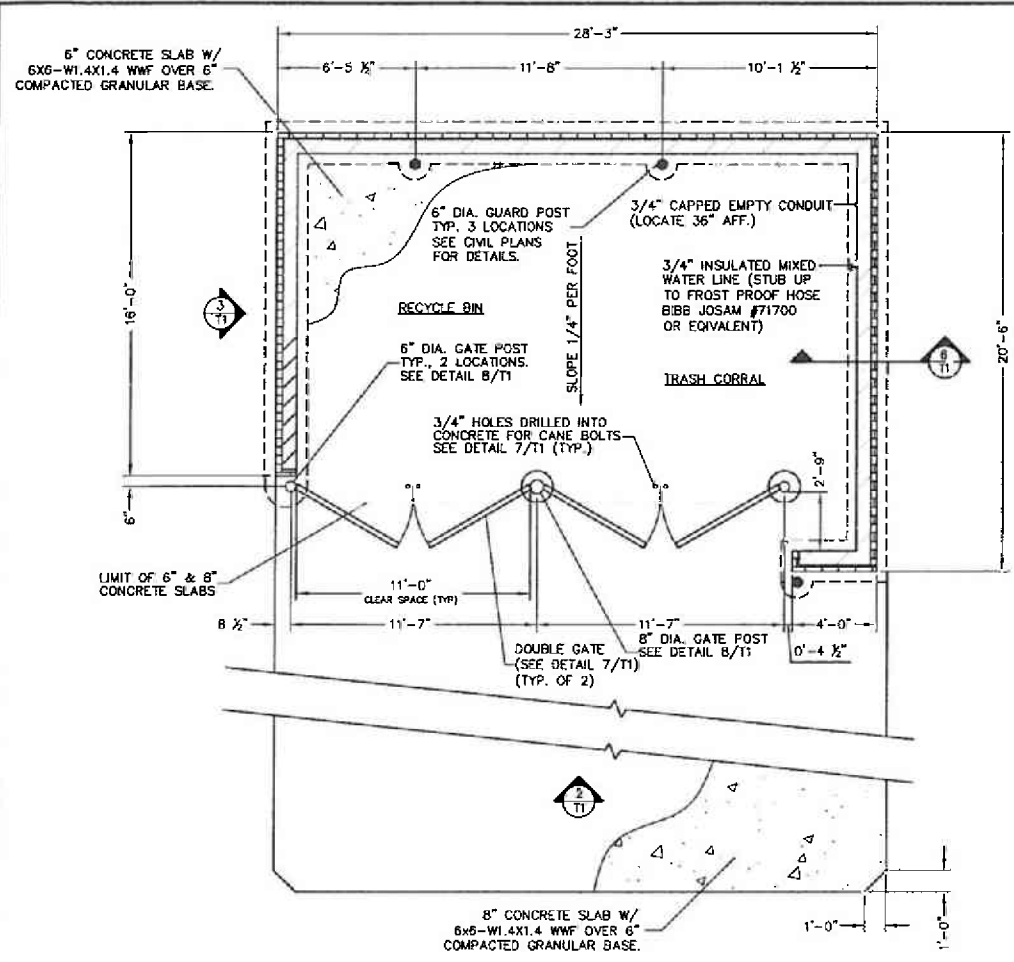
-  EXISTING PERVIOUS AREA
-  EXISTING ASPHALT/GRAVEL AREA
-  EXISTING BUILDING AREA
-  CONCRETE AREA



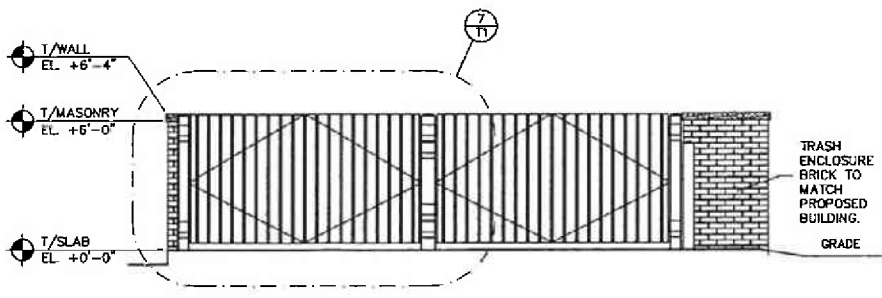
PERVIOUS/IMPERVIOUS EXHIBIT

| <p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th style="width: 10%;">NO.</th> <th style="width: 80%;">DESCRIPTION</th> <th style="width: 10%;">DATE</th> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </table> <p>Prepared For:</p> <p>McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555</p> <p>McDonald's - DOWNERS GROVE, IL 2535 Ogden Avenue Downers Grove, Illinois</p> <p>Prepared By:</p> | NO. | DESCRIPTION | DATE | | | | <p>Checked By: J. MILLER</p> <p>Design By: M. BAILEY</p> <p>Drawn By: M. BAILEY</p> <p>Date: DECEMBER 3, 2013</p> <p>Scale: 1" = 20'</p> <p>Project No.: 12-076</p> <p>9511 Green Woods Parkway, Suite 100 Downers Grove, IL 60555 Phone: 630-215-1800</p> <p>www.valmarkengineering.com</p> |
|--|-------------|-------------|------|--|--|--|--|
| NO. | DESCRIPTION | DATE | | | | | |
| | | | | | | | |
| <p>Valmark Engineering RESOURCES, LTD.</p> | | | | | | | |
| <p>1 of 1</p> | | | | | | | |
| <p>LC #12-2554</p> | | | | | | | |

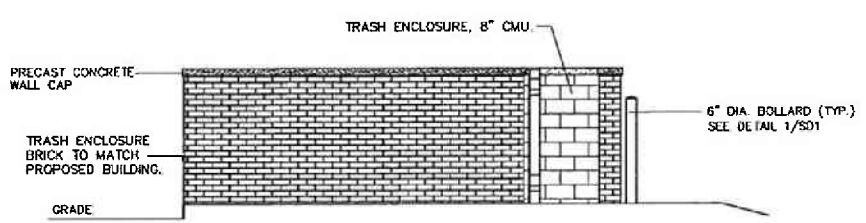
PERVIOUS/IMPERVIOUS EXHIBIT



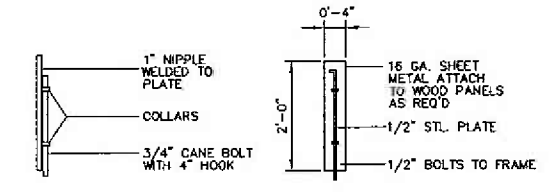
1
11
TRASH ENCLOSURE PLAN W/ RECYCLE BIN
SCALE: 1/4" = 1'-0"



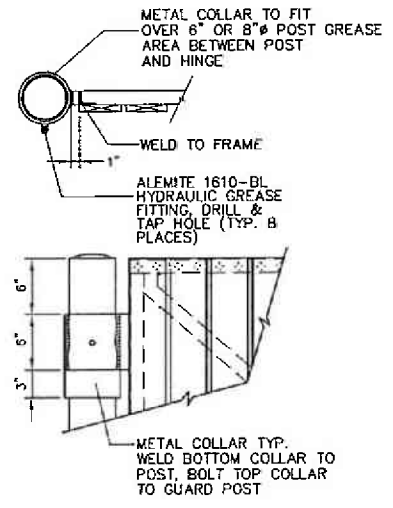
2
11
FRONT ELEVATION
SCALE: 1/4" = 1'-0"



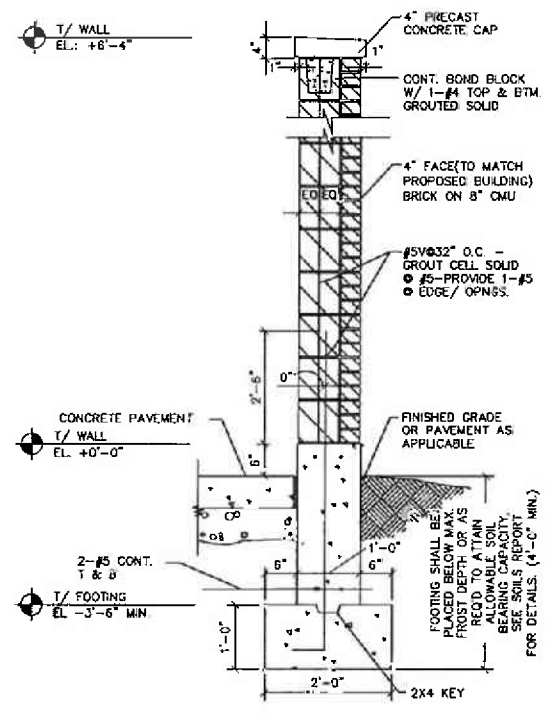
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11
RIGHT SIDE ELEVATION
SCALE: 1/4" = 1'-0"



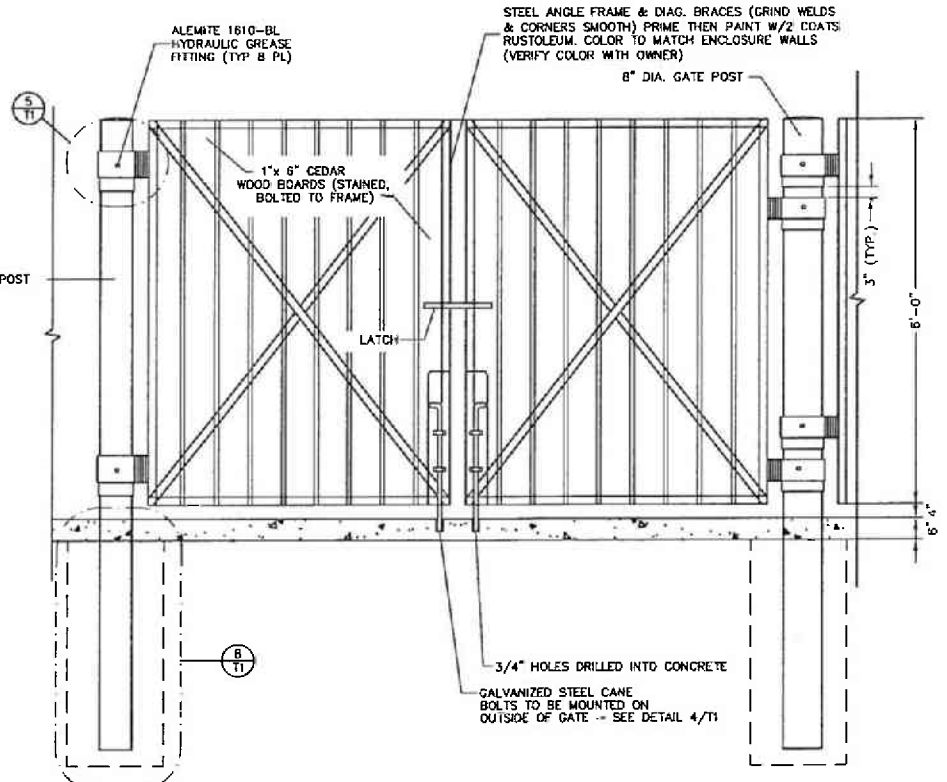
4
11
CANE BOLT DETAILS
N.T.S.



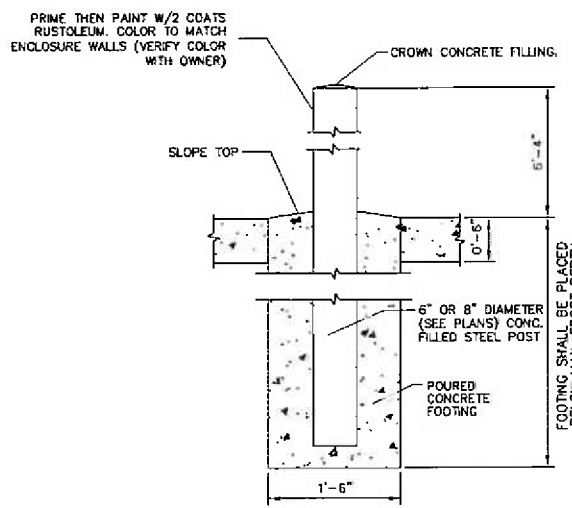
5
11
HINGE DETAILS
N.T.S.



6
11
TYPICAL WALL SECTION
SCALE: 3/4" = 1'-0"



7
11
TYPICAL GATE DETAIL
N.T.S.



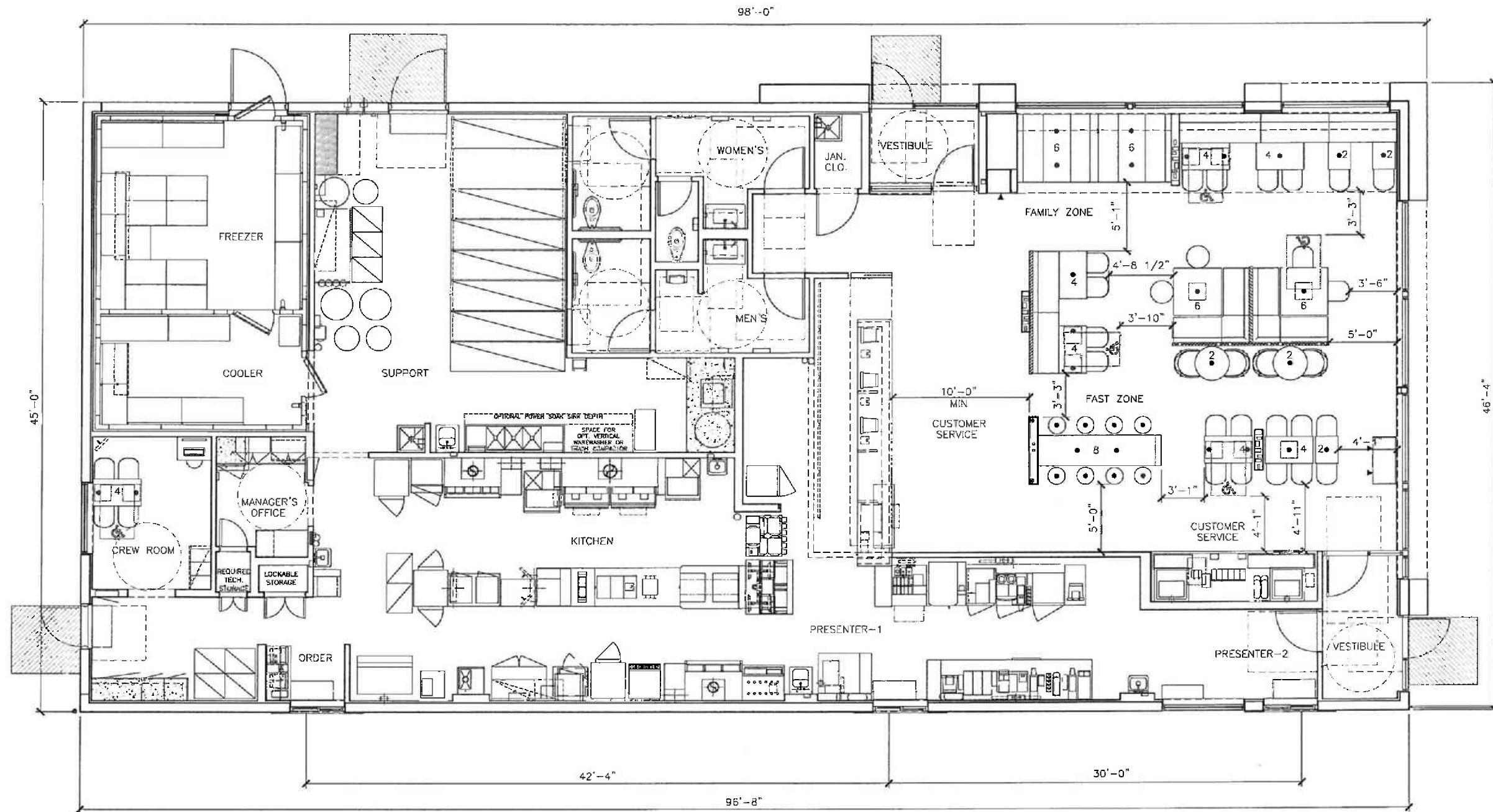
8
11
TYPICAL GATE POST DETAIL
N.T.S.

TRASH ENCLOSURE EXHIBIT

| | |
|---------------|---|
| DATE | |
| NO. | |
| Prepared For: | McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555 |
| Prepared By: | Watermark Engineering Resources, Inc. 2535 Ogden Avenue Downers Grove, Illinois |
| CHECKED BY: | J. MILLER |
| DESIGN BY: | M. BAILEY |
| DRAWN BY: | M. BAILEY |
| DATE: | DECEMBER 05, 2013 |
| SCALE: | |
| PROJECT NO.: | 12-078 |
| PHONE: | 630-315-1600 |
| FAX: | 630-218-8900 |
| WWW: | www.watermark-engineering.com |
| 1 of 1 | |
| LC #12-2554 | |

TRASH ENCLOSURE EXHIBIT

\\MCDONALD\FST04\CAD001\STD\STANDARD\PRELIM\2013 STANDARD BUILDINGS - 4TH QUARTER RELEASE\45 97 - MS (FULL)\A-REFERENCE PLANS.DWG 10-08-2012 07:38



1 SEATING PLAN
R1.3 1/4" = 1'-0"

4597- MS PLAN
4,395 GROSS SQ. FT. / 4,117 NET SQ. FT.
66 SEATS (4 ACCESSIBLE SEATS)

THIS DRAWING IS
FOR REFERENCE ONLY
NOT FOR CONSTRUCTION
SEATING LAYOUT IS SCHEMATIC, THE FINAL
SEATING LAYOUT TO BE PROVIDED BY OTHERS.

| | |
|---|--|
| <p>PREPARED FOR: McDonald's USA, LLC © 2013 McDonald's USA, LLC</p> <p>PREPARED BY: CORE STATES GROUP 111 Peachtree Road Atlanta, GA 30309 Phone: (770) 244-6460 Fax: (770) 244-6460</p> | <p>DATE: _____</p> <p>REV: _____</p> <p>DESCRIPTION: _____</p> |
| <p>DRAWN BY: AMC DATE: 1/2013</p> <p>REVISIONS: DATE: 3/21/2013 BY: PLP</p> <p>DATE: 3/21/2013 BY: PLP</p> | <p>PROJECT NO.: 028-1419.0</p> <p>SHEET NO.: R1.3</p> <p>SEATING PLAN</p> |
| <p>TITLE: 2013 STANDARD BUILDING 4597-MASONRY/STEEL</p> <p>DESCRIPTION: 4" MASONRY (QUICK BRICK) LOAD BEARING WALLS STEEL TRUSS ROOF HAVING SELF-SUPPORTING ZONED EXHAUST SYSTEM</p> <p>SITE ID: 122-354 SITE ADDRESS: 2333 OGDEN AVENUE DOWNSBORO, GA, 30130</p> | <p>DATE: _____</p> <p>REV: _____</p> <p>DESCRIPTION: _____</p> |

EUROWEST TILE PACKAGE:

INCLUDES: TILE, WATERPROOFING, SETTING MATERIAL, GROUT, INITIAL CLEANING

EUROWEST TILE: OYSTER WOODGRAIN 6"X36"X3/8" PORCELAIN BRUSHED HONED FINISH LAYOUT PATTERN: 33% MAX.

WATERPROOFING, SETTING MATERIAL AND GROUT: BY MAPS
GROUT COLOR: 3B AVALANCHE, GROUT WIDTH: 1/8" TO 3/16"

INITIAL CLEANING: DETERGENT
*** POST INSTALLATION CLEANER IS REQUIRED ON ALL TILE APPLICATIONS. VERIFY CLEANING SOLUTION PRODUCT AND INSTRUCTIONS WITH MANUFACTURER.***

EUROWEST CONTACT: JAN DETER, (714) 937-7500

ADDITIONAL MATERIALS
BY SCHLUTER SCHLUTER SYSTEMS L.P.

OUTSIDE CORNERS
COLOR: FOR OYSTER TILE - SAND PEBBLE RONDEC R0100SP (COLOR COATED ALUMINUM)

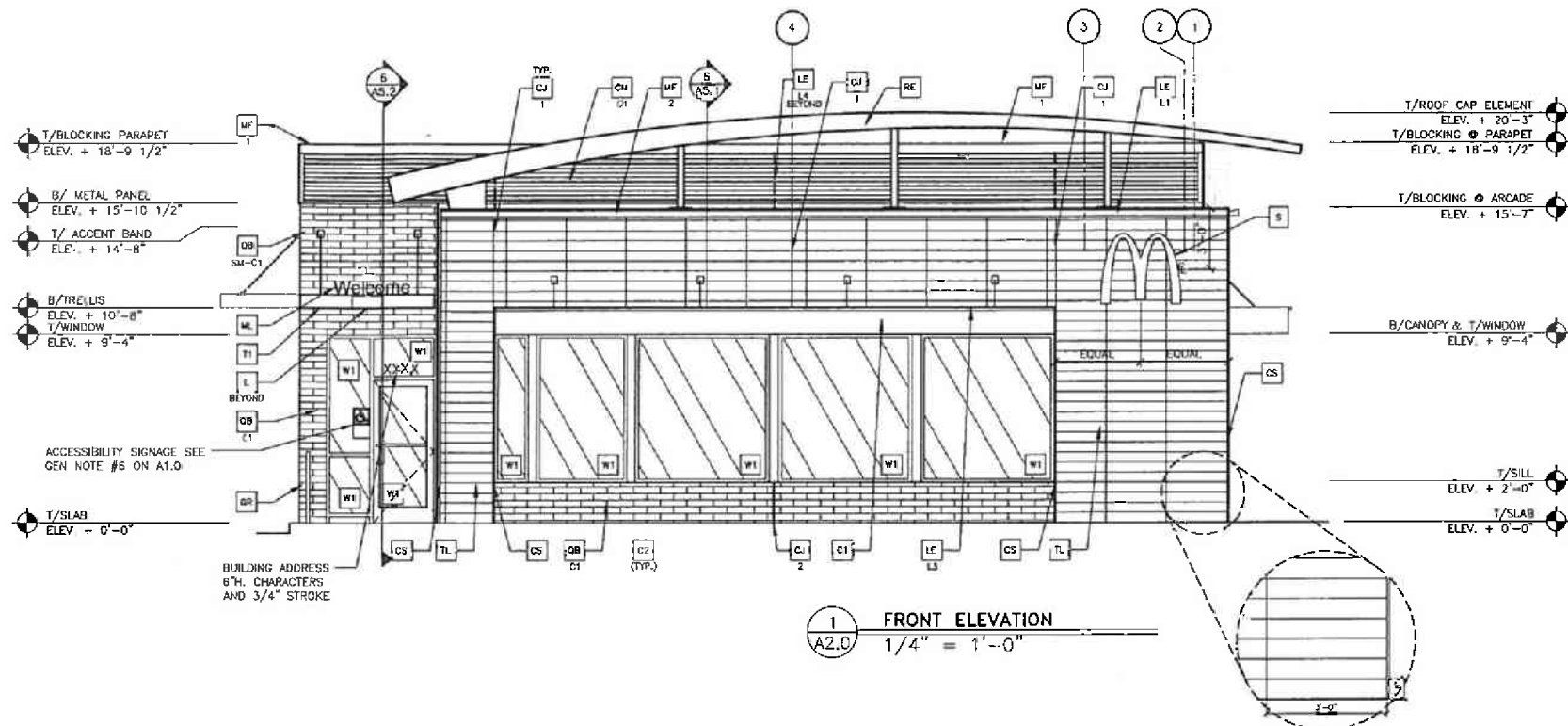
MOVEMENT JOINTS
COLOR: FOR OYSTER TILE - SAND PEBBLE DILEX BMB100SP

END OF TILE FIELD ABOVE ROOF AT HEARTH:
COLOR: FOR OYSTER TILE - SAND PEBBLE RONDEC R0100SP (COLOR COATED ALUMINUM)

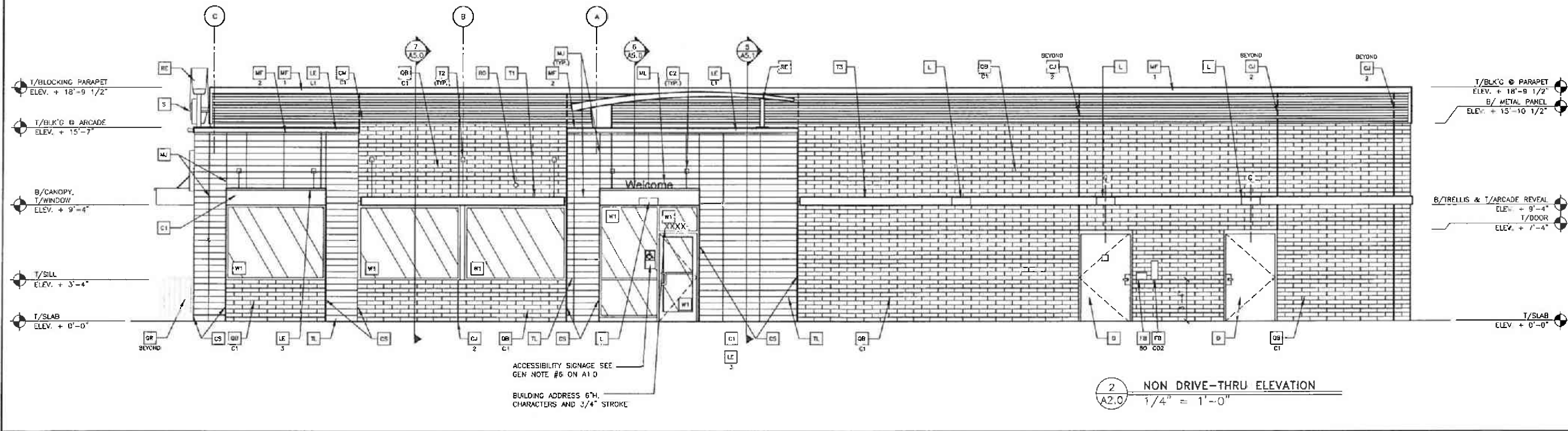
SCHLUTER SYSTEMS L.P. CONTACT: EARL MAICUS, (518) 324-3410

TILE GENERAL NOTES:
REFERENCE 2011 TCNA HANDBOOK -EJ171 MOVEMENT JOINT GUIDELINES

1. LOCATION AND FREQUENCY OF JOINTS
EXTERIOR: 8' TO 12' FEET IN EACH DIRECTION, OR AS INDICATED ON ELEVATIONS.
2. JOINT WIDTH
EXTERIOR: MINIMUM 3/8" FOR JOINTS 8' ON CENTER, MINIMUM 1/2" FOR JOINTS 12' ON CENTER.
3. PERIMETER JOINTS - MOVEMENT JOINTS ARE REQUIRED WHERE TILE WORK ABUTS RESTRAINING SURFACES SUCH AS PERIMETER WALLS, AND GRADE. FLEXIBLE SEALANT AND COMPRESSIBLE BACK-LIP WHEN REQUIRED OR BOND BREAKER TAPE.
JOINT MATERIAL: URETHANE
COLOR: TO MATCH TILE AND GROUT COLOR
4. TE-BACK AND SIGNAGE ATTACHMENT - PRE-DRILL TILE AND SLEEVE AND SEAL ALL ATTACHMENT POINTS.



1 FRONT ELEVATION
1/4" = 1'-0"



2 NON DRIVE-THRU ELEVATION
1/4" = 1'-0"

KEY NOTES:

- CB ALUMINUM CANOPY SYSTEM (COLOR: GOLD)
- CD ALUMINUM CANOPY TIE-BACK SYSTEM
- CE CONTROL JOINT MASONRY, SEE DETAIL 7/4.1
- CF CORRUGATED METAL PANEL, COLOR = CITYSCAPE BY METAL ERA OVER SMOOTH FACED CONCRETE BLOCK - SEE 16/AS.0
- CG OUTSIDE CORNER STRIP, BY SCHLUTER - SEE TILE PACKAGE AND GENERAL NOTES FOR ADDITIONAL INFORMATION
- CH HOLLOW METAL DOOR - PAINT TO MATCH COLOR OF SURROUNDING MATERIAL
- CI ALUMINUM CANOPY SYSTEM (COLOR: GOLD)
- CL ALUMINUM CANOPY TIE-BACK SYSTEM
- CM CONTROL JOINT MASONRY, SEE DETAIL 7/4.1
- CN CORRUGATED METAL PANEL, COLOR = CITYSCAPE BY METAL ERA OVER SMOOTH FACED CONCRETE BLOCK - SEE 16/AS.0
- CO OUTSIDE CORNER STRIP, BY SCHLUTER - SEE TILE PACKAGE AND GENERAL NOTES FOR ADDITIONAL INFORMATION
- CP HOLLOW METAL DOOR - PAINT TO MATCH COLOR OF SURROUNDING MATERIAL
- CR MOVEMENT JOINT, BY SCHLUTER - SEE TILE PACKAGE AND GENERAL NOTE, FOR ADDITIONAL INFORMATION
- CS METAL LETTERING - BY OTHERS
- CT METAL FASCIA - COLOR TO MATCH CORRUGATED METAL PANEL - SEE 7/40.0
1 = PRE-FAB ANCHOR-TITE FASCIA
2 = PRE-FAB CUSTOM ARCADE FASCIA
- CU PIPE BOLLARD - PAINTED YELLOW
- CV (RMC) DOWN COLLECTOR UNIT #WPT 072000 CMU, CALL 1-858-743-7435 TO ORDER
- CA GUNK BRICK STRUCTURAL CMU BY OLDCASTLE ARCHITECTURAL
C1 = RICHFIELD BY OLDCASTLE
- CB ROOF CAP ELEMENT BY OTHERS
- CC ROOF DRAIN OVERFLOW PIPE PAINT TO MATCH SURROUNDING MATERIAL
- CD MCDONALD'S SIGNAGE BY OTHERS - UNDER SEPARATE PERMIT.
- CE WOODGRAIN TILE (RIS U.S.) BY EUROWEST DECORATIVE SURFACES COLOR: OYSTER CONTACT: JAN DETER 714.937.7500
- CF ALUMINUM TRELLIS SYSTEM
- CG TRELLIS TIE-BACK SYSTEM
- CH ALUMINUM TRELLIS 2" x 8" WALL FASCIA SYSTEM - REFER TO SIM DETAIL 3 ON SHEET AS.1
- CI EXTERIOR WINDOW ASSEMBLY - TEMPERED GLASS - SEE ASSEMBLY NOTES
- CL DRIVE-THRU WINDOW BY READY ACCESS 600 SERIES, 36" SERVICE HEIGHT WITH TRANSOM - MANUAL OPEN; ELECTRONIC RELEASE
- CM FLOE DIRECTION: RL = RIGHT TO LEFT
LR = LEFT TO RIGHT



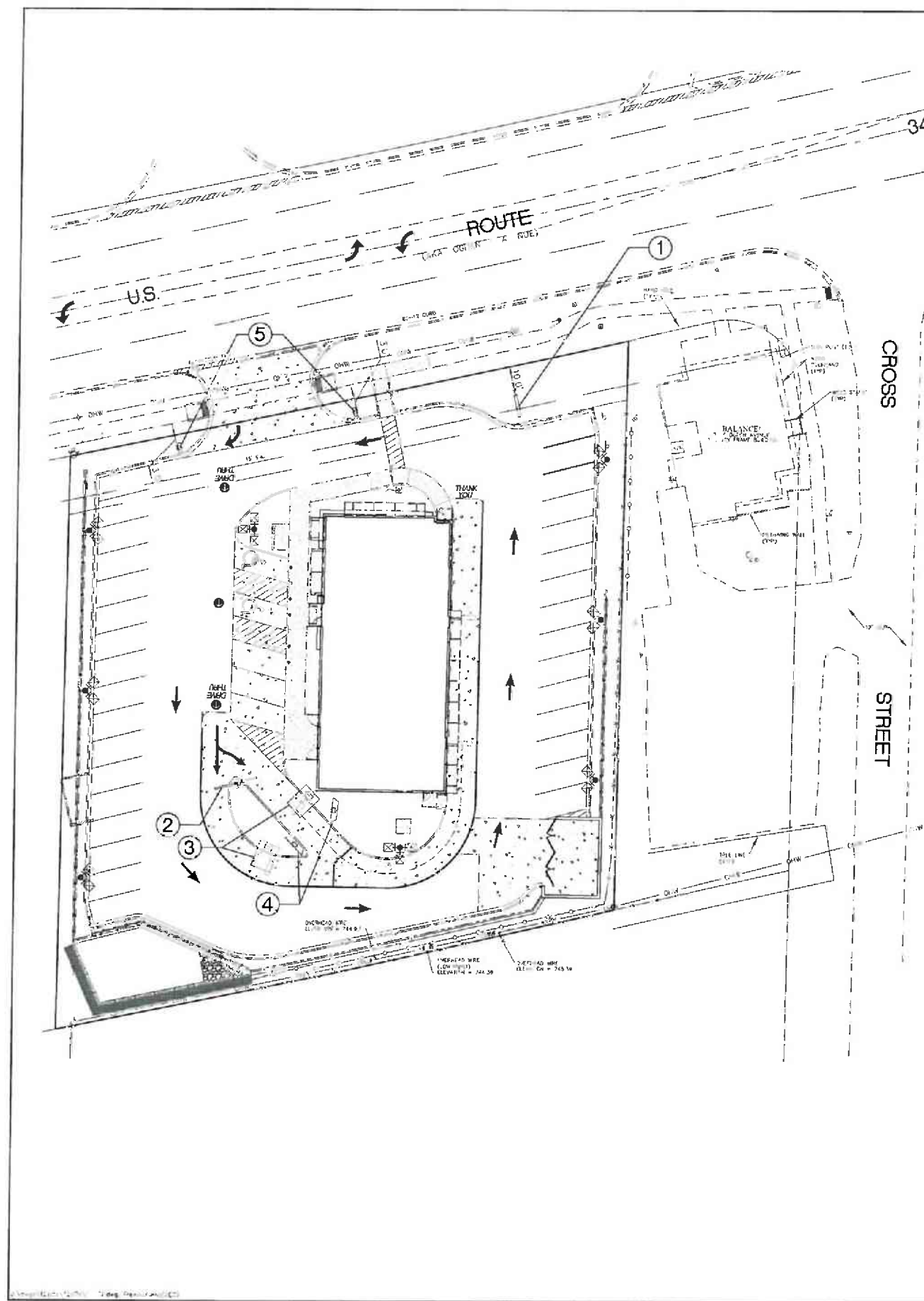
McDonald's USA, LLC
2515 Kildebrandt, USA, LLC
Drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be copied or reproduced in any form without the prior written consent of McDonald's USA, LLC. This drawing is for use on a specific site in conjunction with its plans and shall not be used for any other project. The use of this drawing on any other project without the prior written consent of McDonald's USA, LLC is strictly prohibited. McDonald's USA, LLC is not responsible for any errors or omissions in these drawings. The contractor shall be responsible for verifying all dimensions and conditions of the site before construction. The contractor shall be responsible for obtaining all necessary permits and approvals. The contractor shall be responsible for protecting all existing utilities and structures. The contractor shall be responsible for maintaining access to all adjacent properties. The contractor shall be responsible for maintaining the site in a safe and sound condition at all times. The contractor shall be responsible for removing all debris and materials from the site upon completion of the project. The contractor shall be responsible for restoring the site to its original condition or better. The contractor shall be responsible for all costs associated with the project, including but not limited to labor, materials, and equipment. The contractor shall be responsible for all risks associated with the project, including but not limited to injury, property damage, and environmental damage. The contractor shall be responsible for all compliance with applicable laws and regulations. The contractor shall be responsible for all quality control and inspection of the project. The contractor shall be responsible for all communication and coordination with the owner and other stakeholders. The contractor shall be responsible for all scheduling and completion of the project. The contractor shall be responsible for all documentation and record keeping of the project. The contractor shall be responsible for all safety and health of the project. The contractor shall be responsible for all environmental protection of the project. The contractor shall be responsible for all social and community impact of the project. The contractor shall be responsible for all other matters related to the project.

2013 STANDARD BUILDING
4597--MASONRY/STEEL
DESCRIPTION
4" MASONRY (GUNK BRICK) LOAD BEARING WALLS
STEEL TRUSS ROOF FRAMING
TILE EXTERIOR FINISH ARCADE/STAIR
SITE ADDRESS
122-564 2505 OGDEN AVENUE
DOWNS ORC, LLC
029-1419.0
A2.0
ELEVATIONS

K:\STD\STANDARD\PRELIM\2011 STANDARD BUILDINGS - APRIL 2012 RELEASE\4597 NS - FULL SET\DWGS\A-ELEV\DWG 04-25-2012-19.09





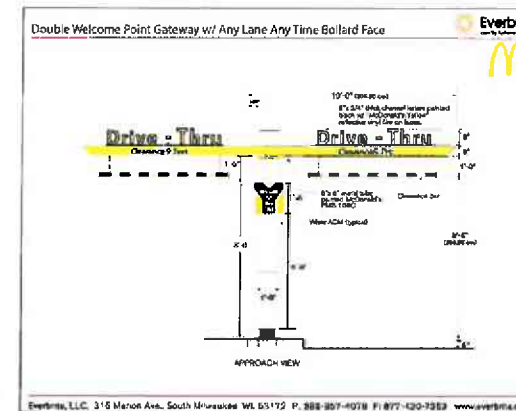


FREESTANDING SIGNAGE LEGEND

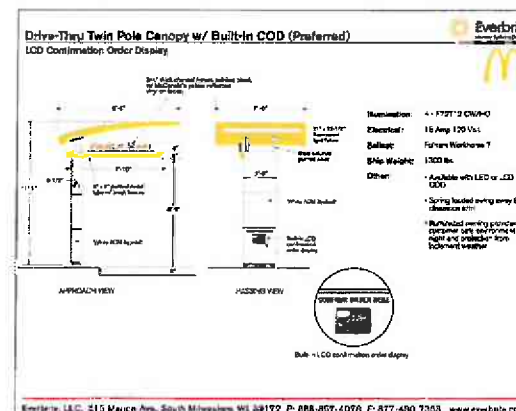
① McDONALD'S MONUMENT SIGN



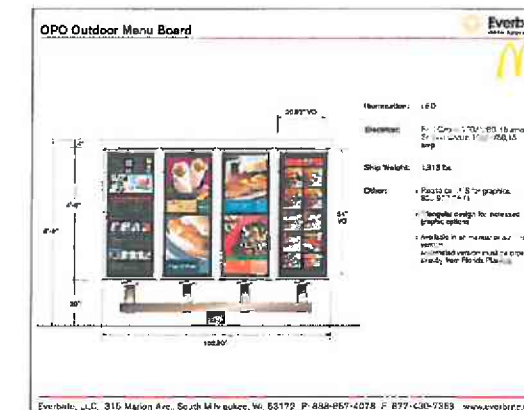
② McDONALD'S DOUBLE WELCOME POINT GATEWAY



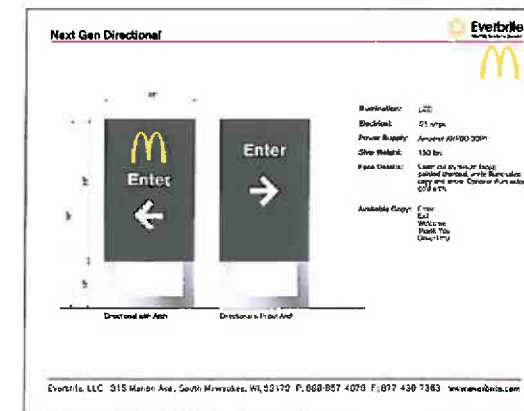
③ McDONALD'S DRIVE-THRU TWIN POLE CANOPY W/BUILT IN COD



④ McDONALD'S OPO MENU BOARD

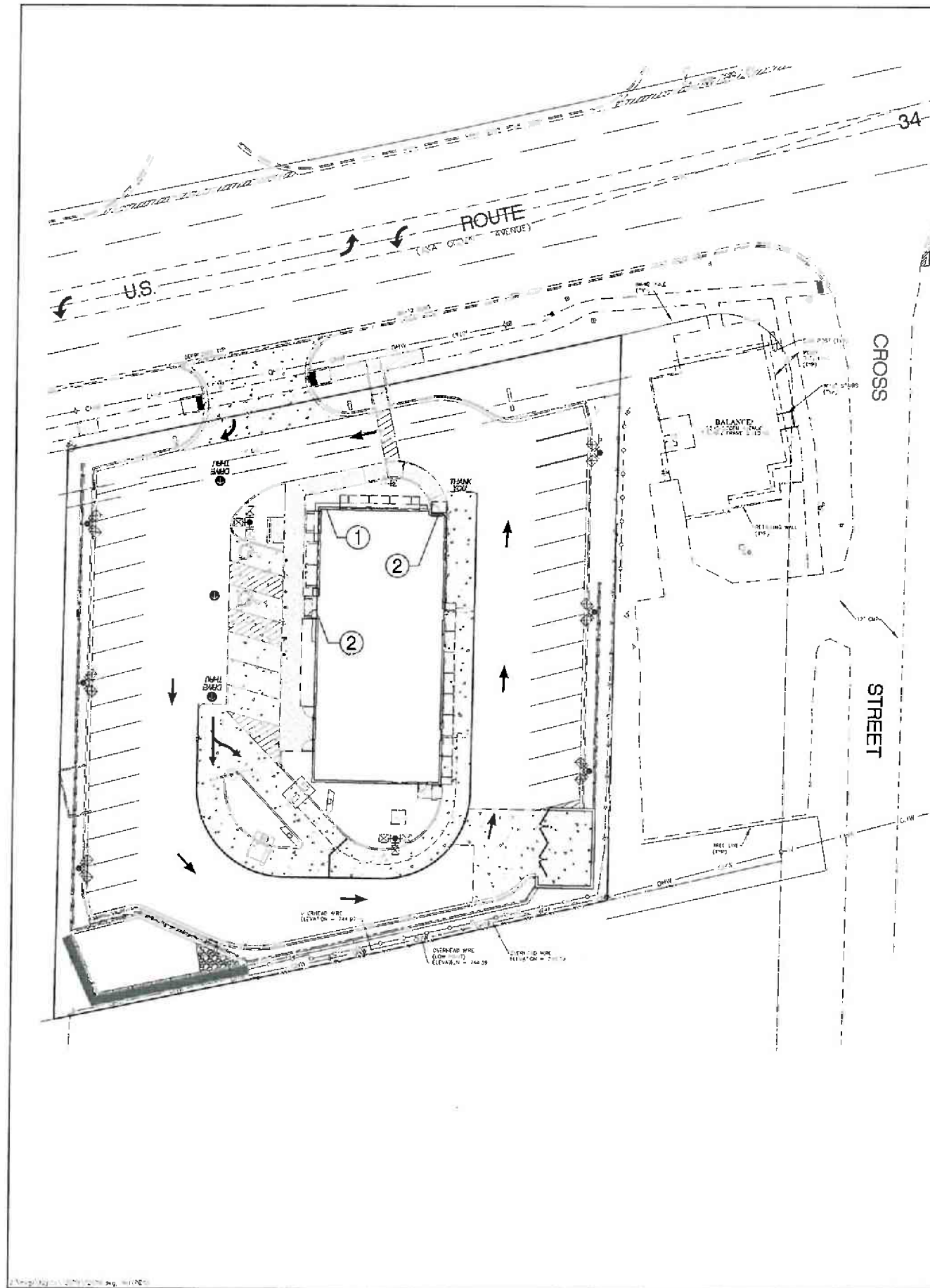


⑤ McDONALD'S DIRECTIONAL SIGN



FREESTANDING SIGNAGE EXHIBIT

DATE: 12/15/11
 PREPARED BY: EVERBILT, LLC
 PROJECT: 4320 Winfield Road, Suite 400, Waterville, IL 60555
 CLIENT: McDONALD'S - DOWNERS GROVE, IL
 2535 Ogden Avenue, Downers Grove, Illinois
 Prepared For: **McDonald's**
 Prepared By: **Watermark Engineering Resources, Ltd.**
 2891 Chigger Woods Parkway, Suite 100, Aurora, IL 60502
 Phone: 630-375-4800 Fax: 630-239-3600 www.watermarkengineering.com
 1 of 1
 LC #12-2554



WALL SIGN LEGEND

- ① McDONALD'S 42" NEXTGEN ILLUMINATED BUILDING ARCH-LED

42" NextGen Illuminated Building Arch - LED

Everbrite

| | |
|--------------|---------------------------|
| Material: | LED |
| Electrical: | 25 AMP'S |
| Estimate: | (1) 00904 0175-120-277-24 |
| Ship Weight: | |

Everbrite, LLC, 515 Kipling Ave, South Milwaukee, WI, 53172 P: 888-857-4075 F: 877-450-1363 www.everbrite.com

- ② McDONALD'S WELCOME SIGN

Welcome Sign

Everbrite

| | |
|--------------|--|
| Material: | N/A |
| Ship Weight: | 15 lbs |
| Other: | 3/4" aluminum extrusion with vinyl lettering |



WALL SIGNAGE EXHIBIT

| | | |
|---|-----------------|-------------------------|
| DATE: 11/13 | BY: JWC | REVIEW: M.A.L. 11/18/13 |
| Prepared For: | | |
| McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555 McDONALD'S - DOWNERS GROVE, IL 2535 Ogden Avenue Downers Grove, Illinois | | |
| Prepared By: | | |
| | | |
| DATE: 11/13 | BY: JWC | REVIEW: M.A.L. 11/18/13 |
| DATE: OCTOBER 21, 2013 | SCALE: 1" = 20' | PROJECT NO.: 12-076 |
| 1 of 1 | | |
| LC #12-2554 | | |

FREESTANDING SIGNAGE EXHIBIT

COLOR LANDSCAPE PLAN
McDONALD'S - 2535 Ogden Ave
Downers Grove, IL
NOVEMBER 26, 2013

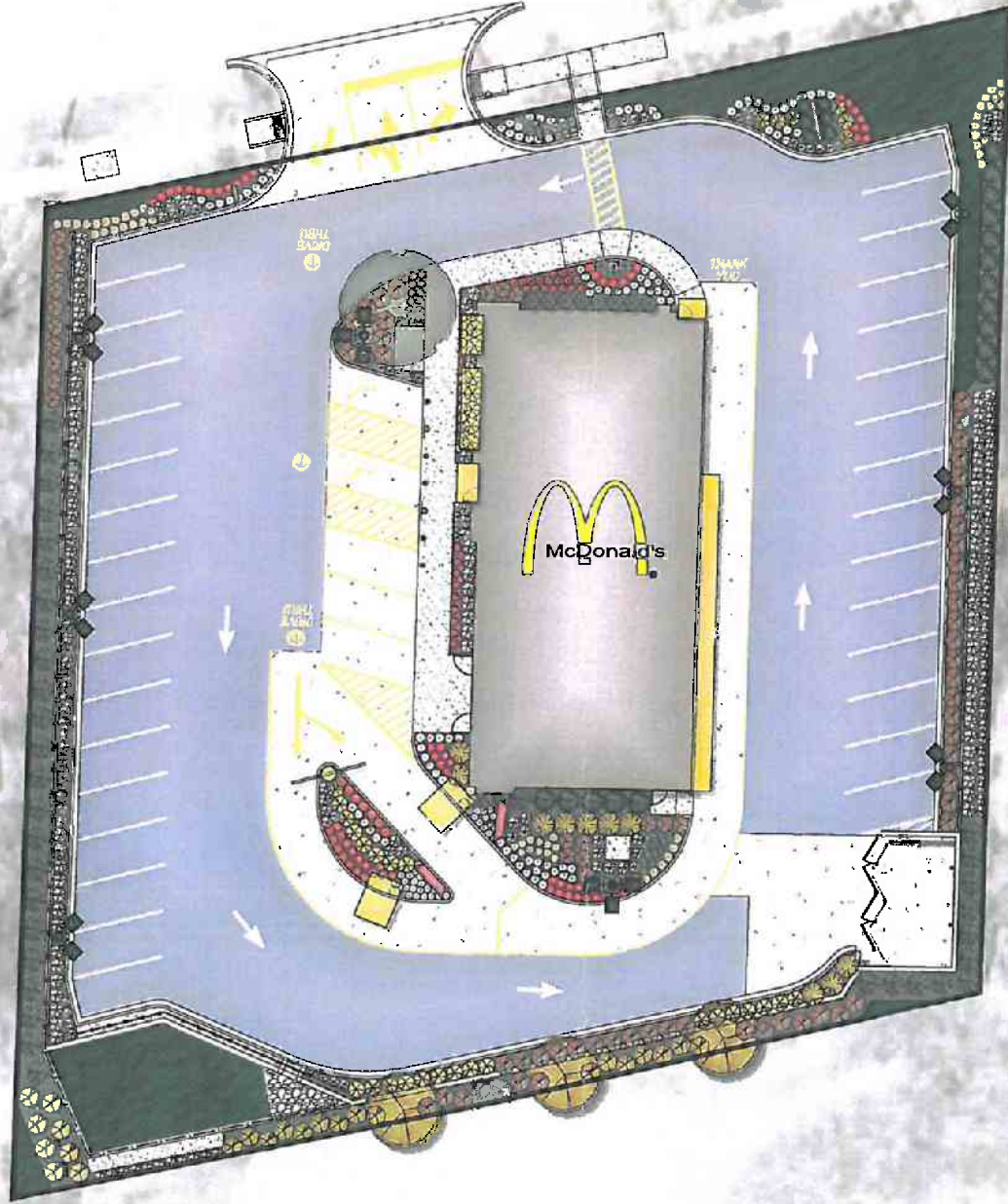
ROUTE
(AKA OGDEN AVENUE)

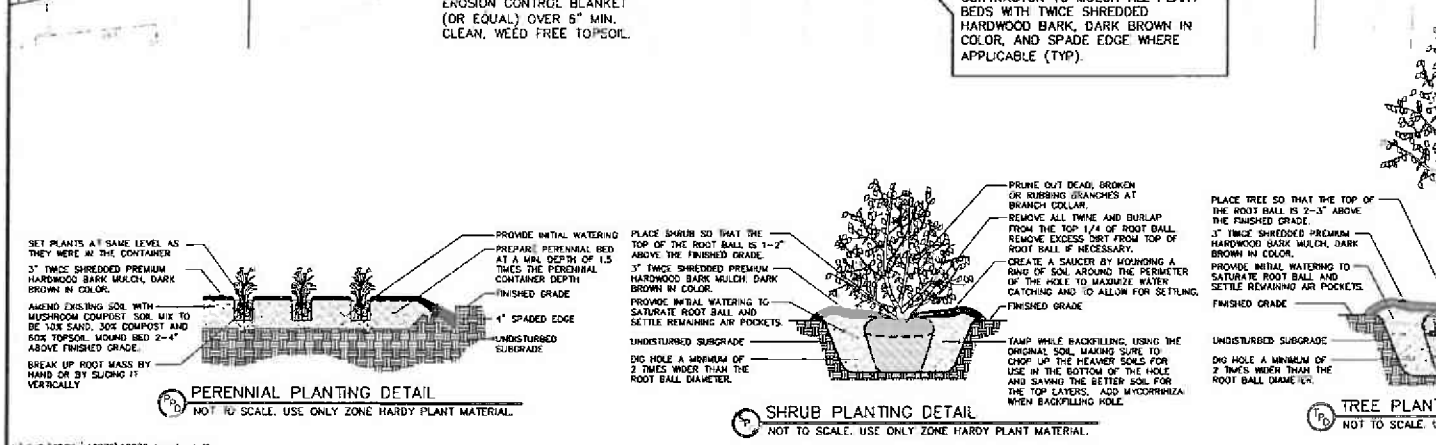
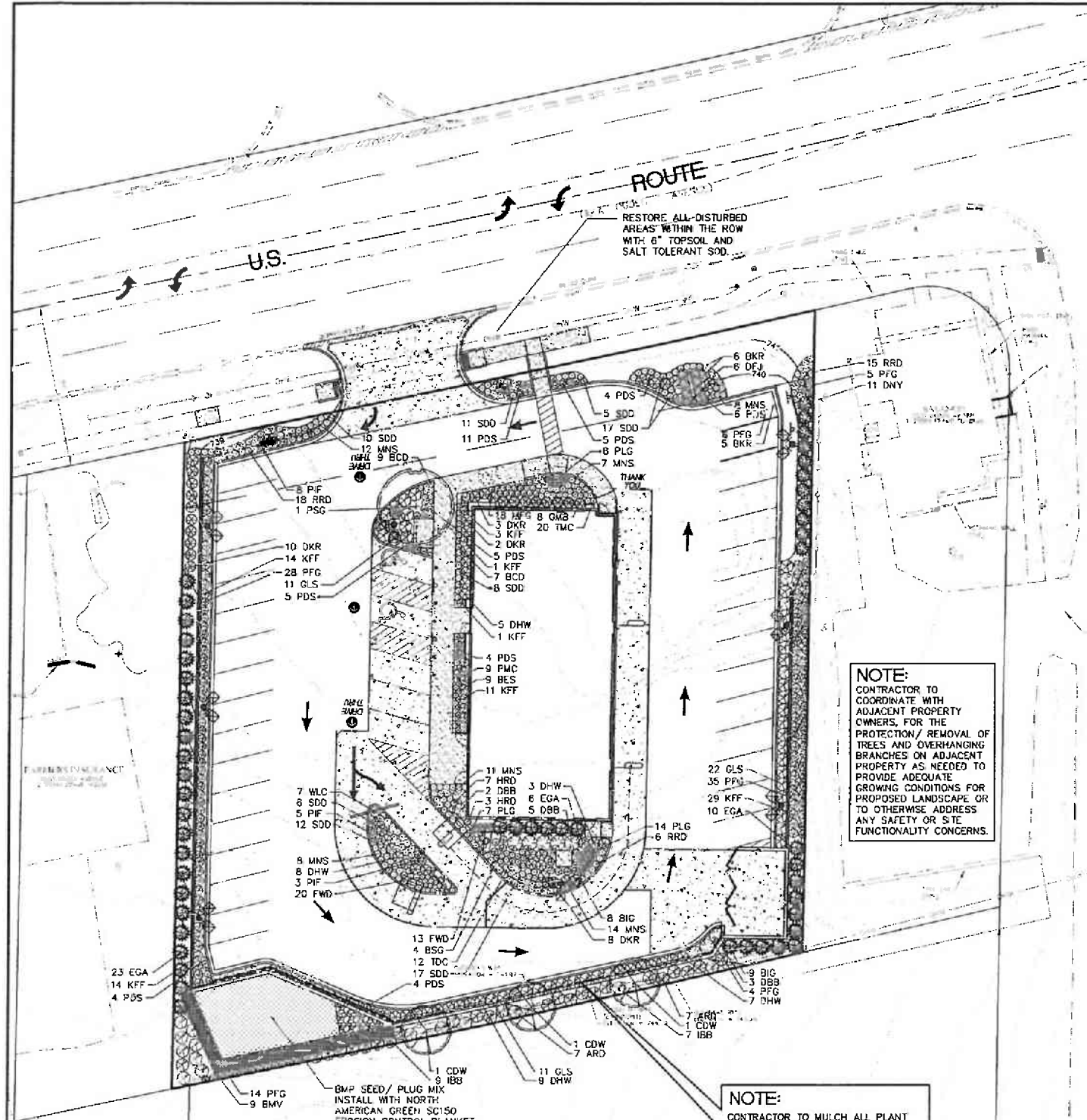
U.S.

BALANCE!
2525 OGDEN AVENUE

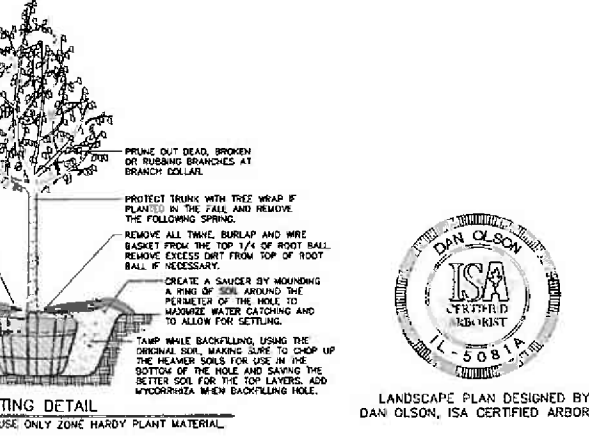
FARMERS INSURANCE
2539 OGDEN AVENUE

CROSS STREET





- ### LANDSCAPE NOTES
- ALL PLANT MATERIAL SHALL BE HARDY TO THE ZONE IT IS BEING PLANTED IN. ALL TREES AND SHRUBS ARE TO BE BALLED AND BURLAPED UNLESS OTHERWISE NOTED AND SHALL BE GROWN IN ACCORDANCE WITH THE STANDARDS SET FORTH BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION.
 - PLANT SIZES CALLED OUT ON THIS PLAN ARE THE MINIMUM SIZE REQUIRED. PLANTS WHICH FAIL TO MEET THE SIZES LISTED, SHALL BE REJECTED AT THE EXPENSE OF THE CONTRACTOR.
 - CONTRACTOR MUST VERIFY ALL MATERIAL QUANTITIES AS DEPICTED ON THE DRAWING. THE PLANT LIST PROVIDED ON THIS PLAN IS FOR CONVENIENCE ONLY.
 - SUBSTITUTIONS MAY NOT BE MADE WITHOUT THE APPROVAL OF THE LANDSCAPE ARCHITECT/DESIGNER.
 - THE CONTRACTOR SHALL NOTIFY ALL APPROPRIATE AGENCIES AND UTILITY LOCATIONS PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOT BEGIN ANY WORK ON-SITE UNTIL ALL UTILITIES HAVE BEEN LOCATED. CONTRACTOR SHALL OBTAIN "AS-BUILT" PLANS FOR ALL IRRIGATION AND LIGHTING PRIOR TO CONSTRUCTION.
 - CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL UTILITIES INCLUDING IRRIGATION AND LIGHTING. ALL DAMAGE SHALL BE REPAIRED TO A NEW CONDITION IN ACCORDANCE WITH ALL CODES AT NO COST TO THE OWNER - SEE NOTE 5.
 - ALL UNSUITABLE MATERIAL (CONCRETE, CRUSHED ASPHALT, BRICK ETC.) SHALL BE REMOVED PRIOR TO PLANTING AND SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
 - A MIXTURE OF 30% MUSHROOM COMPOST, 10% SAND AND 60% TOPSOIL SHALL BE ROTOTILLED INTO ALL PERENNIAL, ANNUAL AND SHRUB PLANTING BEDS PRIOR TO THE INSTALLATION OF THE PLANT MATERIAL. A SLOW RELEASE, GRANULAR FERTILIZER SHALL BE APPLIED TO ALL ANNUAL AND PERENNIAL PLANTING BEDS AT THE RECOMMENDED RATE, AND SHALL BE ROTOTILLED IN WITH THE TOPSOIL AND MUSHROOM COMPOST MIXTURE BEFORE THE PLANT MATERIAL IS INSTALLED.
 - CONTRACTOR TO PROVIDE THOROUGH INITIAL WATERING OF ALL PLANTINGS WITHIN 12 HOURS OF INSTALLATION TO ENSURE ALL AIR POCKETS HAVE BEEN REMOVED AROUND ROOT BALL.
 - ALL PLANT BED AREAS ARE TO BE MULCHED WITH 3" OF DOUBLE SHREDDED HARDWOOD MULCH AND SHALL BE SEPARATED WITH A SPADE EDGE PERIMETERS ADJACENT TO TURF AREAS. FINAL GRADE (AFTER SETTLING) SHALL BE 1" BELOW ADJACENT CURBS.
 - ALL TURF AREAS ARE TO BE A MINIMUM OF A FIVE WAY BLUEGRASS BLEND, UNLESS OTHERWISE NOTED. CONTRACTOR IS RESPONSIBLE FOR WATERING ALL INSTALLED TURF AREAS UNTIL THE TURF SEED AND SOO OCCUR ON THE SAME PROJECT, CONTRACTOR SHALL VERIFY AND USE SEED MIXTURES TO MATCH SOO.
 - AREAS TO BE SOOED SHALL BE WITH AN "APPROVED TURFCORP SOO" OF PREMIUM GRADE. SOO SHALL BE A 5 WAY BLEND OF IMPROVED KENTUCKY BLUEGRASS VARIETIES THAT HAS BEEN GROWN LOCALLY TO THE PROJECT SITE. SOO MUST BE MATURED FOR 2 FULL GROWING SEASONS PRIOR TO HARVEST CUTTING AND BE HEALTHY WITH WELL ESTABLISHED ROOTS. SOO SHALL BE FREE OF DISEASE, PESTS AND DEBRIS. SOO SHALL BE UNIFORM IN LEAF COLOR, TEXTURE, AND DENSITY. SOO SHALL BE DELIVERED, INSTALLED, AND WATERED WITHIN 24 HOURS OF HARVEST IN WHICH TEMPERATURES DO NOT EXCEED 90 DEGREES (F) NOR LESS THAN 55 DEGREES (F). SOO SHALL BE MACHINE-CUT AT A MINIMUM UNIFORM SOIL THICKNESS (1.5" OF SOO IS DESIRED) BUT SOO THICKNESS SHALL BE A THICKNESS NECESSARY FOR PLANT VIABILITY. SOO SHALL BE LAID IN STAGGERED STRAIGHT LINES, TIGHTLY AGAINST EACH OTHER WITHOUT STRETCHING OR OVERLAPPING. SOO STAKES SHALL USED ON ALL SLOPES 4:1 OR GREATER.
 - THE EXISTING PLANT MATERIAL SHOWN ON THIS PLAN IS INTENDED SOLELY TO IDENTIFY THEM AS OBSERVED IN THE FIELD. THIS PLAN DOES NOT MAKE ANY CLAIMS ABOUT THE CONDITION OR SAFETY OF ANY OF THE PLANT MATERIAL DESCRIBED HEREIN OR OBSERVED IN THE FIELD.
 - ALL TRANSPLANTED PLANT MATERIAL MUST BE INSTALLED IMMEDIATELY UPON EXTRACTION FROM ITS ORIGINAL LOCATION, UNLESS SPECIFIC ARRANGEMENTS HAVE BEEN MADE WITH THE LANDSCAPE ARCHITECT/DESIGNER. SHOULD IT BECOME UNREASONABLE TO TRANSPORT ANY OF THE PLANT MATERIAL AS DESCRIBED IN THIS PLAN, DUE TO SITE CONSTRAINTS OR OTHERWISE, CONTRACTOR IS RESPONSIBLE FOR CONTACTING LANDSCAPE ARCHITECT/DESIGNER TO MAKE ALTERNATIVE ARRANGEMENTS.
 - CONTRACTOR IS RESPONSIBLE FOR ALL PLANT MATERIAL REMAINING PLUMB UNTIL THE END OF THE GUARANTEE PERIOD. PLANTS MAY NOT BE STAKED UNLESS APPROVED BY THE LANDSCAPE ARCHITECT/DESIGNER.
 - CONTRACTOR TO GUARANTEE PLANT MATERIAL AND LABOR FOR A MINIMUM OF ONE YEAR FROM THE TIME OF INSTALLATION.
 - THE CONTRACTOR IS RESPONSIBLE FOR BECOMING FAMILIAR WITH AND ABIDING BY THE LANDSCAPE ORDINANCES FOR THE SPECIFIC JURISDICTION IN WHICH THE WORK IS TAKING PLACE.
 - BIDDERS SHALL BE RESPONSIBLE FOR EXAMINING THE SITE TO BECOME FAMILIAR WITH THE SPECIFIC SITE CONSTRAINTS.
 - THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY FOR THE COMPLETION OF ALL THE ITEMS SHOWN ON THE PLANS.
 - IF IRRIGATION IS DEEMED NECESSARY, THE DESIGN AND INSTALLATION OF THE IRRIGATION SYSTEM SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
 - IF EXISTING IRRIGATION IS PRESENT ON SITE, CONTRACTOR SHALL ADJUST, ADD TO, OR SUBTRACT FROM THE EXISTING IRRIGATION SYSTEM TO ACCOMMODATE ANY PROPOSED ALTERATIONS/ADDITIONS TO THE EXISTING LANDSCAPING.
 - PROVIDE TOPSOIL RE-SPREAD PER THE FOLLOWING UNLESS OTHERWISE NOTED:
 - 4" MINIMUM IN GRASS OR SOO AREAS
 - 6" MINIMUM IN PLANTING AREAS
 - 12" MINIMUM IN LANDSCAPE ISLANDS



GENERAL NOTES:

- THESE PLANS ARE BASED ON THE ALTA/ACSM LAND TITLE AND TOPOGRAPHIC SURVEY (SURVEY PROJECT #13.0128.01 DATED 09/23/13) PREPARED BY: COMPASS SURVEYING LTD 2631 GINGER WOODS PARKWAY, STE 100, AURORA, IL 60502 (630) 820-9100
- FROM TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

ON SITE PARKING DATA

| | |
|-----------------------|----|
| REGULAR SPACES | 35 |
| ADA ACCESSIBLE SPACES | 2 |
| TOTAL SPACES | 37 |

PARKING REQUIREMENT:

15 SPACES/1,000 GROSS FLOOR AREA
 2,375 S.F./1,000*15 SPACES = 35.6 OR 36 SPACES
 37 PROPOSED SPACES > 36 REQUIRED SPACES

SITE DATA

| | |
|--------------------|-----------------------------|
| LOT AREA | 38,815 S.F. (0.89 AC.) |
| EX IMPERVIOUS AREA | 9,716 S.F. (0.22 AC.)(25%) |
| EX PERVIOUS AREA | 29,099 S.F. (0.67 AC.)(75%) |
| PR IMPERVIOUS AREA | 29,828 S.F. (0.68 AC.)(76%) |
| PR PERVIOUS AREA | 8,987 S.F. (0.21 AC.)(24%) |
| BUILDING AREA | 4,388 S.F. |
| F.A.R. | 0.11 |

NOTE:
 * PROPOSED IMPERVIOUS AREA (29,828 SF) MINUS EXISTING IMPERVIOUS AREA (9,716 SF) EQUALS 20,112 SF, WHICH IS LESS THAN 25,000 SF DIFFERENCE. THEREFORE NO STORMWATER STORAGE IS REQUIRED.
 * VOLUME CONTROL IS REQUIRED SINCE MORE THAN 2,500 SF OF AREA IS BEING PROPOSED TO BE DISTURBED. VOLUME CONTROL VOLUME REQUIRED EQUALS 1.25"/12" * 20,112 SF (INCREASED IMPERVIOUS AREA) EQUALS 2,095 CF. VOLUME CONTROL VOLUME PROVIDED EQUALS 865.15 SF (BASIN FOOT PRINT) * 2.50 FT (DEPTH TO BOTTOM OF WEIR) EQUALS 2,163 CF. VOLUME CONTROL REQUIREMENT IS MET.

PLANT LIST

SHADE, ORNAMENTAL AND CONIFEROUS TREES

| QTY. | ABRV. | BOTANICAL NAME | COMMON NAME | SIZE |
|------|-------|----------------------------------|--------------------------|--------|
| 1 | PSG | Ginkgo biloba 'Princeton Sentry' | Princeton Sentry Ginkgo | 2 1/2" |
| 3 | CDW | Camus mas | Camellia-Cherry Dogwood | 6' |
| 4 | EGA | Taxus occidentalis 'Emerald' | Emerald Green Arborvitae | 6' |

FLOWERING AND EVERGREEN SHRUBS

| QTY. | ABRV. | BOTANICAL NAME | COMMON NAME | SIZE |
|------|-------|--------------------------------------|----------------------------------|--------|
| 16 | IBB | Aronia melanocarpa 'Worton' | Iriquois Beauty Black Chokeberry | 5 Gal. |
| 14 | ARD | Corrus sericea 'Alemton' | Alemton Compact Redosier Dogwood | 5 Gal. |
| 10 | DBB | Euonymus alatus 'Compactus' | Dwarf Burning Bush | 5 Gal. |
| 44 | GLS | Rhus aromatica 'Grow-Low' | Grow-Low Sumac | 5 Gal. |
| 23 | DKR | Rosa 'Rozdra' | Double Knock Out Rose | 5 Gal. |
| 6 | BKR | Rosa 'Rozdod' | Shrubbing Knockout Rose | 5 Gal. |
| 9 | BMV | Viburnum dentatum 'Christom' | Blue Muffin Viburnum | 5 Gal. |
| 32 | DHW | Weigela x 'Dark Horse' | Dark Horse Weigela | 5 Gal. |
| 8 | GMB | Boxus 'Green Mountain' | Green Mountain Boxwood | 5 Gal. |
| 6 | DFJ | Juniperus chinensis 'Daub's Frosted' | Daub's Frosted Juniper | 5 Gal. |
| 11 | DNY | Taxus x media 'Densiformis' | Densiformis Yew | 5 Gal. |

PERENNIALS, ORNAMENTAL GRASS AND GROUNDCOVERS

| QTY. | ABRV. | BOTANICAL NAME | COMMON NAME | SIZE |
|------|-------|--|----------------------------------|--------|
| 33 | FWD | Dianthus gratiopolitanus 'Firewitch' | Firewitch Dianthus | 1 Gal. |
| 9 | PMC | Echinacea 'CBB Cone 2' | Purple Meadowbrite Coneflower | 1 Gal. |
| 12 | TDC | Echinacea 'Tangerine Dream' | Tangerine Dream Coneflower | 1 Gal. |
| 29 | PLG | Eragrostis spectabilis | Purple Lovegrass | 1 Gal. |
| 18 | WFG | Cerastium sanguineum 'Max Frei' | Max Frei Bloody Cranesbill | 1 Gal. |
| 39 | RRD | Hemerocallis 'Rosy Returns' | Rosy Returns Daylily | 1 Gal. |
| 85 | SDD | Hemerocallis 'Stella de Oro' | Stella de Oro Daylily | 1 Gal. |
| 10 | HRD | Hemerocallis 'Happy Returns' | Happy Returns Daylily | 1 Gal. |
| 20 | TMC | Heuchera 'Tiramisu' | Tiramisu Coral Belts | 1 Gal. |
| 15 | BCD | Leucoanthemum x superbum 'Banana Cream' | Banana Cream Shasta Daisy | 1 Gal. |
| 7 | WLC | Nepeta racemosa 'Walker's Low' | Walker's Low Catmint | 1 Gal. |
| 4 | BSG | Panicum virgatum 'Heavy Metal' | Blue Switch Grass | 1 Gal. |
| 64 | PFG | Panicum virgatum 'Prairie Fire' | Prairie Fire Switch Grass | 1 Gal. |
| 15 | PIF | Pennisetum alopecuroides 'Piglet' | Piglet Fountain Grass | 1 Gal. |
| 5 | BES | Rudbeckia sulcatiflora 'Goldsturm' | Black Eyed Susan | 1 Gal. |
| 60 | MNS | Salvia nemorosa 'May Night' | May Night Salvia | 1 Gal. |
| 8 | BIG | Sorghastrum nutans 'Indian Steel' | Blue Indian Grass | 1 Gal. |
| 73 | KFF | Calamagrostis x acutiflora 'Kort Foerster' | Kort Foerster Feather Reed Grass | 1 Gal. |
| 48 | PDS | Sporobolus heterolepis | Prairie Dropseed | 1 Gal. |

NOTE:

CONTRACTOR TO COORDINATE WITH ADJACENT PROPERTY OWNERS, FOR THE PROTECTION/REMOVAL OF TREES AND OVERHANGING BRANCHES ON ADJACENT PROPERTY AS NEEDED TO PROVIDE ADEQUATE GROWING CONDITIONS FOR PROPOSED LANDSCAPE OR TO OTHERWISE ADDRESS ANY SAFETY OR SITE FUNCTIONALITY CONCERNS.

NOTE:

CONTRACTOR TO MULCH ALL PLANT BEDS WITH TWICE SHREDDED HARDWOOD BARK, DARK BROWN IN COLOR, AND SPADE EDGE WHERE APPLICABLE (TYP).

Call Before You Dig

ISA DAN OLSON CERTIFIED ARBORIST

LANDSCAPE PLAN DESIGNED BY DAN OLSON, ISA CERTIFIED ARBORIST

Scale: 1" = 20'

DATE: NOVEMBER 25, 2013

PROJECT NO.: 12-078

LANDSCAPE PLAN

DATE: 12/05/13

REVISIONS: PER VILLAGE REVIEW LETTER (DATED 11/01/13)

Prepared For: McDonald's 4320 Winfield Road, Suite 400 Warrenville, IL 60555

Prepared By: Watermark Engineering Resources, Ltd. 1861 Ginger Woods Parkway, Suite 100, Aurora, IL 60502 phone 630-215-1500 fax 630-232-9500 www.watermark-engineering.com

CHECKED BY: J. MILLER
 DESIGN BY: M. SPALLEY
 DRAWN BY: M. BAILEY
 DATE: NOVEMBER 25, 2013
 SCALE: 1" = 20'
 PROJECT NO.: 12-078

LC #12 2554

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VILLAGE OF DOWNERS GROVE
PLAN COMMISSION MEETING
PUBLIC HEARING

JANUARY 27, 2014, 7:00 P.M.

Chairwoman Urban called the January 27, 2014 meeting of the Plan Commission to order at 7:00 p.m. and led the Plan Commissioners and the public in the recital of the Pledge of Allegiance.

ROLL CALL:

PRESENT: Chairwoman Urban, Mr. Beggs, Mrs. Lupescu (ex-officio), Ms. Rabatah, Mr. Rickard, Mr. Webster

ABSENT: Mr. Cozzo, Mr. Matejczyk, Mr. Quirk, Mr. Waechtler

STAFF PRESENT: Senior Planner Stan Popovich

VISITORS: Tracy Kasson, Rathje Woodward, 300 E. Roosevelt Road, Wheaton, IL; Andrew Goodman. GMX Real Estate Group, 3000 Dundee Road, Northbrook, IL; Rick Dolan, McDonalds, 4320 Winfield Road, Warrenville, IL; Dennis Sommers, McDonalds, 4320 Winfield Road, Warrenville, IL; Jeff Miller, Watermark Engineering, 2631 Ginger Woods Parkway, Aurora, IL; Luay Aboona, KLOA, 9575 West Higgins Road, Rosemont, IL; Dean Przbyszewski, McDonalds, 4320 Winfield Road, Warrenville, IL; John Hajek, 4500 Cross Street

A brief review of the meeting's protocol followed.

MINUTES OF OCTOBER 28, 2013

Minutes of the October 28, 2013 were approved on motion by Mr. Beggs, seconded by Mrs. Rabatah. Motion carried unanimously by voice vote.

MINUTES OF NOVEMBER 4, 2013

Minutes of the November 4, 2013 meeting were approved on motion by Mr. Beggs, seconded by Mr. Rickard. Motion carried by voice vote. (Mrs. Rabatah abstains.)

FILE PC 45-13: A petition seeking approval of a Special Use for a drive-through facility for the construction of a new restaurant. The property is zoned B-3, General Services and Highway Business District and is located on the south side of Ogden Avenue approximately 60 feet west of Cross Street, commonly known as 2535 Ogden Avenue, Downers Grove, IL (PINs 08-01-305-006, -007, -008, -009). McDonald's USA, LLC., Petitioner; Scherston Real Estate Investments, LLC. Owner.

Chairwoman Urban swore in those individuals who would be speaking on the above petition. (Mrs. Lupescu recused herself from the discussion and left meeting.)

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Senior Planner Stan Popovich reviewed the petition before the commissioners noting it was a request for a special use for a 24-hour drive-through restaurant located at 2535 Ogden Avenue. The property was zoned B-3, as were other surrounding businesses. McDonalds, the petitioner, was requesting to demolish their existing restaurant at 1620 Ogden Avenue and was proposing to construct a new drive-through restaurant at 2535 Ogden Avenue. Mr. Popovich drew the commissioners' attention to the fact that there were four lots of record and a lot consolidation would be necessary if the proposal was approved. Elevations of the proposed building and site plan were depicted on the overhead.

Proposed was an approximate 4,400 sq. foot restaurant in the center of the site with a single access point on Ogden Avenue (one lane in and two lanes out). The Illinois Dept. of Transportation approved the access and curb cut location. The drive-through pick-up windows would be located on the east side of the building with side by side drive-through lanes to the southwest of the building, a counter-clockwise circulation pattern and 37 parking spaces (36 were required) are provided. Trash enclosure, monument sign, perimeter landscaping and a proposed six-foot wooden fence (along rear property line) were pointed out and a four-foot ornamental fences along the east and west property lines were noted. Trash pick-up and food deliveries would be held during normal business hours.

Per Mr. Popovich, the proposal met the village's Comprehensive Plan, the Ogden Avenue Corridor Plan, the Zoning Ordinance, and was consistent with criteria for redeveloping under-utilized commercial properties, as described within the Comprehensive Plan. Twenty percent green space would be provided, wherein ten percent was required. The photometric plan met the village's lighting requirement. All McDonalds signs would comply with the village's sign ordinance and a traffic study was conducted and provided to the commissions for their review. Mr. Popovich reviewed those traffic counts, noting the traffic consultant felt there would be minimal impact on the Ogden Avenue traffic pattern. A gap analysis for vehicle entry onto Ogden Avenue was also reviewed in further detail, with Mr. Popovich confirming that the traffic study indicated there was adequate gaps in the existing traffic flow for the site.

A depiction of a turning radius for emergency vehicles was provided as well as an exhibit reflecting delivery vehicles being able to make their turns on-site. On-site stormwater detention would not be required by the village because McDonalds was providing less than 25,000 square feet of net new impervious area. However, McDonalds was providing a volume control BMP in the southwest corner of the site in order to contain the first one-and one-quarter inch rainfall, which eventually would percolate out into the existing drainage pattern which flowed north to south. The proposal met the county's as well as the village's stormwater ordinance. Retaining walls were noted and the taller areas of the walls would be broken up with landscaping. Details followed. A sprinkler system and automatic alarm system were being required by the village.

Proper sign notification and public notice was provided for this proposal and a few inquiries were made into the proposal. However, per Mr. Popovich, the petitioner did hold two neighborhood meetings and the main concerns were noise, strewn garbage, truck deliveries, and whether the site was appropriate or not. All issues were addressed in the petitioner's proposal.

Mr. Popovich noted the four Standards of Approval for a special use were met and he reviewed each one in more detail for the commissioners. Staff recommended the Plan Commission forward a positive recommendation with the six conditions listed in staff's report.

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Per a question, Mr. Popovich confirmed that sidewalks would be installed around the site, including walks with handicap accessibility.

Mr. Tracy Kasson, attorney with Rathje Woodward, 300 E. Roosevelt Road, Wheaton, IL on behalf of the petitioner, referenced his development team and representatives from McDonalds and commented that the overall proposal met or exceeded many of the village's standards, citing some examples. He was available to answer questions and asked for the commissioners' support.

Chairwoman Urban opened up the meeting to public comment.

Mr. John Hajek, 4500 Cross Street, Downers Street confirmed he attended the two prior neighborhood meetings and believed the proposal was going to impact his neighborhood with traffic flow since it was the last residential neighborhood west of Belmont. He was also concerned about decreased property values and the constant lighting coming from Ogden Avenue at all hours.

Mr. Tracy Kasson, in response to the above resident, indicated that the area south of the proposal was zoned B-3 and from the southern portion of the petitioner's property line to Mr. Hajek's home was almost 300 feet. He believed there would be no adverse effects to Mr. Hajek or any of the residents, noting the space between could provide for another commercial space to the south even before the residential districts began. Mr. Kasson reiterated that the site was zone B-3 for a long time and was even in the Comprehensive Plan. He did not believe the special use would have any adverse impacts that would be any different at this location than anywhere else located in the B-3 district. Lighting and noise issues were addressed through the ordinance standards and the proposal met or exceeded those standards. Lighting shields would be provided etc. He reiterated that enough traffic gaps existed, as defined in the consultant's traffic report, and there would be no impact on Cross Street since there was no access to Cross Street. The two lane drive-through was most efficient because it resulted in better vehicle stacking and getting food quicker.

Chairwoman Urban opened up the meeting to commissioner comments/questions. Mr. Rickard asked for clarification of the grade elevation difference between the pavement at the south end of the property to south property line, wherein Mr. Kasson stated it was ten feet.

Drawing attention to the west side of the building where parking exists, including the two handicap spaces, Mrs. Rabatah inquired if there was concerns with vehicles backing up into the stacking lane.

Mr. Rick Dolan, McDonalds construction supervisor, 4320 Winfield Road, Warrenville, explained that when allocating spaces for ADA parking, he is required by law to place the parking stalls as close to the building as possible, and having the tandem drive-through lanes should alleviate the need for additional queuing for drive-through customers and they should not back up as far as the handicap stalls.

Mr. Dean Przbyszewski, area supervisor for McDonalds, 4320 Winfield Road, Warrenville, also confirmed there were many drive-through layouts but with the tandem drive-through, it reduced the order time of the customer by half the time and with the proposed layout, Mr. Przbyszewski explained that it could stack five cars before they would even block the first parked car, and many people, through his observation, will let a vehicle back out. However, if that were to become an issue during lunch he stated an employee would be placed outside and used as a traffic guide.

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Per Mr. Beggs's questions about the neighborhood meetings, Mr. Andrew Goodman, principal with GMX Real Estate Group, 3000 Dundee Road, #408, Northbrook, Illinois stated a neighborhood meeting was set up on Tuesday, November 26, 2013 at the Downers Grove Recreation Center. Certified letters were sent to same individuals as for the public hearing, with six attendees. A few residents were inquisitive while a few residents were opposed to the proposal and one individual was very supportive of the proposal, stating it would improve the area.

Mr. Kasson closed and emphasized that because the proposal met the village's standards and ordinance requirements, because it complied with the comprehensive plan and would not have any adverse impact on the adjoining area and surrounding neighborhood, and because no variation was being requested, he looked forward to developing the site and providing an asset to the community.

Chairwoman Urban closed the public hearing and invited commissioners to deliberate.

Mr. Quirk concurred with staff's conclusion and believed the proposal was appropriate for the parcel and met the Comprehensive Plan's goals. Other commissioners concurred, but Mr. Beggs added his observations of the parcel over the years, the fact that he had concerns about left-turns on Ogden Avenue from Williams to I-355, the fact that traffic along that stretch of road was fairly even, and the fact that traffic would probably impact the neighborhood but not necessarily unfavorably, since McDonalds was another business that was patronized by many individuals. Overall, he believed the proposal would be an improvement for the area and by the lack of residents showing up opposing the proposal, he believed it was a proper request.

WITH REGARD TO PC 45-13 MR. BEGGS MADE A MOTION THAT THE PLAN COMMISSION FORWARD A POSTIVE RECOMMENDATION TO THE VILLAGE COUNCIL, INCLUDING THE SIX CONDITIONS LISTED IN STAFF'S REPORT:

- 1. THE PROPOSED SPECIAL USE REQUEST FOR A RESTAURANT WITH A DRIVE-THROUGH USE SHALL SUBSTANTIALLY CONFORM TO THE PRELIMINARY ENGINEERING PLANS FOR MCDONALD'S DOWNERS GROVE, PREPARED BY WATERMARK ENGINEERING RESOURCES LTD. DATED OCTOBER 28, 2013, LAST REVISED DECEMBER 5, 2013, THE FREESTANDING SIGN EXHIBIT PLAN AND WALL SIGNAGE EXHIBIT PLAN PREPARED BY WATERMARK ENGINEERING RESOURCES LTD DATED OCTOBER 28, 2013, LAST REVISED DECEMBER 5, 2013, PROPOSED BUILDING ELEVATION PLANS, PREPARED BY CORE STATES GROUP, DATED MARCH 21, 2013, PROPOSED BUILDING FLOOR PLAN PREPARED BY CORE STATES GROUP, DATED MARCH 21, 2013, PERVIOUS/IMPERVIOUS EXHIBIT, TRUCK CIRCULATION PLAN, FIRE TRUCK CIRCULATION PLAN AND TRASH ENCLOSURE EXHIBIT DATED DECEMBER 5, 2013 ATTACHED TO THIS REPORT EXCEPT AS SUCH PLANS MAY BE MODIFIED TO CONFORM TO VILLAGE CODES, ORDINANCES, AND POLICIES;**
- 2. THE PROPERTY CONSISTS OF FOUR LOTS OF RECORD. THE FINAL PLAT OF SUBDIVISION FOR LOT CONSOLIDATION OF THE PROPERTY INTO ONE NEW LOT MUST BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF THE BUILDING PERMIT;**
- 3. THE APPROVAL FROM IDOT, DUPAGE COUNTY HEALTH DEPARTMENT, DOWNERS GROVE SANITARY DISTRICT AS WELL AS A COPY OF THE PAID**

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RECEIPT FOR THE DUPAGE COUNTY IMPACT FEES WILL HAVE TO BE SUBMITTED PRIOR TO ISSUANCE OF THE BUILDING PERMIT;

- 4. THE PROPERTY MUST MEET ALL REQUIREMENTS OF THE SIGN ORDINANCE;**
- 5. THE SPEAKER VOLUME ON THE DRIVE-THROUGH WINDOW MUST BE REDUCED DURING OVERNIGHT HOURS; AND**
- 6. THE BUILDING SHALL BE EQUIPPED WITH AN AUTOMATIC SUPPRESSION SYSTEM AND AN AUTOMATIC AND MANUAL FIRE ALARM SYSTEM.**

SECONDED BY MR. RICKARD.

Chairwoman Urban shared her comments stating that given the grading in the area and the concerns with neighborhood streets, she did not believe cross-access, from a site constraint perspective, would be an issue. If there was cross-access, however, she believed it would encourage drivers to go to the neighborhood streets because it would be an easier option as opposed to Ogden Avenue. As it was, IDOT had strict review standards to be able to get a full access point. Additionally, she noted the zoning was appropriate, the standards for approval were met (as indicated by staff and by the petitioner), and the signage package was tasteful.

ROLL CALL:

AYE: MR. BEGGS, MR. RICKARD, MRS. RABATAH, MR. WEBSTER, CHAIRWOMAN URBAN

NAY: NONE

MOTION CARRIED. VOTE: 5-0.

THE MEETING WAS ADJOURNED AT 7:40 P.M. ON MOTION BY MR. WEBSTER, SECONDED BY MRS. RABATAH. MOTION CARRIED UNANIMOUSLY BY VOICE VOTE OF 5-0.

/s/ Celeste K. Weilandt

Celeste K. Weilandt

(As transcribed by MP-3 audio)