VILLAGE OF DOWNERS GROVE
Report for the Village Council Meeting
10/20/2015

SUBJECT:	SUBMITTED BY:
Special Use - 2212 Ogden Avenue	Stan Popovich, AICP Director of Community Development

SYNOPSIS

An ordinance has been prepared to permit an automobile fueling station as a Special Use at 2212 Ogden Avenue.

STRATEGIC PLAN ALIGNMENT

The goals for 2011-2018 include *Strong and Diverse Local Economy.*

FISCAL IMPACT

n/a.

RECOMMENDATION

Approval on the November 3, 2015 active agenda per the Plan Commission's unanimous 6:0 positive recommendation. The Plan Commission found that the proposal is an appropriate use in the district, compatible with the Comprehensive Plan and meets all standards for approval of a Special Use per Section 28.12.050 of the Zoning Ordinance.

BACKGROUND

Property Information & Zoning Request

The subject property is located on the west side of Finley Road between Ogden Avenue and Warrenville Road and is zoned B-3, General Services and Highway Business. The subject property is currently improved with a 1,100-square-foot convenience store, tunnel car wash, and fuel dispensers. The proposed redevelopment would demolish the car wash and existing convenience store to construct a new 3,500-square-foot convenience store, adjust the location of the fuel dispensers, reduce the canopy width, and convert two full access curb cuts into limited access curb cuts.

Compliance with the Comprehensive Plan

The Comprehensive Plan designates the property as Corridor Commercial, which includes a blend of commercial retail, office, regional commercial retail, service and multi-family uses. The proposed fueling station and convenience store are consistent with the auto orientation of the corridor and nearby automobile uses. The proposed fueling station use supports the Plan's goal for a diversified commercial corridor along Ogden Avenue. The proposal is consistent with the Comprehensive Plan and complements the use of the surrounding properties.

Compliance with the Zoning Ordinance
The subject property is zoned B-3, General Services and Highway Business. The proposed development includes redevelopment of a fueling station, the use of which is listed as an allowable Special Use in the B-3 zoning district. The proposal will comply with all regulations of the Zoning Ordinance. This will increase the amount of landscaped open space and will contribute to the aesthetic enhancement of the Ogden Avenue corridor. The proposal is in compliance with the Zoning Ordinance.

Traffic and Circulation
Site circulation will be modified by restricting the easternmost curb cuts on Ogden Avenue and Warrenville Road to single-access curb cuts. The Ogden Avenue curb cut will be in-bound only while the Warrenville Road curb cut will be out-bound only. This allows the fuel tanker trucks the ability to continue using these access points to refill the underground storage tanks while reducing the number of full access curb cuts, increasing patron safety. IDOT has given preliminary approval of these changes along Ogden Avenue, as has DuPage County for Warrenville Road.

Public Improvements
An administrative lot consolidation will be required in order to consolidate portions of lots previously developed. As part of the administrative lot consolidation, the petitioner will dedicate approximately 2,590 square feet of their property along Ogden Avenue to IDOT for use as public right-of-way and will also grant an easement to the Village over the existing stormwater detention basin. With a decrease in impervious area, no additional on-site stormwater detention is required.

Public Comment
One member of the public asked if stormwater calculations were evaluated and addressed. Stormwater calculations have been prepared, reviewed and have been found to be in conformance with the Village's Stormwater and Floodplain Ordinance.

ATTACHMENTS

Ordinance
Aerial Map
Staff Report with attachments dated October 5, 2015
Draft Minutes of the Plan Commission Hearing dated October 5, 2015

ORDINANCE NO. _____

AN ORDINANCE AUTHORIZING A SPECIAL USE
FOR 2212 OGDEN AVENUE TO PERMIT AN AUTOMOBILE FUELING STATION

WHEREAS, the following described property, to wit:

Parcel 1:
That part of the southeast 1/4 Section 1, Township 38 North, Range 10, East of the Third Principal Meridian, described as: beginning at the point of intersection of the northerly line of Ogden Avenue (U.S. Route No. 34), with the westerly line of Belmont Road, as dedicated by document R61-28016; thence in a southwesterly direction along the northerly line of Ogden Avenue (U.S. Route No. 34), a distance of 150.00 feet; thence in a northerly direction 189.357 feet to a point on the southerly line of Warrenville Road, said point being 150.00 feet northwesterly from the point of intersection of the southerly line of Warrenville Road, with the westerly line of Belmont Road, as dedicated by document R61-28016; thence in a southeasterly direction along the southerly line of Warrenville Road, a distance of 150.00 feet to the westerly line of Belmont Road, as dedicated by document R61-28016; thence southerly along the westerly line of Belmont Road, a distance of 137.608 feet to the point of beginning, in DuPage County, Illinois.

Parcel 2:
Lot 1 (except the west 471.00 feet thereof, as measured along the north line of Ogden Avenue and parallel to the west line of said Lot 1) in Rosen's Ogden Avenue Subdivision of part of the southeast 1/4 of Section 1, Township 38 North, Range 10, East of the Third Principal Meridian, according to the plat thereof recorded January 29, 1964 as document R64-02998, in DuPage County, Illinois.

Commonly known as 2212 Ogden Avenue, Downers Grove, IL 60515 (PINs 08-01-402-006; -007; -008)

(hereinafter referred to as the "Property") is presently zoned in the *"B-3, General Services and Highway Business District"* under the Comprehensive Zoning Ordinance of the Village of Downers Grove; and

WHEREAS, the owner of the Property has filed with the Plan Commission, a written petition conforming to the requirements of the Zoning Ordinance, requesting that a Special Use per Section 28.5.010 of the Zoning Ordinance be granted to allow an automobile fueling station; and,

WHEREAS, such petition was referred to the Plan Commission of the Village of Downers Grove, and said Plan Commission has given the required public notice, has conducted a public hearing respecting said petition on October 5, 2015 and has made its findings and recommendations, all in accordance with the statutes of the State of Illinois and the ordinances of the Village of Downers Grove; and,

WHEREAS, the Plan Commission has recommended approval of the Special Use, subject to certain conditions; and,

WHEREAS, the Village Council finds that the evidence presented in support of said petition, as stated in the aforesaid findings and recommendations of the Plan Commission, is such as to establish the following:

1. That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;

2. That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.

3. That the proposed use will not, in this particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.

NOW, THEREFORE, BE IT ORDAINED by the Council of the Village of Downers Grove, in DuPage County, Illinois, as follows:

SECTION 1. That Special Use of the Property is hereby granted to allow an automobile fueling station.

SECTION 2. This approval is subject to the following conditions:

1. The proposed Special Use request to redevelop the existing fueling station shall substantially conform to the Site Plan, Truck Access and Floor Plans prepared by Ambrose Design Group, LLC, dated October 15, 2014, last revised on September 15, 2015; architectural elevations prepared by F.A. Ross Architecture dated June 4, 2015, attached to this report except as such plans may be modified to conform to Village codes, ordinances, and policies.

2. All proposed signs shall conform to the Village's Sign Ordinance.

3. An administrative lot consolidation shall be prepared that dedicates additional IDOT right-of-way and grants an easement to the Village over the existing stormwater management basin.

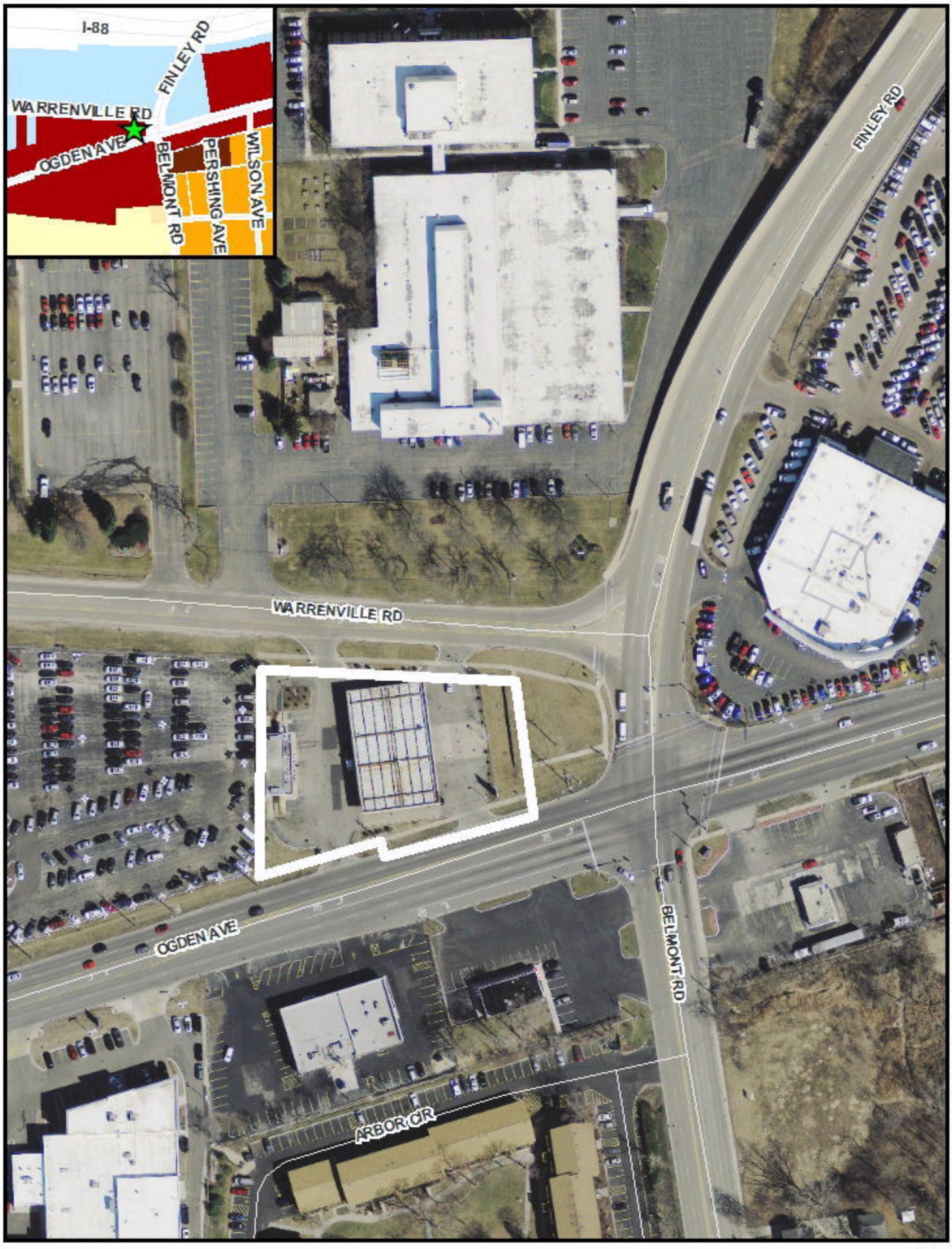
SECTION 3. The above conditions are hereby made part of the terms under which the Special Use is granted. Violation of any or all of such conditions shall be deemed a violation of the Village of Downers Grove Zoning Ordinance, the penalty for which may include, but is not limited to, a fine and/or revocation of the Special Use granted herein.

SECTION 4. That all ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

_____
Mayor

Passed:
Published:
Attest: _____
Village Clerk

1\wp8\ord.15\SU-2212-Ogden-15-PLC-0017



2212 Ogden Avenue - Location Map

VILLAGE OF DOWNERS GROVE
REPORT FOR THE PLAN COMMISSION
OCTOBER 5, 2015 AGENDA

SUBJECT:	TYPE:	SUBMITTED BY:
15-PLC-0017 2212 Ogden Avenue	Special Use for a Fueling Station	Rebecca Leitschuh, AICP Senior Planner

REQUEST
The petitioner is requesting Special Use approval to redevelop the existing Shell gas station at 2212 Ogden Aveune.

NOTICE
The application has been filed in conformance with applicable procedural and public notice requirements.

GENERAL INFORMATION

OWNER: TrueNorth Energy, LLC
10346 Brecksville Road
Brecksville, OH 44141

APPLICANT: Ambrose Design Group, LLC
P.O. Box 1870
Crystal Lake, IL 60039

PROPERTY INFORMATION

EXISTING ZONING: B-3, General Services & Highway Business
EXISTING LAND USE: Fueling Station
FUTURE LAND USE: Corridor Commercial
PROPERTY SIZE: 42,538 square feet (0.97 acres)
PINS: 08-01-402-006, -007, -008

SURROUNDING ZONING AND LAND USES

	ZONING	FUTURE LAND USE
NORTH:	M-1, Light Manufacturing	Office/Corporate Campus
SOUTH:	B-3, General Services and Highway Business	Corridor Commercial
EAST:	B-3, General Services and Highway Business	Corridor Commercial
WEST:	M-1, Light Manufacturing	Corridor Commercial

ANALYSIS

SUBMITTALS
This report is based on the following documents, which are on file with the Department of Community Development and attached to the staff report as indicated:

1. Application/Petition for Public Hearing

2. Location Map
3. Project Summary/Narrative
4. Site Photos
5. ALTA/ACSM Land Title Survey
6. Site Plan
7. Architectural Plans
8. Traffic and Parking Study

PROJECT DESCRIPTION

The petitioner is proposing to construct a new 3,500-square foot convenience store and make other site improvements at the Shell Gas Station located on the west side of Finley Road between Ogden Avenue and Warrenville Road. The subject property is commonly known as 2212 Ogden Avenue, which is zoned B-3, General Services and Highway Business. The petitioner is requesting Special Use approval to redevelop the existing gas station.

Existing Conditions

The subject property is currently improved with a 1,100 square foot convenience store, tunnel car wash, 14 fuel dispensers, a canopy that covers the fuel pumps and convenience store, surface parking and a detention basin. There are four full access curb cuts on the subject property, two from Ogden Avenue and two from Warrenville Road.

Proposed Development

The proposed redevelopment includes demolishing the car wash and existing convenience store to construct a new 3,500 square foot convenience store, relocate four fuel dispensers, install two new fuel dispensers, and convert two full access curb cuts into limited access curb cuts. The new convenience store will be located along the rear (west) property line and the canopy will be reduced with the redistribution of some of the fuel dispensers. While most of the fuel dispensers will remain unchanged, four dispensers will be relocated and two new dispensers added as a result of the demolition of the existing convenience store. The circulation pattern will be modified slightly due to restricting access at the easternmost curb cuts on Ogden Avenue and Warrenville Road. The Ogden Avenue curb cut will be converted to a right-in only curb cut, while the Warrenville Road curb cut will be modified to right-out only. These conversions were made to in an effort to reduce the number of access points along both Ogden Avenue and Warrenville Road while maintaining access to the existing fuel tanks for fuel deliveries.

COMPLIANCE WITH THE COMPREHENSIVE PLAN

The Comprehensive Plan designates the property as Corridor Commercial, which includes a blend of commercial retail, office, regional commercial retail, service and multi-family uses. The Comprehensive Plan notes that Ogden Avenue is an auto-oriented corridor. The proposed fueling station and convenience store is consistent with the auto orientation of the corridor and nearby automobile uses, including the adjacent automobile dealerships. The proposed fueling station use supports the Plan's goal for a diversified commercial corridor along Ogden Avenue. The proposal is consistent with the Comprehensive Plan and complements the use of the surrounding properties.

COMPLIANCE WITH THE ZONING ORDINANCE

The subject property is zoned B-3, General Services and Highway Business. The proposed development includes redevelopment of a fueling station, the use of which is listed as an allowable Special Use in the B-3 zoning district.

The proposal will comply with all regulations of the Zoning Ordinance. The characteristics of the proposed development are outlined in the table below:

2212 Ogden Avenue	Required	Proposed
Street Setback (Ogden)	75 ft. from centerline	92.92 ft
Street Setback (Warrenville)	25 ft.	75 ft.
Rear Setback	0 ft.	5.15 ft
Canopy Setback (Ogden)	50 ft. from centerline or 8 ft. from property line	59.25 ft. from centerline and 9.25 ft (no change)
Canopy Setback (Warrenville)	20 ft.	19.42 ft.. (no change)
Height	60 ft.	20.29 ft.
FAR	.75 max. (33,802 sq. ft.)	0.08 (3,500 sq. ft.)
Open Space - Total	10 % max. (4,507 sq. ft.)	28.6 % (12,868 sq. ft.)
Open Space – Street Yard	5 % max. (2,254 sq. ft.)	28.6 % (12,868 sq. ft.)
Parking Setback – Street Yard (Ogden)	50 ft. (from centerline of Ogden Ave.)	65 ft.
Parking Setback – Street Yard (Warrenville)	25 ft.	25 ft.
Parking	14 total (1 per pump island plus 3.33/1,000 sq. ft. of retail)	14 total
Accessible Parking	1	1
Stacking	32 (2 per pump, per side)	32

An administrative lot consolidation will be required in order to consolidate portions of lots previously developed. As part of the administrative lot consolidation, the petitioner will be dedicating additional right-of-way and granting an easement to the Village over the existing detention basin. Where new, the petitioner will provide site lighting in accordance with Section 10.030 of the Zoning Ordinance. All signage will be required to comply with the sign regulations.

Site circulation will be modified with the restricted right-in at the easternmost curb cut on Ogden Avenue and a restricted right-out at the easternmost curb cut on Warrenville Road. The new curbs will be rolled curbs to allow the fuel tanker trucks the ability to use these access points to refill the underground storage tanks. The main access point for patrons is the western curb cuts, which are being shifted east to provide better alignment with the new site layout. IDOT has given preliminary approval of these changes along Ogden Avenue, as has DuPage County for Warrenville Road.

TRAFFIC AND CIRCULATION

The applicant completed a traffic impact study for the proposed redevelopment. The study finds that the proposed site improvements will have no significant impact on the adjacent roadways or the function of the site as a whole. The study notes the larger convenience store will not increase traffic as the majority of trips to the convenience store are made by people already traveling the area roadways and many convenience store stops are made in conjunction with a fueling stop as well.

The study found that the modification to the two easternmost curb cuts (one along Ogden Avenue and one along Warrenville Road) will not impact the flow of traffic out of site or have a negative impact on either street. The elimination of the outbound turning movement on the easternmost Ogden Avenue curb cut may increase safety as eastbound traffic will not have to cross two lanes of traffic and a northbound turn lane. The full access curb cuts will continue to experience exit delays as is common along the Ogden Avenue corridor and other significant arterial streets in the Chicagoland region.

ENGINEERING/PUBLIC IMPROVEMENTS

The existing utilities servicing the development are sufficient for the proposed fueling station. The Downers Grove Sanitary District has provide conceptual approval for the project. The petitioner will be

dedicating approximately 2,590 square feet of their property along Ogden Avenue to IDOT for use as public right-of-way. This segment is the eastern 150 feet of the property along Ogden Avenue. The dedication will provide a consistent right-of-way width along this portion of Ogden Avenue.

Stormwater management for the property is managed via the existing detention basin at the eastern edge of the property. With a decrease in impervious area, no additional on-site stormwater detention is required. The proposed fueling station will comply with all provisions of the Stormwater and Flood Plain Ordinance, which will be reviewed during the building permit process.

PUBLIC SAFETY REQUIREMENTS

The Fire Prevention Division of the Fire Department has reviewed the proposed plans and has adequate access to the redeveloped fueling station. The proposed building will be required to be fully sprinkled and equipped with a manual and automatic fire alarm system.

NEIGHBORHOOD COMMENT

Notice was provided to all property owners 250 feet or less from the property line in addition to posting the public hearing sign and publishing a legal notice in *Downers Grove Suburban Life*. Staff has received no inquiries or concerns.

FINDINGS OF FACT

The petitioner is requesting a Special Use to operate a fueling station in the B-3 zoning district. Staff finds the proposal meets the standards for granting a Special Use as outlined below:

Section 28.12.050.H Approval Criteria – Special Uses

No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:

1. *That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;*
   The zoning of this property is B-3, General Services and Highway Business. Under Section 5.010 of the Zoning Ordinance, fueling station uses are listed as an allowable Special Use in the B-3 zoning district. This criteria is met.

2. *That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*
   The proposed fueling station is desirable to provide a service that is in the interest of public convenience and will contribute to the general welfare of the community. The proposed use is consistent with the goals of the Comprehensive Plan. The fueling station will provide a service to the community and travelers along Ogden Avenue. The fueling station supports a diversified Ogden Avenue commercial corridor and provides for both the daily needs of residents as well as providing commercial services to the larger region. The proposed use supports the auto-orientation of the Ogden Avenue corridor. This criteria is met.

3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*
   The proposed use is similar to other automobile uses along the Ogden Avenue corridor that are not detrimental to the health, safety or general welfare of the surrounding properties. The fueling station is a complimentary use to other similar auto uses along Ogden Avenue. Additionally, modifying the existing easternmost curb cuts along Ogden Avenue and Warrenville Road may increase the safety of persons

patronizing the fueling station and those traveling along either road. The fueling station will not be injurious to property values in the area. This criteria is met.

RECOMMENDATION

The proposed Special Use for the redevelopment of the fueling station is compatible with surrounding zoning and land use classifications, meets the criteria for Special Uses and is consistent with the Comprehensive Plan. Based on the findings listed above, staff recommends that the Plan Commission make a positive recommendation to the Village Council regarding 15-PLC-0017 subject to the following conditions:

1. The proposed Special Use request to redevelop the existing fueling station shall substantially conform to the Site Plan, Truck Access and Floor Plans prepared by Ambrose Design Group, LLC, dated October 15, 2014, last revised on September 15, 2015; architectural elevations prepared by F.A. Ross Architecture dated June 4, 2015, attached to this report except as such plans may be modified to conform to Village codes, ordinances, and policies.
2. All proposed signs shall conform to the Village's Sign Ordinance.
3. An administrative lot consolidation shall be prepared that dedicates additional IDOT right-of-way and grants an easement over the existing stormwater management basin.

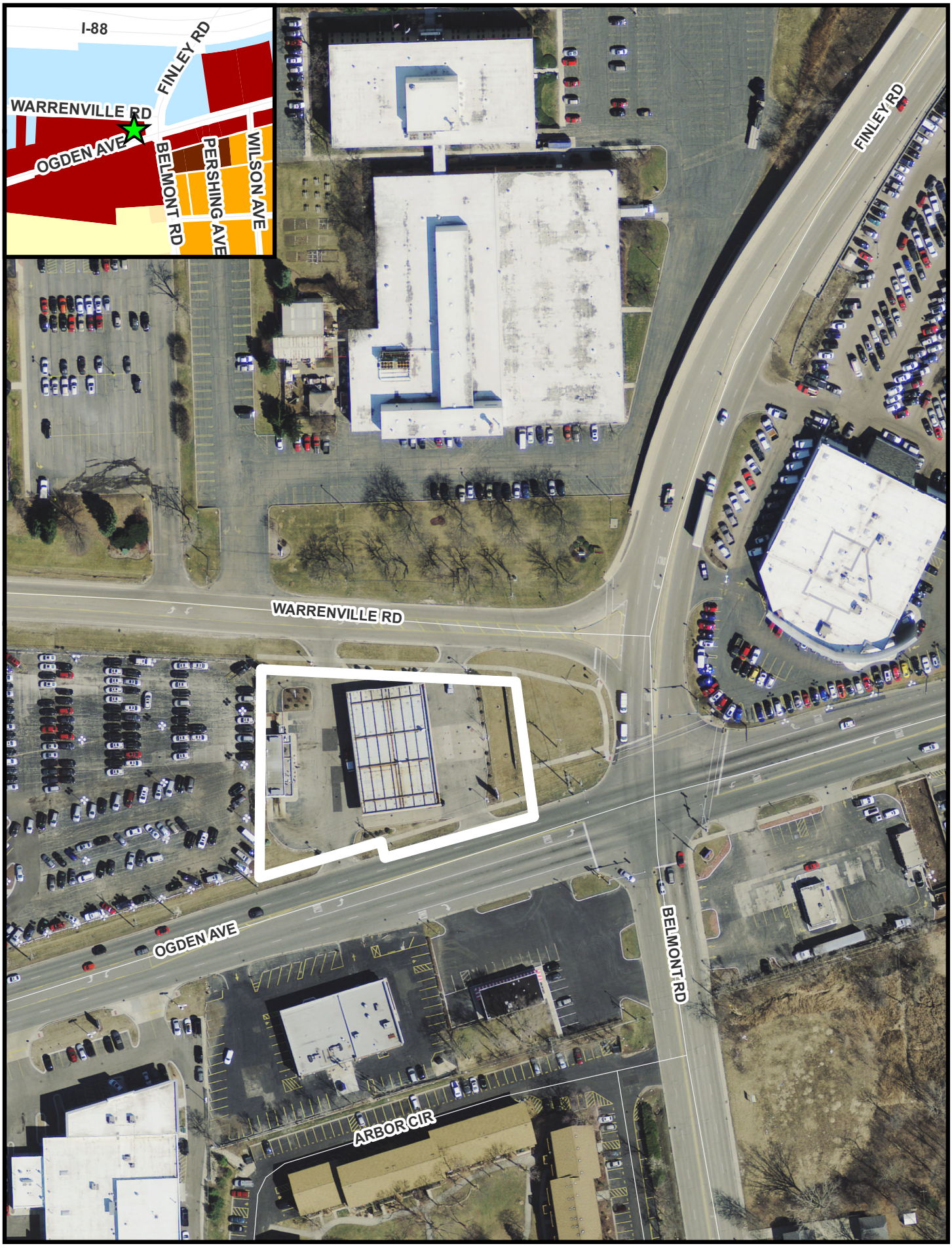
Staff Report Approved By:

_____

Stanley J. Popovich, AICP
Director of Community Development

SP:rl
-att

P:\P&CD\PROJECTS\PLAN COMMISSION\2015 PC Petition Files\15-PLC-0017 - 2212 Ogden Ave_Shell - SPU\Staff Report 15-PLC-0017.docx



0 37.5 75 150 Feet

2212 Ogden Avenue - Location Map

N

Ambrose Design Group, LLC

September 29, 2015

Village of Downers Grove
Community Development
801 Burlington Ave
Downers Grove, Illinois 60515

Project:
TrueNorth Energy, LLC
2212 Ogden Avenue
Downers Grove, Illinois

Department of Community Development;

TrueNorth is committed to enhancing the customer experience along with providing an improved site with accessible parking, a modern Convenience Store, large clean restroom facilities and ease of fueling. The existing site is outdated and not consistent with the surrounding Downers Grove community.

The site in its present level of development is not consistent with Truenorth's 21st Century image and Company direction. Today's developments address the needs of the modern consumer and integrate the site development and building to the current image of the city and surrounding community.

The existing building in its present location does not allow for renovation or alteration as it is situated under the gas canopy and in between the fueling aisles. The site access to the pump islands as well as the access for the tanker truck for fuel deliveries are the driving factors in a successful operation.

This proposal calls for the removal of both the existing 1100 sq. ft. Mart located under the gas canopy and the tunnel Car Wash building located on the west side of the property. We will rework the fuel islands after the removal of the existing Mart. There currently are eight fuel dispensers which provide a total of 14 fueling positions. The Canopy width will be reduced in size as part of the overall site update and remodel.

A new 3500 Sq. Ft. Convenience store providing all the amenities the modern consumer is seeking will be located in the approximate location where the Car Wash Building is currently situated. The building will be of masonry construction with brick on all (4) sides. A pitched roof with Architectural Laminated shingles will complete the buildings exterior. Parking spaces will be located adjacent to the front of the building providing ease of access for the customer. A total of 18 parking spaces are provided which is (5) more than the code requirement. As part of the design for the parking stalls we have moved the westernmost curb cuts on both Ogden and Warrenville Road to the east so they would be better positioned between the parking stalls and the fueling area. This overall placement of the building, drives and parking allowed us to provide large expanses of green space on either side of the new building.

The canopy and (4) of the outside fuel islands will remain in their current location. We will remove the two westernmost fuel islands and reposition those dispensers between the remaining outside fuel islands. This configuration will provide for (16) fueling positions. The canopy width will be reduced as we no longer need to extend to the west to cover islands that are being removed.

The two curb cuts on the east of the site are of critical importance to the functionality of the site. Those two drives serve as the entrance and exit for the Fuel Tanker. The underground storage tanks are located between these two driveways on the east side of the property. This allows the delivery of fuel not to interfere with customer fueling or customer access to the new Convenience Store. The tanker truck is generally on-site 3-4 times a week on an as needed basis. The deliveries are scheduled at non-peak times or when the site traffic is minimal. The tanks require yearly testing for compliance and maintenance on the tanks themselves is not required. Trash pickup will generally be scheduled in the morning after the peak of rush hour.

New site lighting will be installed in the existing lighting locations with an additional two new yard lights to be installed. These two new yard lights will be located in the area of the new building and will serve the parking area new driveway location on Ogden. The Canopy lighting is existing to remain with the exception of some modification where the canopy is being reduced.

The proposal for Special Use we are requesting is to improve a dated facility. We are not seeking any variances in setbacks or parking. This corner has operated as a Service Station for many years in this area and we would expect to do so for many more. This proposed use is in keeping with the allowable uses in the district where this project is located.

I believe in making the improvements we have mentioned we will continue to provide a service to the Downers Grove community that they have come to expect. These improvements will add greatly to the site and ultimately will contribute and be beneficial to the neighborhood.

The site improvements and amenities being proposed will not be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity. These improvements are being designed to enhance the corner and improve the sites appearance which in turn will benefit the surrounding area.

Respectfully Submitted,

Ronald J. Ambrose
Ambrose Design Group, LLC

1 TITLE DESCRIPTION

PARCEL 1:
THAT PART OF THE SOUTHEAST 1/4 OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS: BEGINNING AT THE POINT OF INTERSECTION OF THE NORTHERLY LINE OF OGDEN AVENUE (U.S. ROUTE NO. 34), WITH THE WESTERLY LINE OF BELMONT ROAD, AS DEDICATED BY DOCUMENT R61-28016; THENCE IN SOUTHWESTERLY DIRECTION ALONG THE NORTHERLY LINE OF OGDEN AVENUE (U.S. ROUTE NO. 34), A DISTANCE OF 150.00 FEET; THENCE IN A NORTHERLY DIRECTION 189.357 FEET TO A POINT ON THE SOUTHERLY LINE OF WARRENVILLE ROAD, SAID POINT BEING 150.00 FEET NORTHWESTERLY FROM THE POINT OF INTERSECTION OF THE SOUTHERLY LINE OF WARRENVILLE ROAD, WITH THE WESTERLY LINE OF BELMONT ROAD, AS DEDICATED BY DOCUMENT R61-28016; THENCE IN A SOUTHEASTERLY DIRECTION ALONG THE SOUTHERLY LINE OF WARRENVILLE ROAD, A DISTANCE OF 150.00 FEET TO THE WESTERLY LINE OF BELMONT ROAD, AS DEDICATED BY DOCUMENT R61-28016; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF BELMONT ROAD, A DISTANCE OF 137.608 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

PARCEL 2:
LOT 1 (EXCEPT THE WEST 471.00 FEET THEREOF, AS MEASURED ALONG THE NORTH LINE OF OGDEN AVENUE AND PARALLEL TO THE WEST LINE OF SAID LOT 1) IN ROSEN'S OGDEN AVENUE SUBDIVISION OF PART OF THE SOUTHEAST 1/4 OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 29, 1964 AS DOCUMENT R64-02998, IN DUPAGE COUNTY, ILLINOIS.

SURVEYOR'S DESCRIPTION:

THAT PART OF THE SOUTHEAST 1/4 OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS: BEGINNING AT THE POINT OF INTERSECTION OF THE NORTHERLY LINE OF OGDEN AVENUE (U.S. ROUTE NO. 34), WITH THE WESTERLY LINE OF BELMONT ROAD, AS DEDICATED BY DOCUMENT R61-28016; AND LOT 1 (EXCEPT THE WEST 471.00 FEET THEREOF, AS MEASURED ALONG THE NORTH LINE OF OGDEN AVENUE AND PARALLEL TO THE WEST LINE OF SAID LOT 1) IN ROSEN'S OGDEN AVENUE SUBDIVISION OF PART OF THE SOUTHEAST 1/4 OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 29, 1964 AS DOCUMENT R64-02998, IN DUPAGE COUNTY, ILLINOIS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE POINT OF INTERSECTION OF THE NORTHERLY LINE OF OGDEN AVENUE (U.S. ROUTE NO. 34), WITH THE WESTERLY LINE OF BELMONT ROAD, AS DEDICATED BY DOCUMENT R61-28016; THENCE ALONG THE NORTHERLY LINE OF SAID OGDEN AVENUE, SOUTH 75 DEGREES 07 MINUTES 00 SECONDS WEST, A DISTANCE OF 150.00 FEET; THENCE NORTH 05 DEGREES 18 MINUTES 03 SECONDS WEST, A DISTANCE OF 17.24 FEET TO THE SOUTHEAST CORNER OF SAID LOT 1 OF ROSEN'S OGDEN AVENUE SUBDIVISION; THENCE ALONG THE SOUTHERLY LINE OF SAID LOT 1, SOUTH 75 DEGREES 07 MINUTES 00 SECONDS WEST, A DISTANCE OF 121.01 FEET TO A POINT 471.00 FEET NORTHEASTERLY FROM THE SOUTHWEST CORNER OF SAID LOT 1; THENCE ALONG A LINE PARALLEL WITH THE WESTERLY LINE OF SAID LOT 1, NORTH 00 DEGREES 04 MINUTES 14 SECONDS EAST, A DISTANCE OF 211.25 FEET TO A POINT ON THE SOUTHERLY RIGHT OF WAY LINE OF WARRENVILLE ROAD; THENCE ALONG SAID RIGHT OF WAY LINE, SOUTH 85 DEGREES 01 MINUTE 00 SECONDS EAST, A DISTANCE OF 215.17 FEET TO A POINT ON THE WESTERLY RIGHT OF WAY LINE OF BELMONT ROAD AS DEDICATED IN DOCUMENT NO. R61-28016; THENCE ALONG SAID RIGHT OF WAY LINE, SOUTH 05 DEGREES 26 MINUTES 00 SECONDS EAST, A DISTANCE OF 137.608 FEET TO THE POINT OF BEGINNING, CONTAINING 45,069 SQUARE FEET, OR 1.03 ACRES, MORE OR LESS.

THE ABOVE DESCRIPTION DESCRIBES THAT PROPERTY DESCRIBED IN CHICAGO TITLE INSURANCE COMMITMENT NO. 1401 008426132 D1, HAVING AN EFFECTIVE DATE OF

14 BUILDING AREA

2212 OGDEN RD.
±1,166 S.F.
CAR WASH
±1,586.66 S.F.

6 CEMETERY

THERE WAS NO OBSERVABLE EVIDENCE OF CEMETERIES FOUND AT THE TIME OF THIS SURVEY.

15 BUILDING HEIGHT

2212 OGDEN RD.
±13.2' HIGH
CAR WASH
±16.2' HIGH

2 TITLE INFORMATION

The Title Description and Schedule B items hereon are from Chicago Title Insurance Company, Commitment No. 1401 008426132 D1, having an effective date of JUNE 16, 2009.

3 SCHEDULE 'B' ITEMS

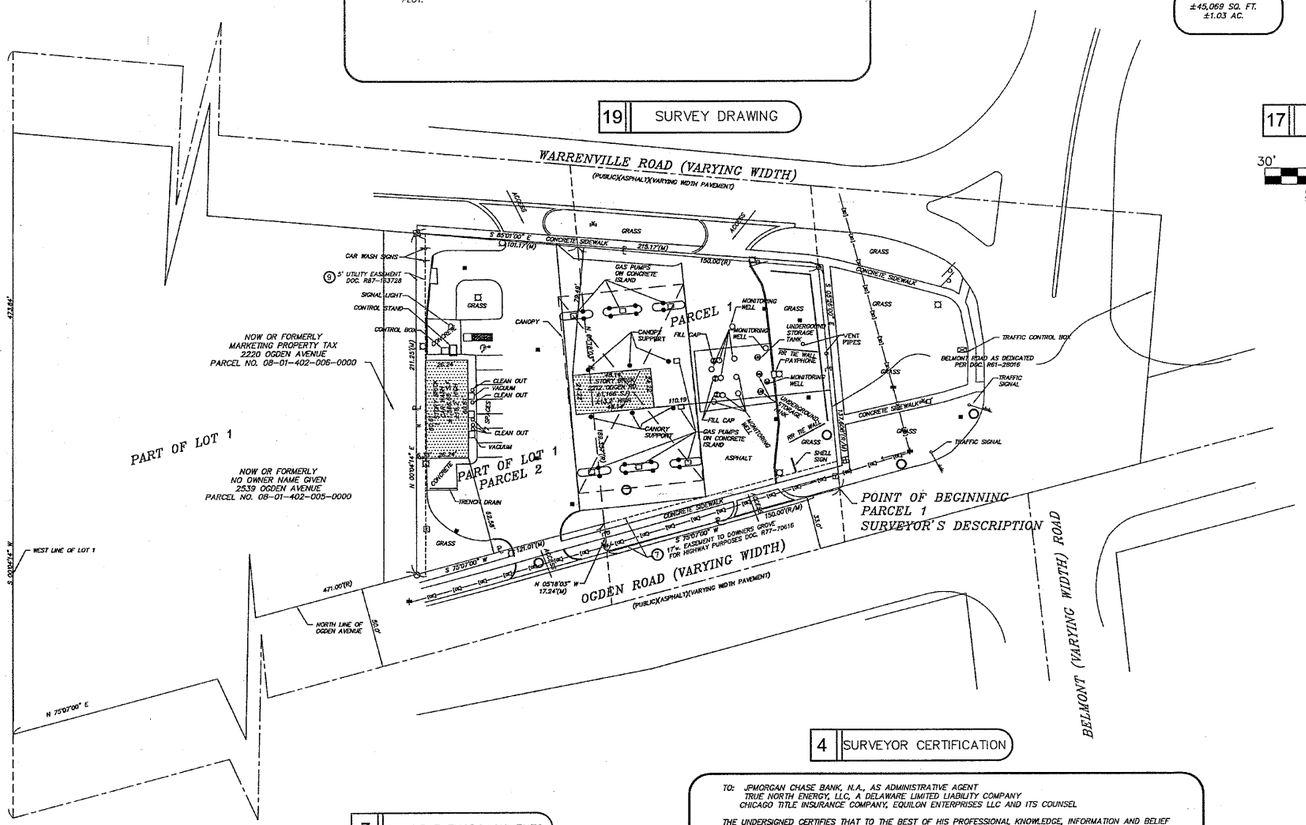
NOTES CORRESPONDING TO SCHEDULE "B":

(7) GRANT OF EASEMENT MADE BY AND BETWEEN ARTHUR TREACHER'S FISH AND CHIPS, INC., A CORPORATION OF DELAWARE, AND THE CITY OF DOWNERS GROVE, GRANTING AN EASEMENT FOR HIGHWAY PURPOSES, DATED APRIL 26, 1976 AND RECORDED AUGUST 15, 1977 AS DOCUMENT R77-70816. (AFFECTS THE SOUTHERLY 17.00 FEET OF PARCEL 1) AFFECTS AND APPLIES AS SHOWN.

(9) EASEMENT IN, UPON, UNDER, OVER AND ALONG THE WESTERLY 5.00 FEET OF THE LAND TO INSTALL AND MAINTAIN ALL EQUIPMENT FOR THE PURPOSE OF SERVING THE LAND, AND OTHER PROPERTY WITH TELEPHONE AND ELECTRIC SERVICE, TOGETHER WITH RIGHT OF ACCESS TO SAID EQUIPMENT, AS CREATED BY GRANT TO COMMONWEALTH EDISON COMPANY AND ILLINOIS BELL TELEPHONE COMPANY RECORDED NOVEMBER 17, 1987 AS DOCUMENT R87-163728. (AFFECTS PARCEL 2) AFFECTS AND APPLIES AS SHOWN.

(10) RIGHTS, IF ANY, OF PUBLIC AND QUASI-PUBLIC UTILITIES IN THE LAND, AS DISCLOSED BY CATCH BASINS, MANHOLE COVERS, ELECTRICAL TRANSFORMER AND PROPOSED DRAINAGE EASEMENT AS DISCLOSED BY SURVEY DATED APRIL 29, 1998 AND PREPARED BY MARCHESE AND SONS, INC. DO NOT HAVE DOCUMENT. UNABLE TO PLOT.

19 SURVEY DRAWING



7 POSSIBLE ENCROACHMENTS

NONE APPARENT AT TIME OF SURVEY

5 FLOOD INFORMATION

BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE X OF THE FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 17043C0803H, WHICH BEARS AN EFFECTIVE DATE OF DECEMBER 16, 2004 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA. THIS COMMUNITY DOES CURRENTLY PARTICIPATE IN THE PROGRAM. NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE AND AN ELEVATION CERTIFICATE MAY BE NEEDED TO VERIFY THIS DETERMINATION OR APPLY FOR A VARIANCE FROM THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

ZONE X-AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

4 SURVEYOR CERTIFICATION

TO: JPMORGAN CHASE BANK, N.A., AS ADMINISTRATIVE AGENT
TRUE NORTH ENERGY, LLC, A DELAWARE LIMITED LIABILITY COMPANY
CHICAGO TITLE INSURANCE COMPANY, EQUILON ENTERPRISES LLC AND ITS COUNSEL

THE UNDERSIGNED CERTIFIES THAT TO THE BEST OF HIS PROFESSIONAL KNOWLEDGE, INFORMATION AND BELIEF THAT (i) THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WAS MADE (x) ON THE DATE SHOWN BELOW OF THE PROPERTY SPECIFICALLY DESCRIBED IN COMMITMENT NO. 1401-008426132-D1 ISSUED BY CHICAGO TITLE INSURANCE CORPORATION DATED , (y) IN ACCORDANCE WITH "MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS," JOINTLY ESTABLISHED AND ADOPTED BY ALTA, ACSM AND NSPS IN 2005, AND INCLUDES ITEMS 1, 2, 3, 4, 6, 7(A), 8, 9, 10 AND 11(A) OF TABLE A, AND (z) PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA, NSPS, AND ACSM AND IN EFFECT ON THE DATE OF THIS CERTIFICATION, AND (ii) THE SURVEY MEASUREMENTS WERE MADE IN ACCORDANCE WITH THE "MINIMUM ANGLE, DISTANCE, AND CLOSURE REQUIREMENTS FOR SURVEY MEASUREMENTS WHICH CONTROL LAND BOUNDARIES FOR ALTA/ACSM LAND TITLE SURVEYS." THE UNDERSIGNED CERTIFIES THAT THE SURVEY CORRECTLY SHOWS THE TOTAL AREA OF THE PROPERTY IN ACRES AND SQUARE FEET, THE EXACT DIMENSIONS AND LOCATION OF IMPROVEMENTS, WALKWAYS, PAVED AREAS, AND PARKING AREAS; AND OTHER MATTERS ON THE GROUND WHICH MAY ADVERSELY AFFECT TITLE TO THE PROPERTY; THE EXACT RELATION OF BUILDINGS AND OTHER STRUCTURES TO THE PROPERTY LINES OF THE LAND INDICATED HEREON; THE EXACT LOCATION OF VISIBLE AND RECORDED EASEMENTS AND OTHER MATTERS OF RECORD AFFECTING THE PROPERTY. THE UNDERSIGNED FURTHER CERTIFIES THAT THERE ARE NO ENCROACHMENTS OF ADJOINING BUILDINGS OR STRUCTURES ONTO SAID LAND NOR OVERLAP OF BUILDINGS OR STRUCTURES FROM SAID LAND OTHER THAN AS SHOWN; THAT ADEQUATE INGRESS AND EGRESS TO THE PROPERTY ARE PROVIDED BY WARRENVILLE ROAD AND OGDEN ROAD, AS SHOWN ON THE SURVEY, THE SAME BEING PAVED, DEDICATED PUBLIC RIGHTS OF WAY; THAT THE LOCATION OF ALL IMPROVEMENTS ON THE PROPERTY IS IN ACCORD WITH ALL APPLICABLE ZONING LAWS REGULATING THE USE OF THE PROPERTY AND WITH ALL APPLICABLE LAWS CONTAINING MINIMUM SET BACK PROVISIONS AND COVENANTS AND RESTRICTION OF RECORD; THAT THE PROPERTY DOES NOT SERVE ANY ADJOINING PROPERTY FOR DRAINAGE INGRESS AND EGRESS OR FOR ANY OTHER PURPOSE; AND THAT THE PROPERTY IS NOT IN A FLOOD PLAIN (AS SHOWN BY MAP NO. 17043C0803H AS DATED 12-16-2004).

DAVID J. SHERRILL, P.L.S.
REGISTRATION NO. 2534
DATE OF EXPIRATION: 11-30-2010
DATE OF SURVEY: 08-06-09
DATE OF LAST REVISION:

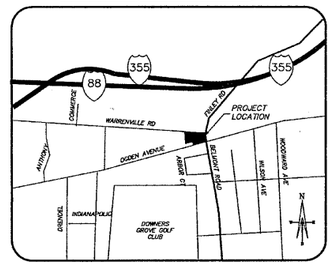
12 PARKING INFORMATION

7 REGULAR SPACES
1 HANDICAP SPACE
8 TOTAL SPACES

13 LAND AREA

±45,069 SQ. FT.
±1.03 AC.

16 VICINITY MAP



NOT TO SCALE

17 SCALE

30' 0' 30' 60'

SCALE: 1" = 30'

9 LEGEND

- IRON PIN FOUND
- CONC. MON. FOUND
- PK NAIL FOUND
- CHISELED "X" FOUND
- STONE FOUND
- FENCE POST FOUND AND USED
- IRON PIN SET
- CONC. MON SET
- PK NAIL SET
- CHISELED "X" SET
- ANCHOR FOUND
- AXLE FOUND
- EXISTING STRUCTURE
- OVERHEAD ELECTRIC
- POWER POLE
- GUY WIRE
- LIGHT STANDARD
- WATER METER
- WATER VALVE
- FIRE HYDRANT
- GAS METER
- GRATED INLET
- MANHOLE
- SIGN
- HANDICAP
- PIPE BOLLARD
- CURB INLET
- CABLE SPLICE BOX
- TELEPHONE SPLICE BOX
- ELECTRIC SPLICE BOX
- ELECTRIC METER
- TRANSFORMER

ABBREVIATIONS

CMP	CORRUGATED METAL PIPE
ROW	RIGHT OF WAY
TRANS	TRANSFORMER
CONC	CONCRETE
CL	CENTER LINE
PL	PROPERTY LINE
(TYP)	TYPICAL
(R)	RECORD
(M)	MEASURED

8 ZONING INFORMATION

ZONING CLASSIFICATION:
B3 - GENERAL SERVICES AND HIGHWAY BUSINESS

MINIMUM LOT SIZE: 15,625 S.F. PLUS 2,00 S.F. FOR EACH PUMP ISLAND IN EXCESS OF 4 PUMP ISLANDS, AND 2,000 S.F. FOR EACH SERVICE BAY IN EXCESS OF 3 SERVICE BAYS.

MINIMUM FRONTAGE: 125 FEET
MINIMUM DEPTH: 125 FEET
SETBACKS:
SIDE: 20 FEET WHEN ABUTTING RESIDENTIAL
REAR: 30 FEET WHEN ABUTTING RESIDENTIAL
FRONT: NONE GIVEN

PARKING REQUIRMENTS:
1 SPACE PER GAS PUMP ISLAND PLUS 1 PER SERVICE BAY PLUS 3.3 PER KSF RETAIL GFA

ZONING CONTACT:
COMMUNITY DEVELOPMENT
VILLAGE OF DOWNERS GROVE
801 BURLINGTON AVENUE
DOWNERS GROVE, IL 60515
630-434-5515
www.downers.us

10 BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTHERLY LINE OF LOT 1 OF ROSEN'S OGDEN AVENUE SUBDIVISION, RECORDED AS DOCUMENT NO. R64-02998; SAID BEARING BEING SOUTH 85 DEGREES 01 MINUTES 00 SECONDS EAST.

11 SURVEYOR'S NOTES

1. THERE WAS OBSERVABLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR REPAIRS.
2. THERE WAS NO OBSERVABLE EVIDENCE OF SITE BEING USED AS A SOLID WASTE DUMP, SUMP OR SANITARY LANDFILL.
3. THERE WAS NO OBSERVABLE EVIDENCE OF STREET WIDENING.
4. THE LOCATION OF UTILITIES SHOWN HEREON IS FROM OBSERVED EVIDENCE OF ABOVE GROUND APPURTENANCES ONLY. THE SURVEYOR WAS NOT PROVIDED WITH UNDERGROUND PLANS OR SURFACE GROUND MARKINGS TO DETERMINE THE LOCATION OF ANY SUBTERRANEAN USES.
5. ALL STATEMENTS WITHIN THE CERTIFICATION, AND OTHER REFERENCES LOCATED ELSEWHERE HEREON, RELATED TO: UTILITIES, IMPROVEMENTS, STRUCTURES, BUILDINGS, PARTY WALLS, PARKING, EASEMENTS, SERVITUDES AND ENCROACHMENTS; ARE BASED SOLELY ON ABOVE GROUND, VISIBLE EVIDENCE, UNLESS ANOTHER SOURCE OF INFORMATION IS SPECIFICALLY REFERENCED HEREON.

KEY TO ALTA-SURVEY

1 TITLE DESCRIPTION	8 ZONING INFORMATION	15 BUILDING HEIGHT
2 TITLE INFORMATION	9 LEGEND	16 VICINITY MAP
3 SCHEDULE 'B' ITEMS	10 BASIS OF BEARING	17 SCALE
4 SURVEYOR CERTIFICATION	11 SURVEYOR'S NOTES	18 CLIENT INFORMATION BOX
5 FLOOD INFORMATION	12 PARKING INFORMATION	19 SURVEY DRAWING
6 CEMETERY	13 LAND AREA	
7 POSSIBLE ENCROACHMENTS	14 BUILDING AREA	

Surveyor Title Block
SHERRILL ASSOCIATES, INC.
Surveyors - Engineers - Planners
316 Main Street
Edwardsville, IL 62025
618-656-6251 Phone
618-656-9468 Fax
2727 Telegraph Road
St. Louis, MO 63125
314-487-5800 Phone
314-487-3365 Fax
ILLINOIS DESIGN FIRM #184-001238

ALTA/ACSM Land Title Survey

18 CLIENT INFORMATION BOX

This Work Coordinated By:

E:\PROTOTYP\ALTA\FAPLS.tif

621 24th Avenue S.W.
Norman, Oklahoma 73069
(405) 701-1111 www.FirstamPLS.com

Drwn By: JAC	Date:02/15/10
Surveyor Ref.No: 08370-02	Revision: TITLE UPDATE
Aprvd By: DJS	Date:
Field Date: 08/06/09	Revision:
Scale: 1"=30'	Date:
	Revision:

Prepared For:

Client Ref. No: #1985

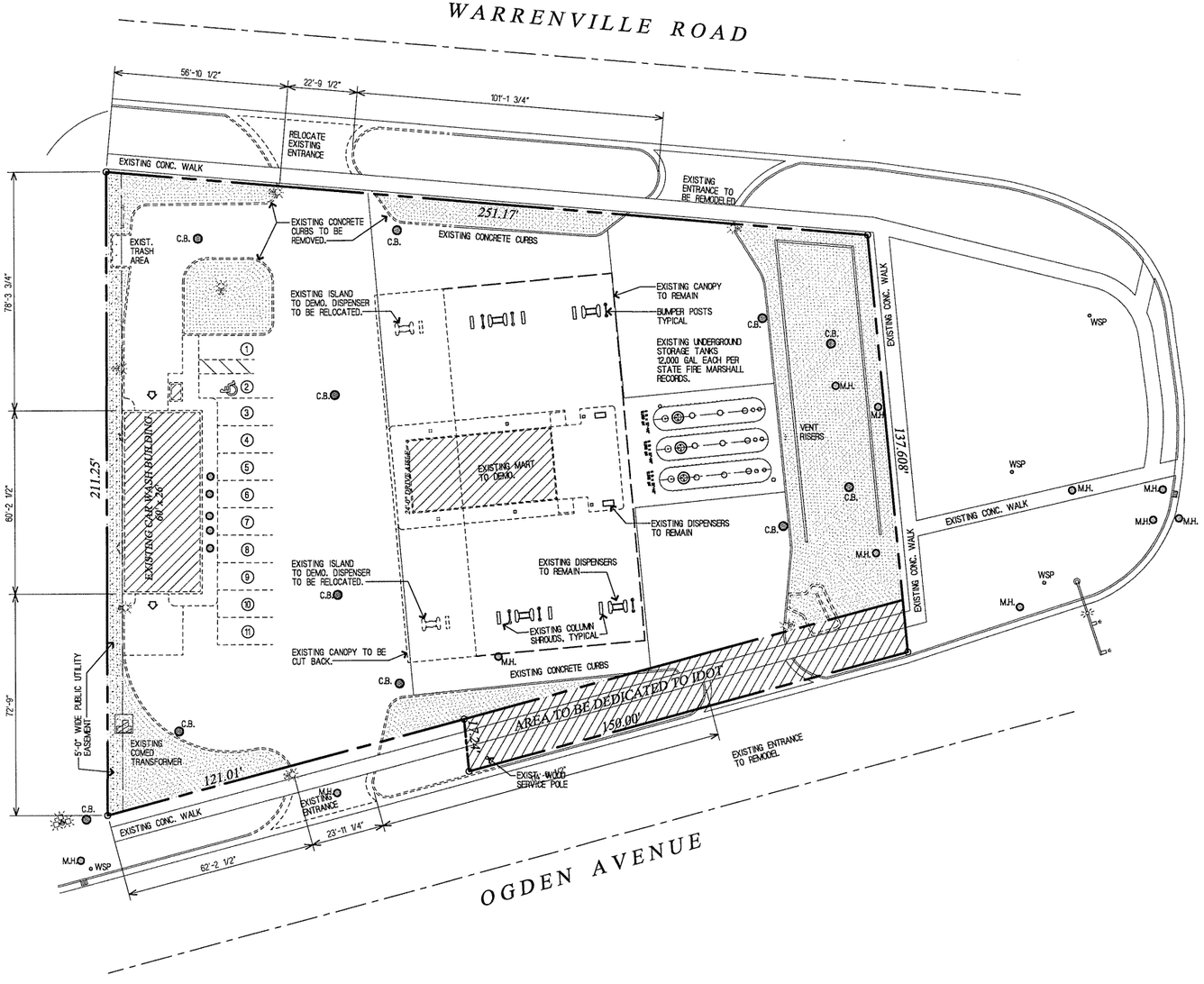
Project Address:
2212 50 OGDEN ROAD

Project Location:
Downers Grove, IL

Project Name:
Shell Chicago

Job Number:
09-05-034:122

Sheet 1 of 1



EXISTING SITE/DEMO PLAN - 2

SCALE: 1" = 20'-0"

DOWNERS GROVE ZONING ANALYSIS

ZONING DISTRICT : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO NORTH : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO SOUTH : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO EAST : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO WEST : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS

SETBACK REQUIREMENTS:
OGDEN AVENUE (FROM ROW CENTERLINE) = 75'-0" PRINCIPAL BLDG
OGDEN AVENUE (FROM ROW CENTERLINE) = 50'-0" CANOPY
WARRENVILLE ROAD = 25'-0"
SIDE YARD (WEST PROPERTY LINE) = NONE REQUIRED

CONVENIENCE STORE IS A PERMITTED USE IN B-3 DISTRICT
FUELING STATION IS A SPECIAL USE IN B-3 DISTRICT

IDOT ENCROACHMENT AREA =2549.57 SQ. FT. OR 0.0585 ACRES
SITE AREA = 45,069 SQ. FT. OR 1.035 ACRES
TOTAL SITE AREA LESS ENCROACHMENT = 42,538.43 SQ. FT. OR 0.976 ACRES

PARKING REQUIREMENT : RETAIL = 4/1000 GROSS SF
RETAIL = 4/1000 LESS STORAGE AREA (1200 SF)
One Space per pump island = 8 required
3.33 spaces per 1000 sf of retail area = 3.33/1000 x 1832 = 6

Total parking required = 14 stalls

LANDSCAPE CALCULATIONS:
EXISTING LANDSCAPE GREEN SPACE = 8745 SQ. FT
8745/42,538.43 = 20% EXISTING SITE GREEN AREA
PROPOSED LANDSCAPE GREEN SPACE = 12,868 SQ. FT
12,868/42,538.43 = 30.25% PROPOSED SITE GREEN AREA

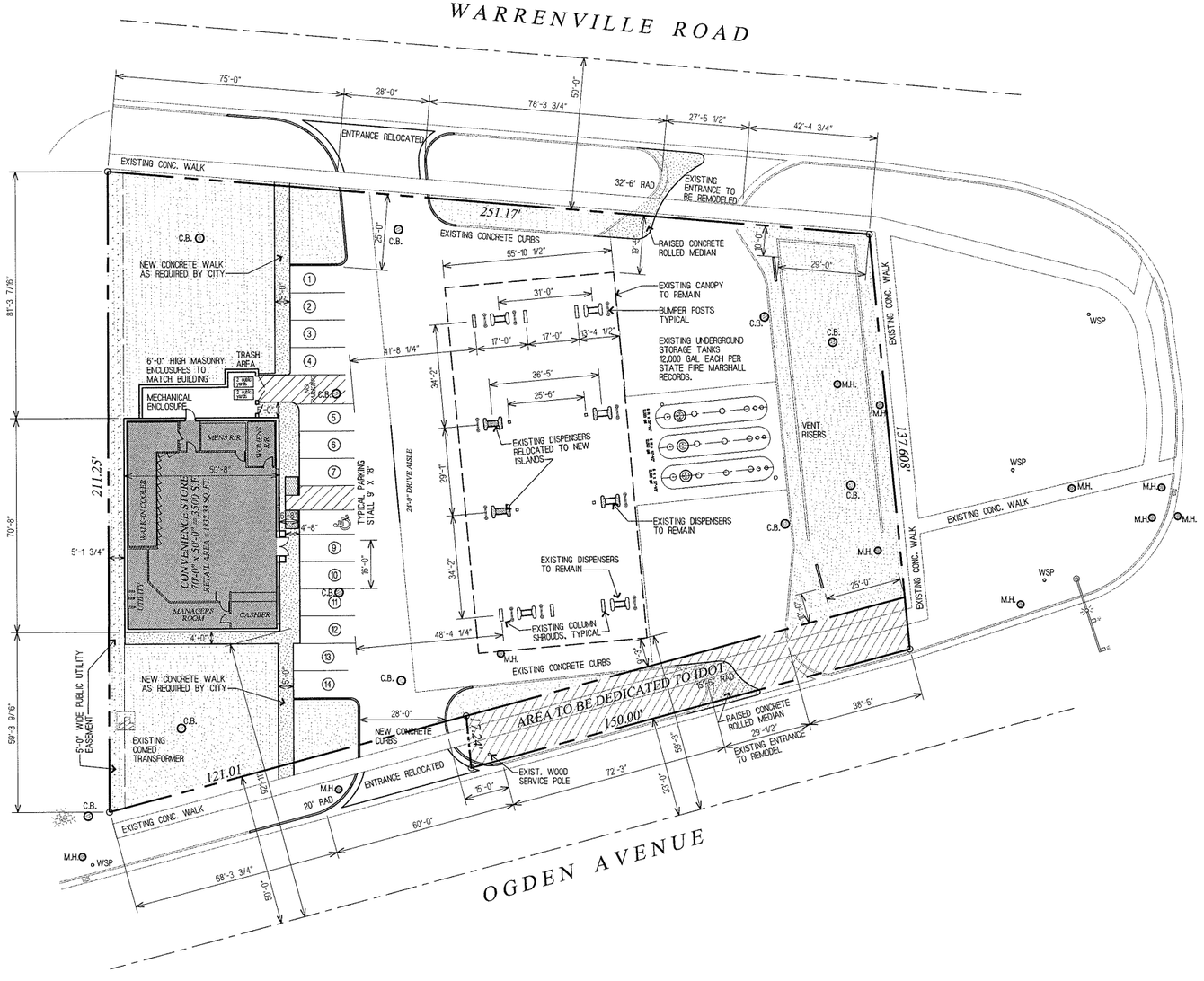
Ambrose Design Group
P.O. BOX 1870
CRYSTAL LAKE, ILLINOIS
60039-1870
(847) 347-3721

NO	DESCRIPTION	DATE
	REVISIONS	
	UPDATE PER SITE VISIT	10-18-14
	REVISE PER CITY REVIEW COMMENTS	1-23-15
	REVISE TRASH ENCLOSURE	1-27-15
	REVISE PER CITY REVIEW COMMENTS	6-1-15
	REVISE PER CITY REVIEW COMMENTS	6-25-15
	SUBMITTED FOR OCTOBER PLAN COMMISSION	9-15-15

PROJECT NAME AND LOCATION:
PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210-2250 W. OGDEN AT BELMONT
DOWNERS GROVE, ILLINOIS
PIN: 08-01-402-007 (008)
CLIENT:
truenorth energy, llc

SHEET TITLE:
EXISTING/DEMO SITE PLAN - 2

DATE: 10-15-14
SCALE: 1" = 20'-0
DRAWN BY: RJA
PROJECT NO.: 141001
SHEET **SP-1** OF TWO



DOWNERS GROVE ZONING ANALYSIS

ZONING DISTRICT : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO NORTH : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO SOUTH : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO EAST : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO WEST : B-3 GENERAL SERVICES AND HIGHWAY BUSINESS

SETBACK REQUIREMENTS:
OGDEN AVENUE (FROM ROW CENTERLINE) = 75'-0" PRINCIPAL BLDG
OGDEN AVENUE (FROM ROW CENTERLINE) = 50'-0" CANOPY
WARRENVILLE ROAD = 25'-0"
SIDE YARD (WEST PROPERTY LINE) = NONE REQUIRED

CONVENIENCE STORE IS A PERMITTED USE IN B-3 DISTRICT
FUELING STATION IS A SPECIAL USE IN B-3 DISTRICT

IDOT ENCROACHMENT AREA =2549.57 SQ. FT. OR 0.0585 ACRES
SITE AREA = 45,069 SQ. FT. OR 1.035 ACRES
TOTAL SITE AREA LESS ENCROACHMENT = 42,538.43 SQ. FT. OR 0.976 ACRES

PARKING REQUIREMENT : RETAIL = 4/1000 GROSS SF
RETAIL = 4/1000 LESS STORAGE AREA (1200 SF)
One Space per pump island = 8 required
3.33 spaces per 1000 sf of retail area = 3.33/1000 x 1832 = 6
Total parking required = 14 stalls

LANDSCAPE CALCULATIONS:
EXISTING LANDSCAPE GREEN SPACE = 8745 SQ. FT
8745/42,538.43 = 20% EXISTING SITE GREEN AREA
PROPOSED LANDSCAPE GREEN SPACE = 12,868 SQ. FT
12,868/42,538.43 = 30.25% PROPOSED SITE GREEN AREA

PROPOSED SITE PLAN - 2

SCALE: 1" = 20'-0"

Ambrose Design Group
PO BOX 1870
CRYSTAL LAKE, ILLINOIS
60039-1870
(847) 347-3721

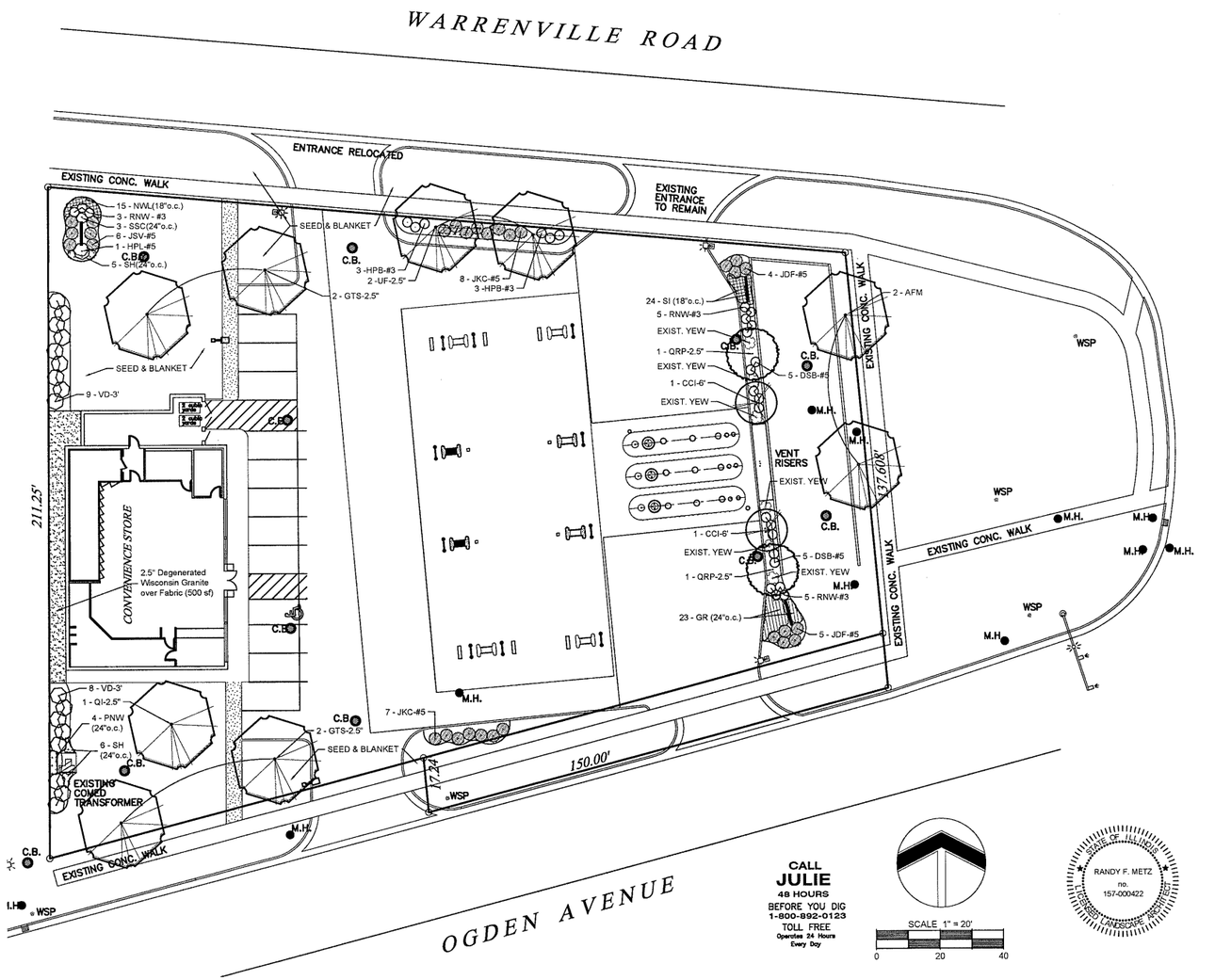
NO	DESCRIPTION	DATE
	REVISIONS	
	UPDATE PER SITE VISIT	10-16-14
	REVISE PER CITY REVIEW COMMENTS	1-23-15
	REVISE TRASH ENCLOSURE	1-27-15
	REVISE PER CITY REVIEW COMMENTS	6-1-15
	REVISE PER CITY REVIEW	6-25-15
	SUBMITTED FOR OCTOBER PLAN COMMISSION	9-5-15

PROJECT NAME AND LOCATION:
PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210-2250 W. OGDEN AT BELMONT
DOWNERS GROVE, ILLINOIS
PIN: 08-01-402-007 (008)
CLIENT:
truenorth energy, llc

SHEET TITLE:
NEW PROPOSED SITE PLAN - 2

DATE: 10-15-14
SCALE: 1" = 20'-0
DRAWN BY: RJA
PROJECT NO.: 141001

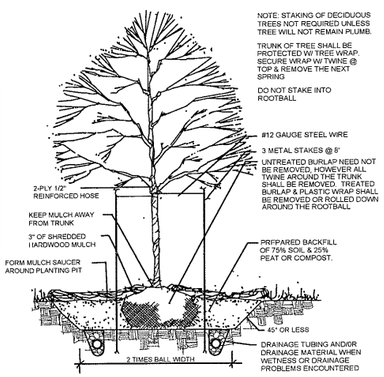
SHEET
SP-2
OF TWO



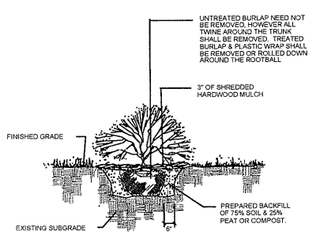
CALL JULIE
48 HOURS
BEFORE YOU DIG
1-800-892-0123
TOLL FREE
Operates 24 Hours Every Day

SCALE 1" = 20'
0 20 40

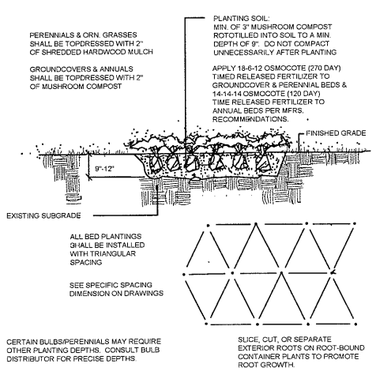
STATE OF ILLINOIS
RANDY F. METZ
no.
157-000422
REGISTERED LANDSCAPE ARCHITECT



DECIDUOUS TREE
NTS



SHRUBS
NTS



BED PLANTING DETAIL
(PERENNIALS, ORNAMENTAL GRASSES VINES, GROUNDCOVERS & ANNUALS)
NTS

PLANT LIST

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE/ TYPE
		DECIDUOUS SHADE TREES		
AFM	2	Acer f. 'Morno'	Morno Freeman Maple	2.5" BB
GTS	4	Gleditsia t.i.'Skyline'	Skyline Honeylocust	2.5" BB
QI	1	Quercus imbricaria	Shingle Oak	2.5" BB
QRP	2	Quercus r. 'Long'	Regal Prince English Oak	2.5" BB
UF	2	Ulmus 'Frontier'	Frontier Elm	2.5" BB
		DECIDUOUS ORNAMENTAL TREES		
CCI	2	Crataegus c. inermis	Thornless Cockspur Hawthorn	6' BBcl.
		DECIDUOUS SHRUBS & SHRUB ROSES		
DSB	10	Diervilla s. 'Butterfly'	Butterfly Bush Honeysuckle	#5
HPB	6	Hydrangea p. 'ILVOBO'	Bobo Hydrangea	#3
HPL	1	Hydrangea p. 'Limelight'	Limelight Hydrangea	#5
RNW	13	Rosa 'Nearly Wild'	Nearly Wild Shrub Rose	#3
VD	17	Viburnum dentatum	Arrowwood Viburnum	3' BB
		EVERGREEN SHRUBS		
JDF	9	Juniperus c 'Daub's Frosted'	Daub's Frosted Juniper	#5
JKC	15	Juniperus c. 'Kalleys'	Kalleys Compact Juniper	#5
JSV	6	Juniperus c. sargentii 'Verdis'	Green Sargent Juniper	#5
		ORNAMENTAL GRASS		
PNW	4	Panicum v. 'Northwind'	Northwind Switch Grass	#1
SSC	3	Schizachyrium s. 'Carousel'	Carousel Little Bluestem	#1
SH	11	Sporobolus heterolepis	Prairie Dropseed	#1
		PERENNIALS		
GR	23	Geranium 'Gerwat'	Rozanne Geranium	#1
NWL	15	Nepeta f. 'Walker's Low'	Walker's Low Catmint	#1
		GROUNDCOVERS & VINES		
SI	24	Sedum 'Immergruenchen'	Little Evergreen Sedum	from 24 flat

MATERIAL & LABOR LIST:

QTY	ITEM	DESCRIPTION
500 SF	Wls. Degenerated Granite over Fabric	(2.5" Depth)
848 SY	Seed w/ Straw Blanket	Kentucky Bluegrass Blend w/Perennial Rye S75 by North American Green (or equal)
14 CY	Mulch	Shredded Hardwood Bark
1 CY	Mulch	Compost (Yard Waste or Mushroom)

GENERAL NOTES:

Plant material shall be nursery grown and be either balled and burlapped or container grown. Sizes and spreads on plant list represent minimum requirements.

Size & grading standards of plant materials shall conform to the latest addition of ANSI Z60.1 AMERICAN STANDARD OF NURSERY STOCK, by the American Nursery & Landscape Association.

Any materials with damaged or crooked/disfigured leaders, bark abrasion, sunscald, insect damage, etc. are not acceptable and will be rejected. Trees with multiple leaders will be rejected unless called for in the plant list as multi-stem or clump (cl.).

Grading shall provide slopes which are smooth and continuous. Positive drainage shall be provided in all areas.

Quantity lists are supplied as a convenience. However, the contractor should verify all quantities. The drawings shall take precedence over the lists.

All plant species specified are subject to availability. Material shortages in the landscape industry may require substitutions. All substitutions must be approved by the Landscape Architect and/or Owner.

Contractor shall verify location of all underground utilities prior to digging. For location outside the City of Chicago call "J.U.L.I.E." (Joint Utility Location for Excavators) 1-800-892-0123.

Grass seed shall be applied mechanically so that the seed is incorporated into the top one-half inch (1/2") of the seed bed. The seed shall then be covered with the specified straw blanket or Hydro-mulch.

All plant material shall be guaranteed for one (1) year from the date of acceptance.

Contractor shall report any discrepancies in the field to the Landscape Architect and/or Owner.

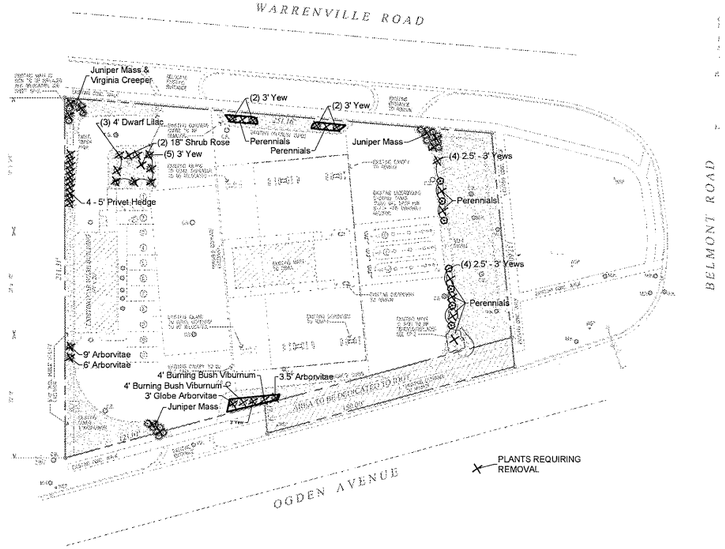
Plant symbols illustrated on this plan are a graphic representation of proposed plant material types and are intended to provide for visual clarity. However, the symbols do not necessarily represent actual plant spread at the time of installation.

All perennial, ornamental grass, groundcover and annual beds shall be topdressed with a minimum of three inches (3") of mushroom compost. The topdressing shall be worked into the soil to a minimum depth of nine inches (9") by the use of a cultivating mechanism. Upon completion perennials & ornamental grasses shall be mulched with an additional two inch (2") layer of shredded wood mulch; Annuals & groundcovers shall be covered with an additional two inch (2") layer of mushroom compost.

All other planting beds and tree saucers shall be mulched with a minimum of three inches (3") of shredded wood mulch.

All bed lines and tree saucers shall require a hand spaded edge between lawn and mulched areas.

Planting beds adjacent to building shall be mulched in their entirety to the building foundation. Plant materials shall not be installed under building overhangs and other such areas which do not receive natural rainfall.



PLANT REMOVAL PLAN
1"=40'

REVISIONS

PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210 - 2250 W. OGDEN AVENUE
DOWNERS GROVE, ILLINOIS
TrueNorth Energy, LLC

IRG Ives/Ryan Group, Inc.
324 EISENHOWER LANE N.
LOMBARD, IL 60148
PHONE: 630.717.0726

Landscape Architecture
Park & Recreation Design
Site & Community Planning

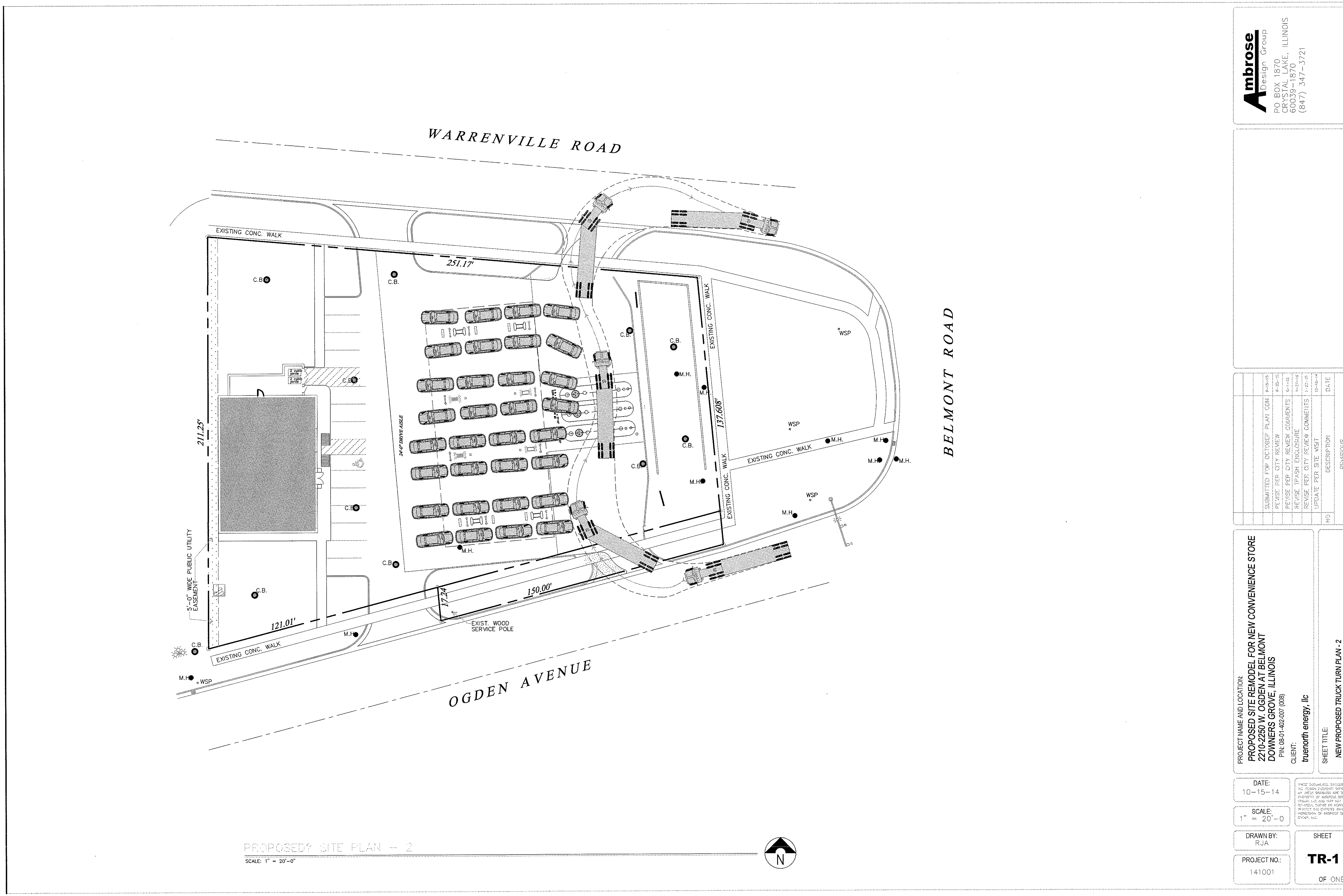
www.ivesryangroup.com

LANDSCAPE PLAN

PROJECT NO.: JOB NO.:
L1015 8746A

DATE: 06/03/15
SCALE: 1"=20'
PLANNER: RM
DRAWN BY: RM
CHECKED:

SHEET
L-1


WARRENVILLE ROAD
BELMONT ROAD
OGDEN AVENUE
EXISTING CONC. WALK
251.17'
211.25'
121.01'
150.00'
137.608'
17.24
24'-0" DRIVE AISLE
5'-0" WIDE PUBLIC UTILITY EASEMENT
EXIST. WOOD SERVICE POLE
C.B.
M.H.
WSP
PROPOSED SITE PLAN - 2
SCALE: 1" = 20'-0"
N
Ambrose Design Group
PO BOX 1870
CRYSTAL LAKE, ILLINOIS
60039-1870
(847) 347-3721
SUBMITTED FOR OCTOBER PLAN COM
REVISE PER CITY REVIEW
REVISE PER CITY REVIEW COMMENTS
REVISE TRASH ENCLOSURE
REVISE PER CITY REVIEW COMMENTS
UPDATE PER SITE VISIT
NO.
DESCRIPTION
DATE
REVISIONS
PROJECT NAME AND LOCATION:
PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210-2250 W. OGDEN AT BELMONT
DOWNERS GROVE, ILLINOIS
PIN: 08-01-402-007 (008)
CLIENT:
truenorth energy, llc
SHEET TITLE:
NEW PROPOSED TRUCK TURN PLAN - 2
DATE:
10-15-14
SCALE:
1" = 20'-0
DRAWN BY:
RJA
PROJECT NO.:
141001
SHEET
TR-1
OF ONE

AREA LIGHTS

SYMBOL	QTY	LABEL
	4	A3B-EP

BXSP C HT 3ME E 57K-UL WH + XA-SP1BLS (EXISITING POLE)

AREA LIGHTS

SYMBOL	QTY	LABEL
	2	A4B-NP

BXSP C HT 4ME E 57K-UL WH + XA-SP1BLS (NEW POLE)

CANOPY LIGHTS

SYMBOL	QTY	LABEL
	48	CEXISTING

CPY250-A-DM-F-A-UL-WH (82 Watt Flat Lens)

SOFFIT LIGHTS

SYMBOL	QTY	LABEL
	11	S

LRP38-10L-25D (Lamp Only) + RC6 (Optional Housing)

XSP Series

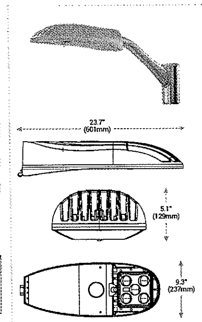
XSP1™ LED Street/Area Light – Single Module – Version C

Product Description

Performance Summary

Accessories

Ordering Information



CREE

XSP Series - Single Module - Version C

Product Specifications

CREE

XSP Series

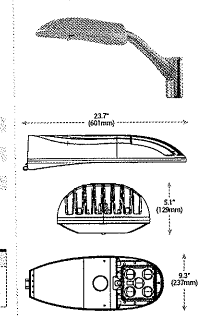
XSP1™ LED Street/Area Light – Single Module – Version C

Product Description

Performance Summary

Accessories

Ordering Information



CREE

XSP Series - Single Module - Version C

Product Specifications

CREE

CPY Series

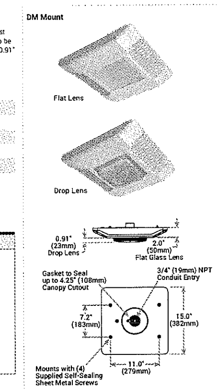
CPY250™ LED Canopy/Soffit Luminaire

Product Description

Performance Summary

Accessories

Ordering Information



CREE

CPY250™ LED Canopy/Soffit Luminaire

Product Specifications

CREE

LRP38-10L

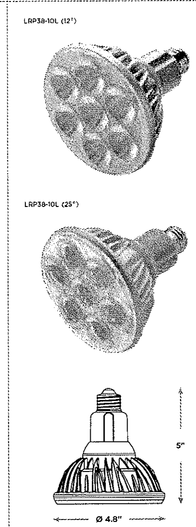
High Output PAR38 LED Lamp

Product Description

Performance Summary

Ordering Information

Housings & Accessories

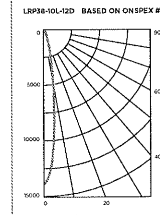


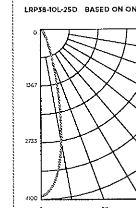
CREE

LRP38-10L

Product Specifications

Photometry





CREE

RC6

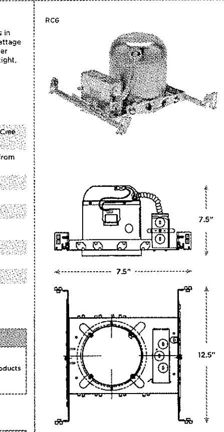
Six-inch Recessed Housing

Product Description

Product Specifications

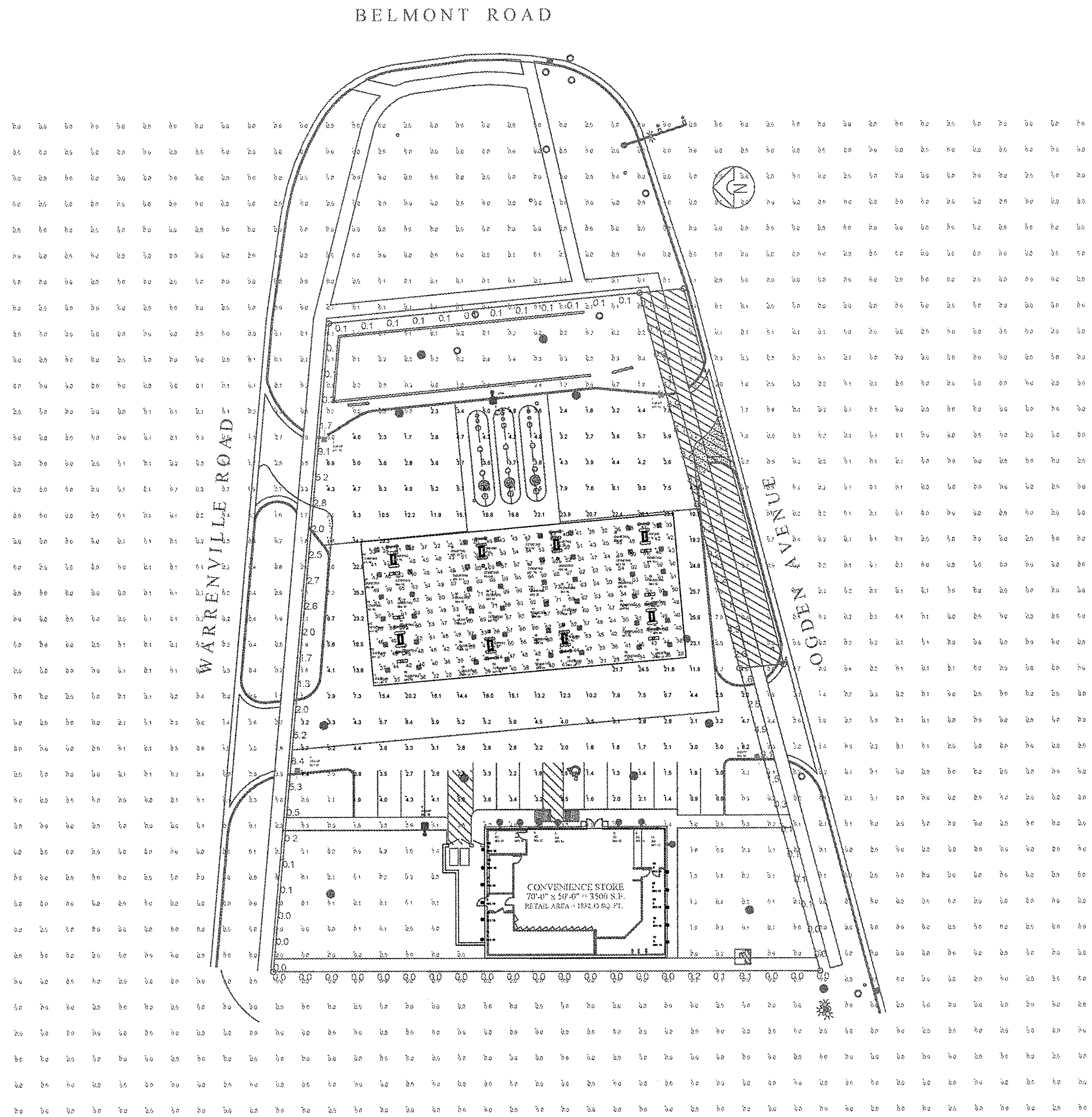
Compatible Downlights

Ordering Information



CREE

red leonard associates
5630 Bridgetown Rd, Ste 2 | Cincinnati, OH, 45248 | 513-574-9500
www.redleonard.com



Luminaire Schedule

Symbol	Qty	Label	Lumens	LLF	Arr. Watts	Total Watts	Manufacturer	Description
	4	A3B-EP	7190	1.030	101	404	Cree Inc	BXSP C HT 3ME E 57K-UL WH + XA-SP1BLS (EXISTING POLE)
	2	A4B-NP	6807	1.030	101	202	Cree Inc	BXSP C HT 4ME E 57K-UL WH + XA-SP1BLS (NEW POLE)
	7	AD	700	1.000	10	70	LSI INDUSTRIES	AD-150-10-CW-LED-UE-GWT-DO
	48	CEXISTING	8821	1.040	81.5	3912	CREE, INC.	CPY250-A-DM-F-A-UL-WH (82Watt Flat Lens)
	11	S	977	1.000	12.8	140.8	CREE	LRP38-10L-25D (Lamp Only) + RC6 (Optional housing)

Luminaire Location Summary

LumNo	Label	MTG HGT
1	A3B-EP	16
2	A3B-EP	16
3	A3B-EP	16
4	A3B-EP	16
5	A4B-NP	16
6	A4B-NP	16
7	AD	12
8	AD	12
9	AD	12
10	AD	12
11	AD	12
12	AD	12
13	AD	12
14	CEXISTING	15
15	CEXISTING	15
16	CEXISTING	15
17	CEXISTING	15
18	CEXISTING	15
19	CEXISTING	15
20	CEXISTING	15
21	CEXISTING	15
22	CEXISTING	15
23	CEXISTING	15
24	CEXISTING	15
25	CEXISTING	15
26	CEXISTING	15
27	CEXISTING	15
28	CEXISTING	15
29	CEXISTING	15
30	CEXISTING	15
31	CEXISTING	15
32	CEXISTING	15
33	CEXISTING	15
34	CEXISTING	15
35	CEXISTING	15
36	CEXISTING	15
37	CEXISTING	15
38	CEXISTING	15
39	CEXISTING	15
40	CEXISTING	15
41	CEXISTING	15
42	CEXISTING	15
43	CEXISTING	15
44	CEXISTING	15
45	CEXISTING	15
46	CEXISTING	15
47	CEXISTING	15
48	CEXISTING	15
49	CEXISTING	15
50	CEXISTING	15
51	CEXISTING	15
52	CEXISTING	15
53	CEXISTING	15
54	CEXISTING	15
55	CEXISTING	15
56	CEXISTING	15
57	CEXISTING	15
58	CEXISTING	15
59	CEXISTING	15
60	CEXISTING	15
61	CEXISTING	15
62	S	10
63	S	10
64	S	10
65	S	10
66	S	10
67	S	10
68	S	10
69	S	10
70	S	10
71	S	10
72	S	10

NOTES:
- THIS SITE IS A REMODEL, NEW C-STORE AND EXISTING CANOPY.
- CARWASH REMOVED REPLACED WITH NEW C-STORE.
- THERE ARE EXISTING AREA LIGHTS AS NOTED.
- ADDED TWO NEW AREA LIGHT LOCATIONS AS NOTED TO BRING MIN FC'S IN PAVED AREA UP TO 0.5 FC'S.

AREA LIGHT MOUNTING HEIGHT = 16 FT (15 FT POLE AND 1 FT BASE)

Footcandle Levels Calculated at Grade Using INITIAL Lumen Values

Label	Avg	Max	Min	Avg/Min	Max/Min
PAVED AREA	7.36	29.3	0.6	12.27	48.83
PROPERTY LINE	1.69	9.2	0.0	N.A.	N.A.
UNDEFINED AREA	0.28	9.3	0.0	N.A.	N.A.
UNDER CANOPY	50.38	73	28	1.80	2.61

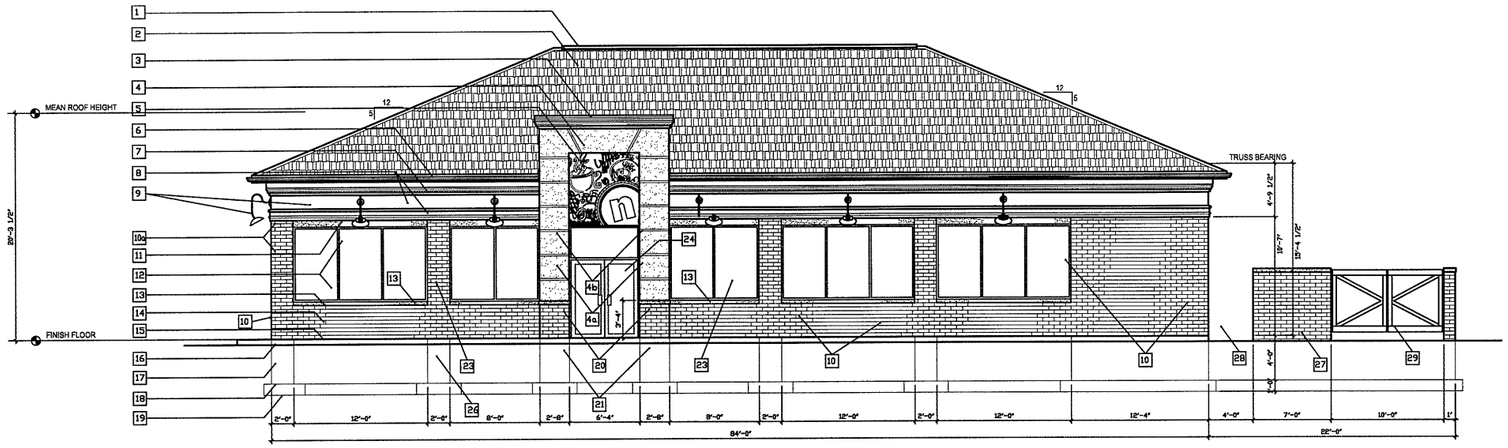
REV.	BY	DATE	DESCRIPTION
R1	BJM	2/19/15	REVISED AREA LIGHTS TO XSP'S
R2	BJM	6/29/15	REMOVED AREA LIGHTS TO MEET CITY REQUIREMENTS
R3	BJM	7/01/15	ADDED LOWER WATTAGE AREA LIGHTS WITH BACKSHIELDS

SCALE: 1" = 30' LAYOUT BY: BJM
DWG SIZE: D DATE: 2/17/15

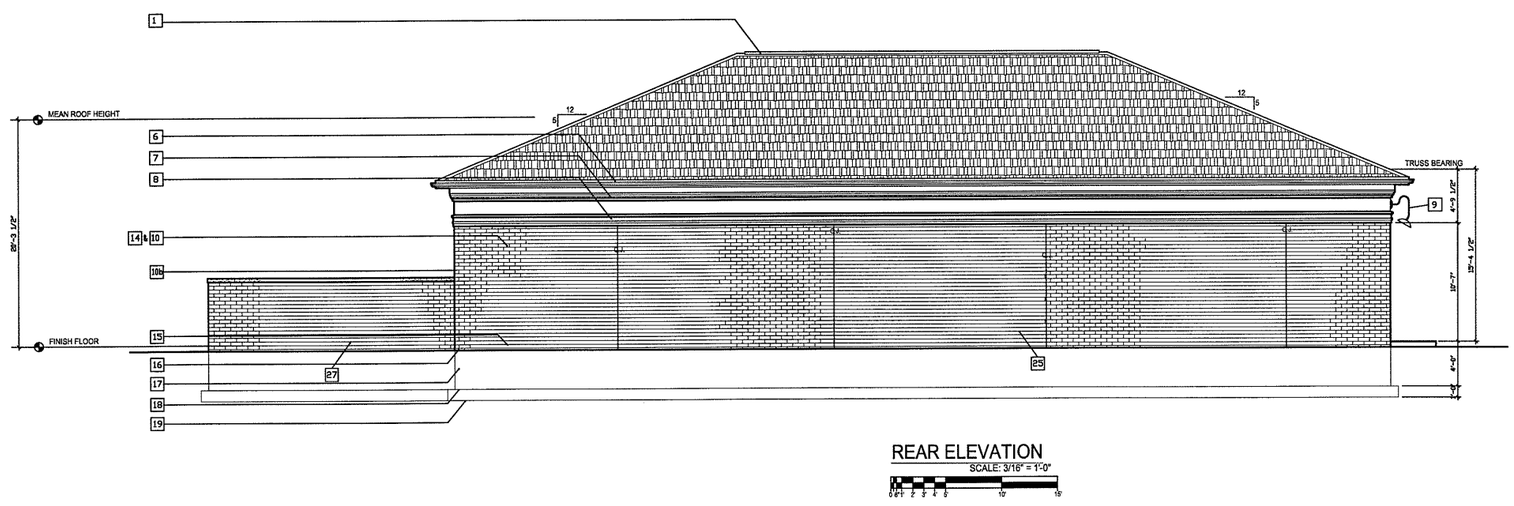
PROJECT NAME:
COLLINS EQUIPMENT - TRUE NORTH
DOWNERS GROVE, IL.

DRAWING NUMBER:
RL-2616-S1-R3

rla



FRONT ELEVATION
SCALE: 3/16" = 1'-0"



REAR ELEVATION
SCALE: 3/16" = 1'-0"

CODED NOTES
for the new elevations...

1. RIDGE VENT w/FILTER FABRIC AND MATCHING SHINGLES
2. OWENS CORNING ARCHITECTURAL LAMINATED SHINGLES (same line appearance) "DIAMONDE DRIFTWOOD" LAMINATED SHINGLES (see installation procedures and make, etc. for 110 mph)
3. .050 ALUMINUM CAP OVER THE TOP (CAP) OF THE ENTRY STRUCTURE w/KYNAR FINISH. BEND ALUMINUM TO CREATE POSITIVE SLOPE AWAY ON ALL SIDES. w/EIFS MOULDING BELOW, REFER TO WALL SECTIONS.
4. CONSTRUCT NEW MAIN ENTRY SRUCTURE w/EIFS AS INDICATED (the note points to the wall atop the medallion) FRAMING, ROOF PLANS & THE ELEVATIONS & WALL SECTIONS
4a. IMPACT RESISTANT MESH AND EIFS SYSTEM FROM THE TOP/LIMESTONE BLOCK TO A HEIGHT OF 7'-4" AFF. TRANSITION FROM 4a TO 4b AT AN INDENTATION JOINT...
4b. STANDARD EIFS MESH & INSULATION SYSTEM FROM 7'-4" aff TO THE TOP TRANSITION FROM 4a TO 4b AT AN INDENTATION JOINT...
PLEASE REFER TO NOTE NUMBER "20" AS IT RELATED TO THE BOTTOM COURSE OF THE ENTRY STRUCTURE
5. "TRUENORTH" ENTRY INTERNALLY ILLUMINATED ENTRY MEDALLION (color and graphics to be submitted w/ sign permit)
6. .032 ALUMINUM GUTTER ON THE FASCIA (the fascia is wrapped in .032 aluminum to match gutter color) & MATCHING SOFFIT & DOWNSPOUT COLOR
7. EIFS MOULDING, REFER TO WALL SECTIONS FOR PROFILE. MOULDING SHOWN AT THE UNDERSIDE OF THE SOFFIT
8. EIFS MOULDING, REFER TO WALL SECTIONS FOR PROFILE. MOULDING SHOWN AT THE JOINT OF THE EIFS TO THE MASONRY UNITS; THERE IS AN EIFS BAND BETWEEN MOULDIGS AS INDICATED ON THE WALL SECTIONS
9. GOOSENECK LIGHT FIXTURE AS SELECTED BY OWNER (SATIN NICKEL COLOR)
10. JUMBO BRICK COLOR AS SUBMITTED TO THE LOCAL AUTHORITIES; RED BRICK COLOR: TRIANGLE BRICK CO. "WINDSOR" BRICK
11. LIMESTONE HEADER ABOVE WINDOW. SAME MATERIAL AS SILL.
12. WINDOW FRAME (clear anodized and thermally broken #1" insulated glass) REFER TO PLANS AND WINDOW INFO AND SCHEDULES, ETC.
13. LIMESTONE SILL CUT AND LAID IN A MANNER THAT SHEDS WATER AWAY FROM THE WINDOW; PLACE AND SEAL FLASHING ATOP STONE SILL (window sill must slope water away from the window, less as required)
14. BRICK WALL UNITS SHOWN BELOW THE WINDOW; REFER TO NOTE "10a" FOR TYPE OF BRICK...
15. TOP OF CONCRETE WALK/FLOOR LINE. CONCRETE WALK AROUND BUILDG. AT THIS FLOOR LINE, SLOPE AWAY FROM THE BUILDING 1/8"/FT. REFER TO SITE DRAWINGS FOR LOCATIONS AND DETAILS.
16. GRADE LINE, REFER TO SITE DRAWINGS.
17. FOUNDATION WALL BELOW GRADE.
18. TOP/FTG.
19. BOTTOM OF FOOTING.
20. LIMESTONE FACE ON THREE SIDES FOR THE BOTTOM (5) 8" COURSES (3'-4") CONSTRUCT NEW MAIN ENTRY SRUCTURE w/EIFS AS INDICATED; THE EIFS ON ALL THREE SIDES OF THE 2'x 2' BUILDOUT SHALL HAVE IMPACT RESISTANT EIFS TO A HEIGHT OF (AT LEAST 7'-4" FROM THE TOP OF THE LIMESTONE BASE AT THE BOTTOM, REFER TO THE WALL SECTIONS
21. FOUNDATION TO SUPPORT THE ENTRY BUILDOUT, REFER TO THE FOUNDATION PLANS AND WALL SECTIONS.
22. WALL PAK LIGHT FIXTURE; REFER TO LIGHTING PLAN
23. WALL w/TRIPLE JACK STUD AND SINGLE KING STUD EACH SIDE OF THE 24" WALL BETWEEN WINDOW OPENINGS; TO SUPPORT LINTEL ABOVE; REFER TO FRAMING PLANS...
24. ENTRY DOOR AND FRAME/TRANSOM: REFER TO DOOR AND FRAME SCHEDULE; COLOR AS LISTED BELOW.
25. REAR DOOR AND FRAME; REFER TO DOOR AND FRAME SCHEDULE; COLOR AS SELECTED by OWNER.
26. GROUTED SOLID BLOCK FOUNDATION; REFER TO FOUNDATION PLAN & DETAILS (rebar reinf.)
27. MASONRY DUMPSTER ENCLOSURE CONSTR. OF THE SAME MASONRY MATERIALS TO A HEIGHT OF 8' FROM BLDG. FLOOR TO THE TOP OF STONE COPING; REFER TO WALL SECTION 3 7 A-11.
28. ENTRY GATE w/CANE BOLT; DOOR TYPE AND STYLE AND MATERIAL AS SELECTED BY OWNER. COLOR AS SELECTED by OWNER.
29. DUMPSTER ENTRY GATE w/CANE BOLT; DOOR TYPE AND STYLE AND MATERIAL AS SELECTED BY OWNER. (double door to appear as the two of the single coor) COLOR AS SELECTED by OWNER.

BUILDING MATERIAL COLORS

- ALL EIFS MATERIALS
  PLEASE NOTE: ALL EIFS MATERIALS SHALL BE "STOW 93240 - SMOKED PUTTY"
- ALL FINISH ALUMINUM (except window frames)
  FASCIA, SOFFIT, TRIM, COPING AND GUTTER MATERIALS; ALCOA MASTIC "DESERT SAND"
- GOOSENECK LIGHTS
  SATIN NICKEL
- ROOF SHINGLES
  OWENS CORNING ARCHITECTURAL LAMINATED SHINGLES (same line appearance) "DIAMONDE DRIFTWOOD" LAMINATED SHINGLES
- BRICK
  RED BRICK COLOR: TRIANGLE BRICK CO. "WINDSOR" BRICK
- WINDOW / DOOR FRAME
  CLEAR ANODIZED ALUMINUM
- REAR DOOR AND FRAME
  AURORA BROWN SW7837 PAINT by SHERWIN WILLIAMS or EQUIVALENT
- DUMPSTER GATE
  AURORA BROWN SW7837 SHERWIN WILLIAMS or EQUIVALENT
- SIGNAGE ABOVE THE DOOR
  THE SIGNAGE COLOR AND GRAPHICS WILL BE SUBMITTED TO THE LOCAL AUTH. IN THE PROCESS OF SUMBITTING FOR A SIGN PERMIT

LICENSED ARCHITECT — FRANK A. ROSS 21587 — STATE OF ILLINOIS

expires:* 11-30-2016*

signature:

date:

*subject to renewal in accordance w/ the Illinois Architecture Practice Act of 1989 (225 ICLS 305 16)
expiration date as listed references the current expiration date

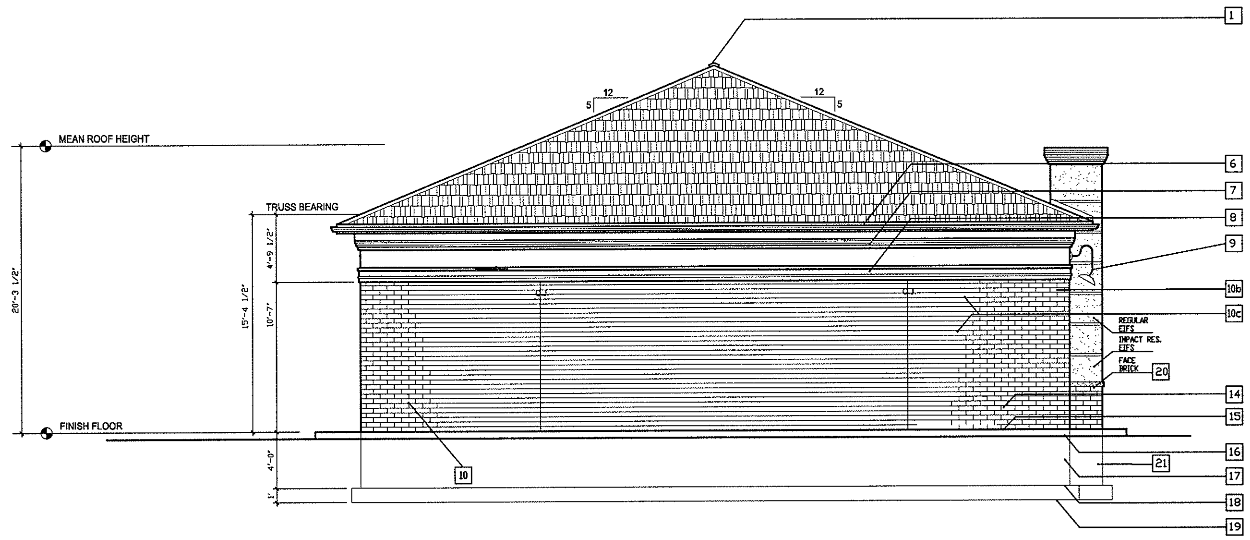
F.A. Ross architecture

F. A. Ross - Architects
P.O. BOX 5073
NEW CASTLE, PA 16105
PHONE: (724) 658-7886
FAX: (724) 658-7892

drawing: Preliminary Building elev. and notes
project: Truenorth
Downer's Grove, Illinois

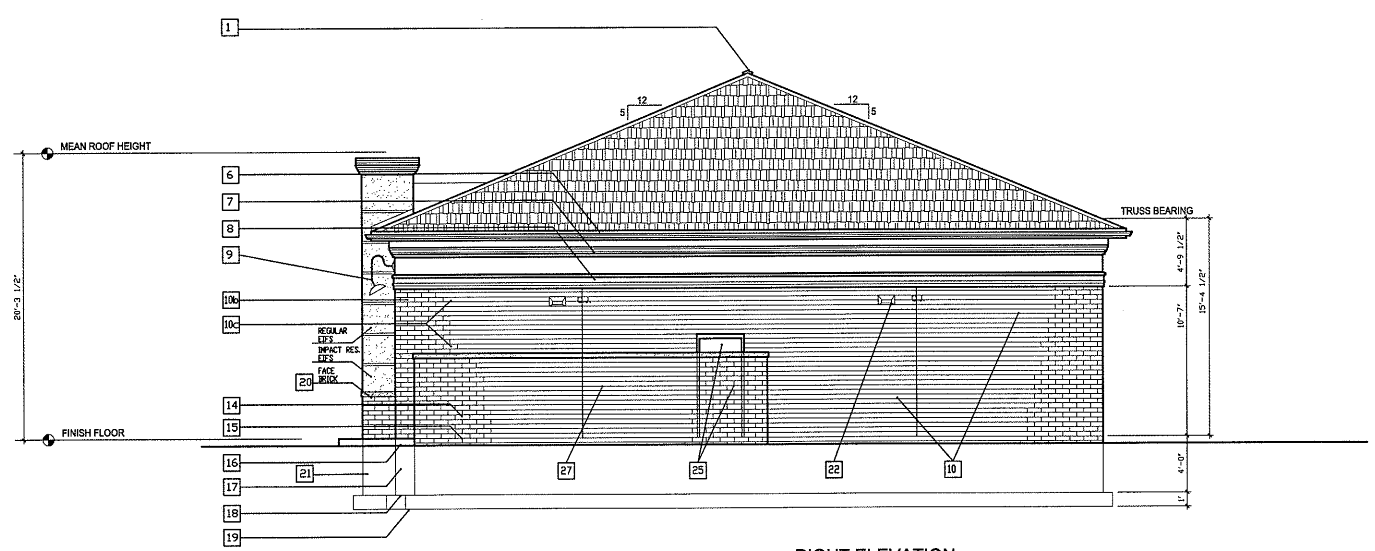
date
6/04/2015
project number
TN-#
sheet -

A-1



LEFT ELEVATION

SCALE: 3/16" = 1'-0"



RIGHT ELEVATION

SCALE: 3/16" = 1'-0"

CODED NOTES

For the new elevations...

- 1 RIDGE VENT w/FILTER FABRIC AND MATCHING SHINGLES
- 2 OWENS CORNING ARCHITECTURAL LAMINATED SHINGLES (slate like appearance) "OAKRIDGE DRIFTWOOD" LAMINATED SHINGLES (use installation procedures and nails, etc. for 110 mph)
- 3 .050 ALUMINUM CAP OVER THE TOP (CAP) OF THE ENTRY STRUCTURE w/KYNAR FINISH. BEND ALUMINUM TO CREATE POSITVE SLOPE AWAY ON ALL SIDES, w/EIFS MOULDING BELOW, REFER TO WALL SECTIONS.
- 4 CONSTRUCT NEW MAIN ENTRY SRUCTURE w/EIFS AS INDICATED (this note points to the eifs atop the medallion) FRAMING, ROOF PLANS & THE ELEVATIONS & WALL SECTIONS
- 4a IMPACT RESISTANT MESH AND EIFS SYSTEM FROM THE TOP/LIMESTONE BLOCK TO A HEIGHT OF 7'-4" AFF.
  TRANSITION FROM 4a TO 4b AT AN INDENTATION JOINT...
- 4b STANDARD EIFS MESH & INSULATION SYSTEM FROM 7'-4" aff TO THE TOP
  TRANSITION FROM 4a TO 4b AT AN INDENTATION JOINT...
  PLEASE REFER TO NOTE NUMBER "20" AS IT RELATES TO THE BOTTOM COURSE OF THE ENTRY STRUCTURE...
- 5 "TRUENORTH" ENTRY INTERNALLY ILLUMINATED ENTRY MEDALLION (color and graphics to be submitted w/ sign permit)
- 6 .032 ALUMINUM GUTTER ON THE FASCIA (the fascia is wrapped in .032 aluminum to match gutter color) & MATCHING SOFFIT & DOWNSPOUT COLOR
- 7 EIFS MOULDING, REFER TO WALL SECTIONS FOR PROFILE. MOULDING SHOWN AT THE UNDERSIDE OF THE SOFFIT
- 8 EIFS MOULDING, REFER TO WALL SECTIONS FOR PROFILE. MOULDING SHOWN AT THE JOINT OF THE EIFS TO THE MASONRY UNITS; THERE IS AN EIFS BAND BETWEEN MOULINGS AS INDICATED ON THE WALL SECTIONS
- 9 GOOSENECK LIGHT FIXTURE AS SELECTED BY OWNER (SATIN NICKEL COLOR)
- 10 JUMBO BRICK COLOR AS SUBMITTED TO THE LOCAL AUTHORITIES. RED BRICK COLOR: TRIANGLE BRICK CO. "WINDSOR" BRICK
- 11 LIMESTONE HEADER ABOVE WINDOW. SAME MATERIAL AS SILL.
- 12 WINDOW FRAME (clear anodized and thermally broken w/1" insulated glass) REFER TO PLANS AND WINDOW INFO AND SCHEDULES, ETC.
- 13 LIMESTONE SILL CUT AND LAID IN A MANNER THAT SHEDS WATER AWAY FROM THE WINDOW; PLACE AND SEAL FLASHING ATOP STONE SILL (window sill must slope water away from the window, seal as required)
- 14 BRICK WALL UNITS SHOWN BELOW THE WINDOW; REFER TO NOTE "10c" FOR TYPE OF BRICK....
- 15 TOP OF CONCRETE WALK/FLOOR LINE. CONCRETE WALK AROUND BUILD'G. AT THE FLOOR LINE, SLOPE AWAY FROM THE BUILDING 1/8"/FT. REFER TO SITE DRAWINGS FOR LOCATIONS AND DETAILS.
- 16 GRADE LINE, REFER TO SITE DRAWINGS.
- 17 FOUNDATION WALL BELOW GRADE.
- 18 TOP/FTG.
- 19 BOTTOM OF FOOTING.
- 20 LIMESTONE FACE ON THREE SIDES FOR THE BOTTOM (5) 8" COURSES (3'-4")
  CONSTRUCT NEW MAIN ENTRY SRUCTURE w/EIFS AS INDICATED; THE EIFS ON ALL THREE SIDES OF THE 2' x 2' BUILDOUT SHALL HAVE IMPACT RESISTANT EIFS TO A HEIGHT OF AT LEAST 7' - 4" FROM TOP OF THE 4" LIMESTONE BASE AT THE BOTTOM, REFER TO THE WALL SECTIONS
- 21 FOUNDATION TO SUPPORT THE ENTRY BUILDOUT, REFER TO THE FOUNDATION PLANS AND WALL SECTIONS.
- 22 WALL PAK LIGHT FIXTURE; REFER TO LIGHTING PLAN
- 23 WALL w/TRIPLE JACK STUD AND SINGLE KING STUD EACH SIDE OF THE 24" WALL BETWEEN WINDOW OPENINGS; TO SUPPORT LINTEL ABOVE; REFER TO FRAMING PLANS...
- 24 ENTRY DOOR AND FRAME/TRANSOM; REFER TO DOOR AND FRAME SCHEDULE; COLOR AS LISTED BELOW.
- 25 EXIT DOOR AND FRAME; REFER TO DOOR AND FRAME SCHEDULE; COLOR AS SELECTED by OWNER.
- 26 GROUTED SOLID BLOCK FOUNDATION; REFER TO FOUNDATION PLAN & DETAILS (rebar reinf.)
- 27 MASONRY DUMPSTER ENCLOSURE CONSTR. OF THE SAME MASONRY MATERIALS TO A HEIGHT OF 6' FROM BLDG. FLOOR TO THE TOP OF STONE COPING; REFER TO WALL SECTION 3 / A-11.
- 28 ENTRY GATE w/CANE BOLT; DOOR TYPE AND STYLE AND MATERIAL AS SELECTED BY OWNER. COLOR AS SELECTED by OWNER.
- 29 DUMPSTER ENTRY GATE w/CANE BOLT; DOOR TYPE AND STYLE AND MATERIAL AS SELECTED BY OWNER. (double door to appear as the two of the single door) COLOR AS SELECTED by OWNER.

BUILDING MATERIAL COLORS

ALL EIFS MATERIALS
PLEASE NOTE: ALL EIFS MATERIALS SHALL BE "STOW 93240 - SMOKED PUTTY"

ALL FINISH ALUMINUM (except window frames)
FASCIA, SOFFIT, TRIM, COPING AND GUTTER MATERIALS; ALCOA MASTIC "DESERT SAND"

GOOSENECK LIGHTS
SATIN NICKEL

ROOF SHINGLES
OWENS CORNING ARCHITECTURAL LAMINATED SHINGLES (slate like appearance) "OAKRIDGE DRIFTWOOD" LAMINATED SHINGLES

BRICK
RED BRICK COLOR: TRIANGLE BRICK CO. "WINDSOR" BRICK

WINDOW / DOOR FRAME
CLEAR ANODIZED ALUMINUM

REAR DOOR AND FRAME
AURORA BROWN SW2837 PAINT by SHERWIN WILLIAMS or EQUIVALENT

DUMPSTER GATE
AURORA BROWN SW2837 SHERWIN WILLIAMS or EQUIVALENT

SIGNAGE ABOVE THE DOOR
THE SIGNAGE COLOR AND GRAPHICS WILL BE SUBMITTED TO THE LOCAL AUTH. IN THE PROCESS OF SUMBITTING FOR A SIGN PERMIT

LICENSED ARCHITECT FRANK A. ROSS 21587 STATE OF ILLINOIS

expires:* 11-30-2016*
signature:
date:

*subject to renewal in accordance w/ the Illinois Architecture Practice Act of 1989 (225 ILCS 305 16) expiration date as listed references the current expiration date

F. A. ROSS architecture

F. A. Ross - Architects
P. O. BOX 5073
NEW CASTLE, PA 16105
PHONE: (724) 658-7886
FAX: (724) 658-7892

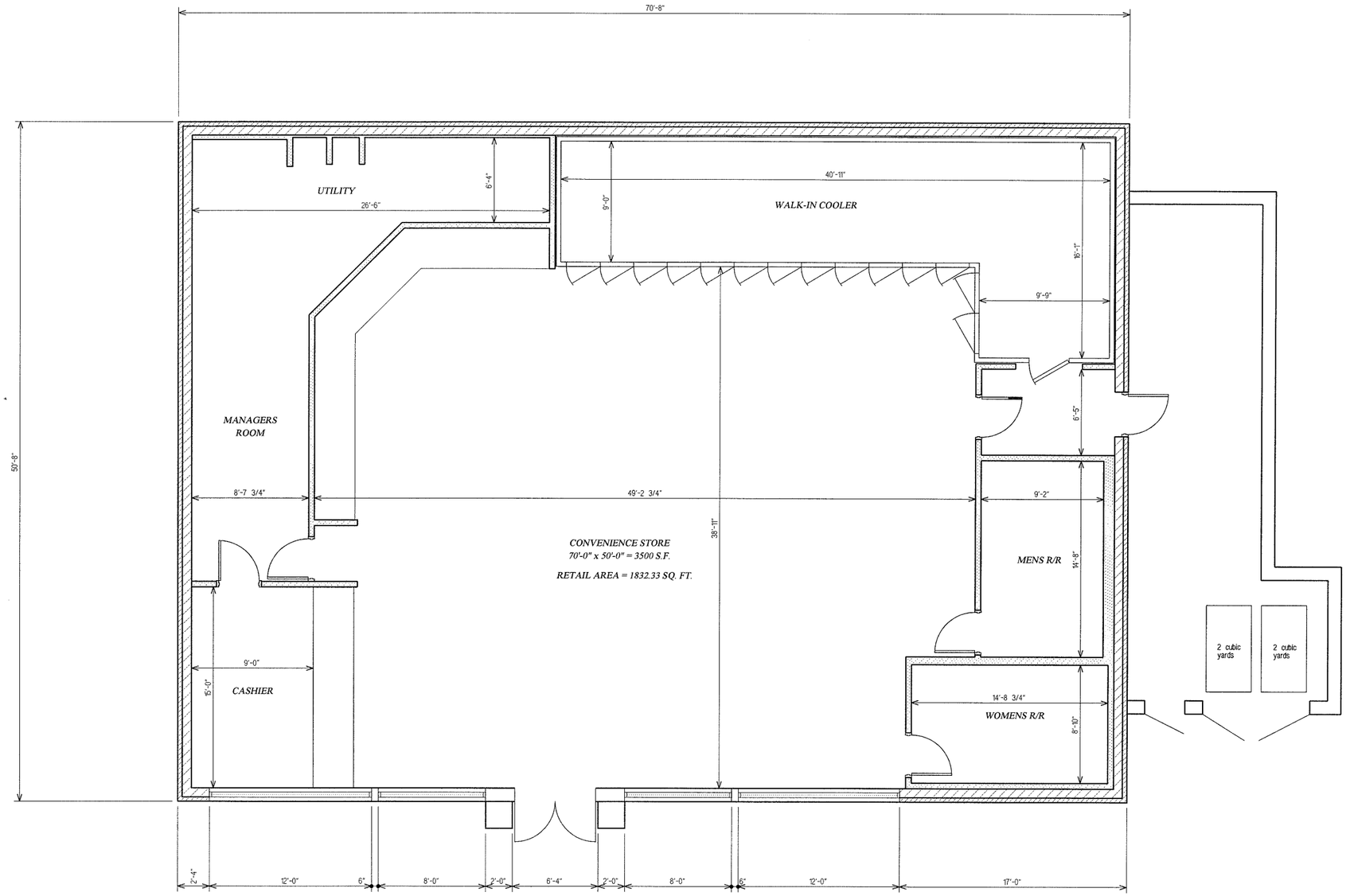
drawing: Preliminary Building elev. and notes

project: Truenorth Downer's Grove, Illinois

date: 6/04/2015

project number: TN-#

sheet: A-2



FLOOR PLAN

Ambrose Design Group
PO BOX 1870
CRYSTAL LAKE, ILLINOIS
60039-1870
(847) 347-3721

NO	DESCRIPTION	DATE
	REVISIONS	
	UPDATE PER SITE VISIT	10-8-14
	REVISE PER CITY REVIEW COMMENTS	1-23-15
	REVISE TRASH ENCLOSURE	1-27-15

PROJECT NAME AND LOCATION:
PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210-2250 W. OGDEN AT BELMONT
DOWNERS GROVE, ILLINOIS
PIN: 08-01-402-007 (006)

CLIENT:
truenorth energy, llc

SHEET TITLE:
FLOOR PLAN

DATE:
10-15-14

SCALE:
1/4" = 1'-0"

DRAWN BY:
RJA

PROJECT NO.:
141001

SHEET
A-3
OF TWO

GHA GEWALT HAMILTON ASSOCIATES, INC.
CONSULTING ENGINEERS
625 Forest Edge Drive, Vernon Hills, IL 60061
TEL 847.478.9700 ▪ FAX 847.478.9701
www.gha-engineers.com

TRAFFIC IMPACT STUDY

To: **Ron Ambrose**
Ambrose Design Group

From: Bill Grieve BG
Senior Transportation Engineer, P.E., PTOE

Date: August 3, 2015

Subject: ***Proposed Shell Gas Station Remodeling***
Ogden Avenue (US 34) / Warrenville Road / Finley Road
Downers Grove, Illinois

PART I – PROJECT CONTEXT AND SUMMARY STATEMENT

GEWALT HAMILTON ASSOCIATES, INC. (GHA) has conducted a traffic impact study (TIS) for the above referenced project. As we understand the project, the owner proposes to remodel the existing Shell gas station with a new convenience store at the west end of the property that will replace the existing car wash and two more fueling positions. The Shell is bounded by Ogden Avenue (US 34) on the south, Warrenville Road on the north, and Finley Road on the east in Downers Grove, Illinois.

The following summarizes our findings and provides various recommendations for your consideration. *Exhibits* referenced are located at the end of this document. Briefly summarizing, we believe that the new Shell traffic generated can be integrated on the adjacent roads and streets. Reasons include:

- The site is located among very busy routes that can take advantage of travel patterns that will be predominantly right turn movements.
- The access drives have been designed in conformance with Village planning guidelines.
- New traffic generated by the new convenience store and additional fueling positions will be limited.

PART II - BACKGROUND INFORMATION

Site Location Aerial and Photo Inventory

Exhibit 1 provides a site location aerial and *Exhibit 2* provides a photo inventory of current traffic operations. Pertinent comments include:

- Ogden Avenue (US 34) is an arterial east-west roadway that is under the jurisdiction of the Illinois Department of Transportation (IDOT), but is not classified as a Strategic Regional Arterial (SRA) route. Ogden Avenue has a five lane pavement section that has separate left turn lanes striped at major intersections such as at Finley Road. Separate right turn lanes are also provided on Ogden at Finley Road, which has traffic signal control. The posted speed limit on Ogden is 35-mph.

- Warrenville Road (CR 3) is a major east-west route that is under the jurisdiction of the DuPage County Division of Transportation (DCDOT). Warrenville Road has its eastern terminus at Finley Road and accessibility is limited to right turns only via a large channeling island due to its very close proximity to Ogden Avenue. Warrenville has a three lane pavement section along the site and has a posted speed limit of 40-mph. Eastbound Warrenville Road has Stop control at Finley Road.
- Finley Road (CR 2) is a major north-south roadway that is also under DCDOT jurisdiction. Finley Road has a five lane pavement section and separate left turn lanes are provided at Ogden Avenue. South of Ogden Avenue, Finley Road becomes Belmont Road. Finley has a posted speed limit of 45-mph just north of Warrenville Road.
- The Shell gas station currently has four full access drives; two each on Ogden Avenue and Warrenville Road.

Existing and Baseline Traffic

GHA conducted weekday morning and evening peak period traffic counts at the Shell drives in June 2015. No unusual events occurred during the traffic counts, such as road construction, severe weather, or emergency vehicle activity that would affect the gas station traffic counts or travel patterns. The gas station turning data was augmented by IDOT through traffic counts conducted in 2013 (see *Appendix A*). *Exhibit 3A* illustrates the Existing Traffic volumes.

Per Village planning guidelines along the Ogden Avenue corridor, the east drive on Ogden Avenue will be restricted to right turns in only and the east drive on Warrenville Road will be restricted to right turns out only. The existing traffic volumes were adjusted for the changes in access. The resulting Baseline Traffic volumes are illustrated in *Exhibit 3B*.

PART III - TRAFFIC PROJECTIONS

Site Plan

Exhibit 4 provides the site plan for the Shell gas station remodeling prepared by Ambrose Design Group. As proposed, a new convenience store will be built on the west end of the site replacing the existing car wash. The existing convenience store kiosk located among the fuel pumps will be eliminated and there will be 2 more vehicle fueling positions provided for a total of 16.

Project Traffic Characteristics

Exhibit 5 – Part A tabulates the traffic generation calculations for the proposed development. Traffic generations are based on historically observed trip rate data published by the Institute of Transportation Engineers (ITE) in the most recent, 9th Edition of the manual *Trip Generation.* For the proposed Shell gas station remodeling, the generations were based on the size of the retail space within the convenience store and the number of additional fueling positions that will be provided.

GHA surveys at various gas stations indicate that 50-70% of convenience store trips are also combined with a fueling stop. As can be seen, the Shell station is expected to generate 50 and 61 (combined entering and exiting) driveway trips during the weekday Morning and Evening Peak Hours respectively.

Discussion Point. ITE indicates that 60% or more of peak hour gas station trips are made by people already traveling the area roadways, such as a stop on the way to work in the morning for fuel and convenience store items. This available trip discount was not taken, so as to help ensure that the maximum new site traffic impacts are tested.

Exhibit 5 – Part B presents the anticipated trip distribution, which is primarily based on the current and anticipated travel patterns. As can be seen, the vast majority of new site trips are expected to take advantage of convenient right turn movements.

Traffic Assignments

Exhibit 6 illustrates the site traffic assignment, which is based on the traffic characteristics summarized in *Exhibit 5* (e.g. traffic generations and trip distribution) and the site access system. Site traffic and Baseline volumes (see *Exhibits 3B and 6*) were combined to produce the Total Traffic Assignment, which is illustrated in *Exhibit 7*.

PART IV - TRAFFIC EVALUATION

Intersection Capacity Analyses

The analysis parameters are listed in *Exhibit 8 - Part A*, as published in the Transportation Research Board's (TRB) 2010 Highway Capacity Manual (HCM). At unsignalized intersections, the HCM methodology reports the results differently for Two-Way Stop Controlled (TWSC) or All-Way Stop Controlled (AWSC) intersections.

For TWSC intersections, Level of Service (LOS) is "reported" for conflicting movements on the major street (i.e. left turns onto the minor approach) and for each movement on the stopped approach. Approach "control delay" is also reported in seconds per vehicle. Results of AWSC analyses are slightly different. LOS is reported for each stopped approach as well as an Average overall intersection "control delay" in seconds per vehicle. With both analyses, LOS A is best and LOS F is worst. LOS C and D are considered as "design" and "acceptable" operations respectively.

Exhibit 8 – Part B summarizes the intersection capacity analysis results for the weekday morning and evening peak hours and the capacity analysis worksheets are provided in *Appendix B*. As can be seen, left turns out of the west drives on both Ogden Avenue and Warrenville Road will experience lengthy delays during both the morning and evening peak hours, which is a very common occurrence at many locations throughout the Chicagoland area.

Recommended Traffic Operations

Ogden Avenue (US 34) Access Drives

- As noted, the east drive will be restricted to right turns in only. A review of the Total Traffic volumes (see *Exhibit 7*) indicates that a separate westbound right turn lane is not needed.
- The west drive is to be relocated very slightly to the east. As can be seen from the capacity analyses (see *Exhibit 8*), long delays can be expected during the peak hours due to the heavy through traffic volumes on Ogden Avenue. This suggests that two outbound lanes be provided so that exiting right turns can proceed while the occasional driver exiting left out awaits a gap in both directions of through traffic on Ogden Avenue. A driveway width of 35 feet, with one 15-foot inbound lane and two 10-foot outbound lanes striped for separate left and right turns, is a typical IDOT standard dimension. Exiting traffic should have Stop control.

Warrenville Road (CR 3) Access Drives

- As noted, the east drive will be restricted to right turns out only. Exiting gas station traffic should have Stop control.
- The west drive is to be relocated slightly to the east. As with the west access drive on Ogden Avenue, consideration should be given to providing one inbound and two outbound lanes. Exiting traffic should have Stop control.

PART VI - TECHNICAL ADDENDUM

The following *Exhibits* and *Appendices* were previously referenced. They provide technical support for our observations, findings, and recommendations discussed in the text.

Exhibits

1. Site Location Aerial
2. Photo Inventory
3A. Existing Traffic
3B. Baseline Traffic
4. Site Plan
5. Project Traffic Characteristics
6. New Site Traffic
7. Total Traffic
8. Intersection Capacity Analyses

Appendices

A. Traffic Count Summaries
B. Capacity Analysis Worksheets

EXHIBITS

Shell Gas Station – Downers Grove, Illinois

GHA GEWALT HAMILTON ASSOCIATES, INC.

Exhibit 1
Site Aerial

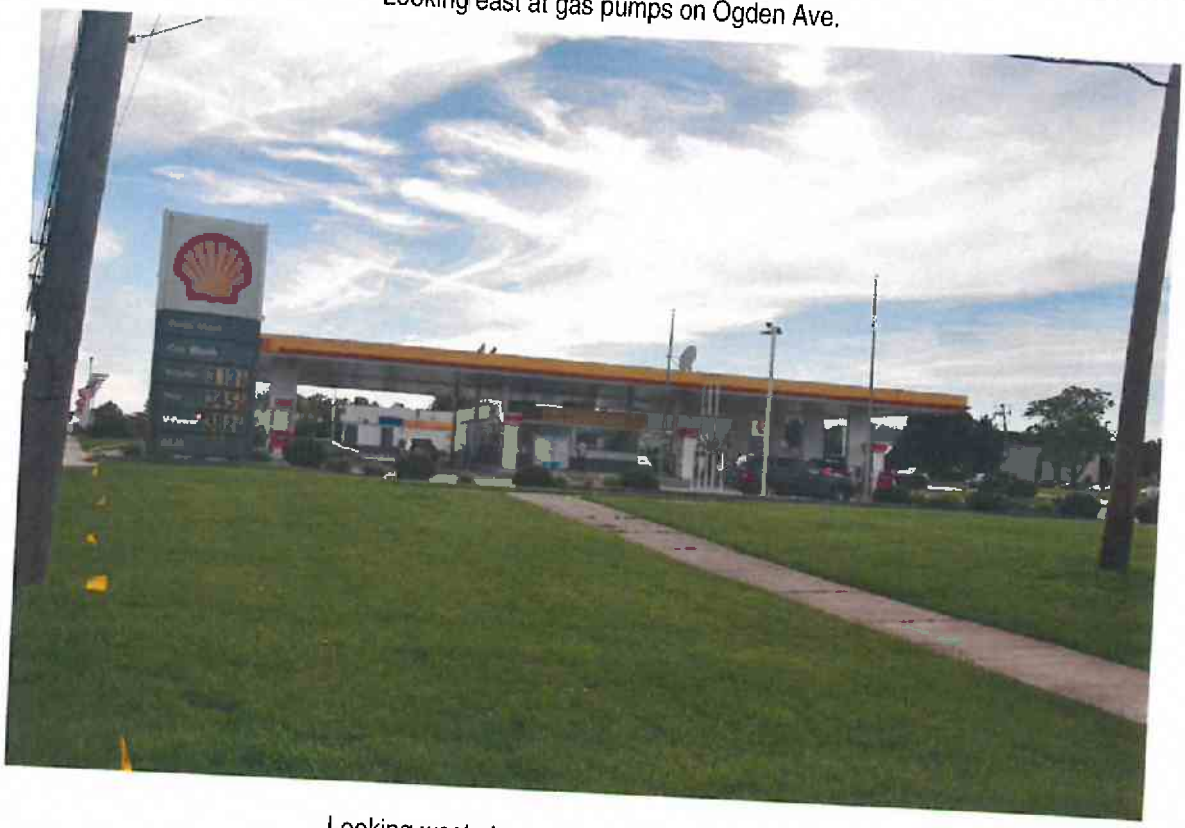
Looking at east drive on Ogden Ave.

Looking at west drive on Ogden Ave.

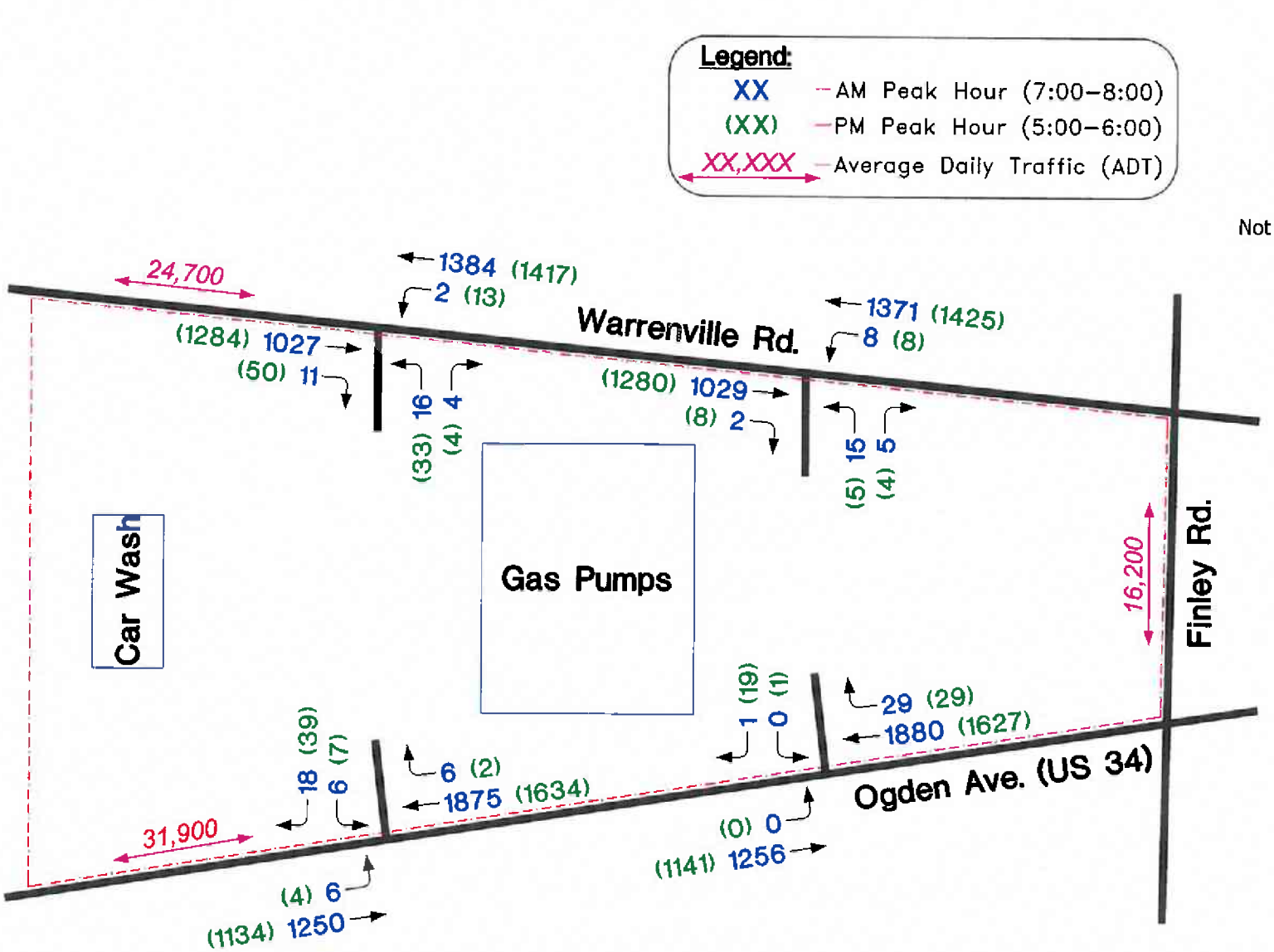
Looking at east drive on Warrenville Rd.

Looking at west drive on Warrenville Rd.

Looking east at gas pumps on Ogden Ave.

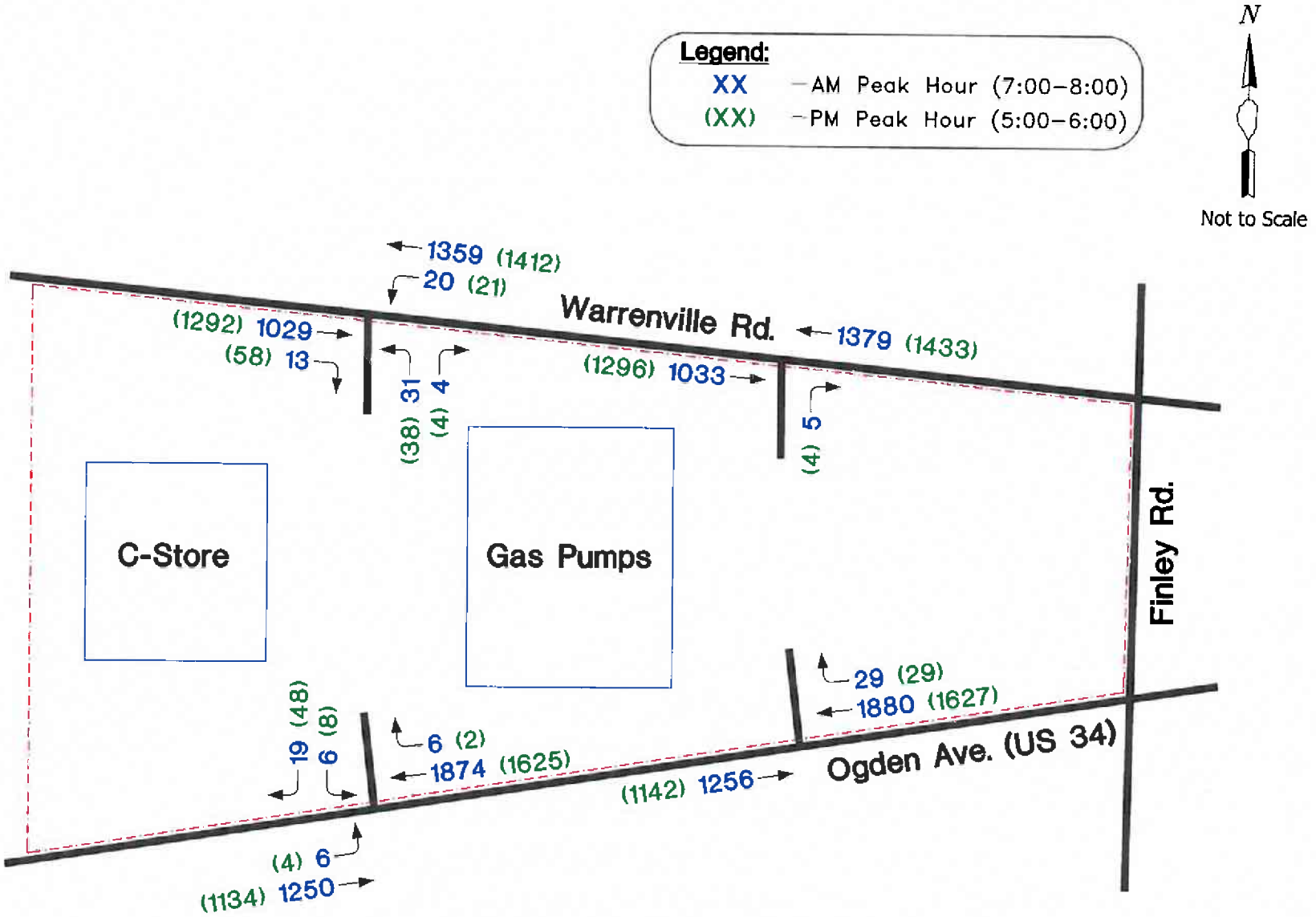


Looking west at gas pumps from Belmont Rd.



GHA GEWALT HAMILTON ASSOCIATES, INC.

Exhibit 3A
Existing Traffic
Sources: GHA June 2015, IDOT 2013



GHA GEWALT HAMILTON ASSOCIATES, INC.

Exhibit 3B
Baseline Traffic

DOWNERS GROVE ZONING ANALYSIS

ZONING DISTRICT : B3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO NORTH : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO SOUTH : B 3 GENERAL SERVICES AND HIGHWAY BUSINESS
ZONING DISTRICT TO EAST : M-1 LIGHT MANUFACTURING
ZONING DISTRICT TO WEST . B-3 GENERAL SERVICES AND HIGHWAY BUSINESS

SETBACK REQUIREMENTS:
- OGDEN AVENUE (FROM ROW CENTERLINE) = 75'-0" PRINCIPAL BLDG
- OGDEN AVENUE (FROM ROW CENTERLINE) = 50'-0" CANOPY
- WARRENVILLE ROAD = 25'-0"
- SIDE YARD (WEST PROPERTY LINE) = NONE REQUIRED

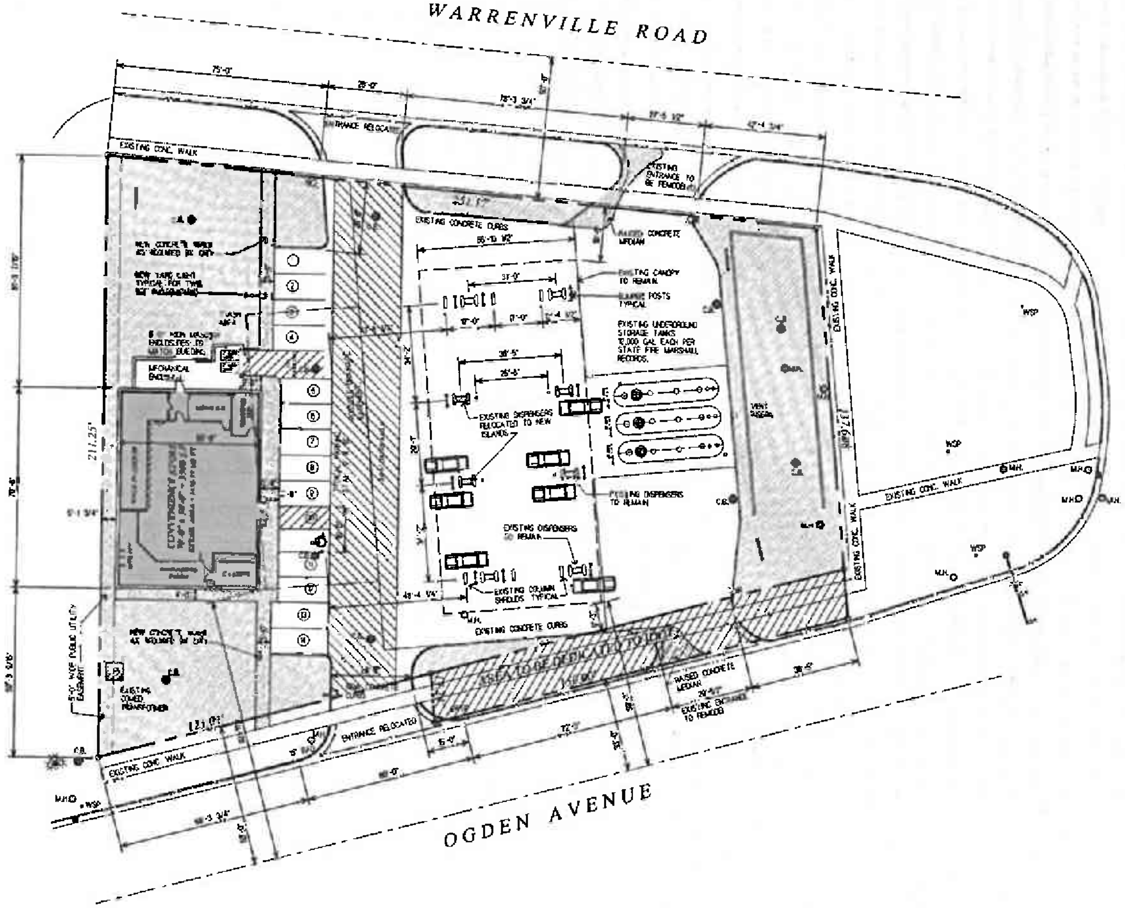
CONVENIENCE STORE IS A PERMITTED USE IN B-3 DISTRICT
FUELING STATION IS A SPECIAL USE IN B-3 DISTRICT

IDOT ENCROACHMENT AREA =2549.57 SQ. FT. OR 0.0585 ACRES
SITE AREA = 45,068 SQ. FT. OR 1.035 ACRES
TOTAL SITE AREA LESS ENCROACHMENT =42,538.43 SQ. FT. OR 0.976 ACRES

PARKING REQUIREMENT : RETAIL = 4/1000 GROSS SF
- RETAIL = 4/1000 LESS STORAGE AREA (1700 SF)
- One Space per pump island = 8 required
- 3.13 spaces per 1000 sf of retail area = 3.13/1000 x 1832 = 6

Total parking required = 14 stalls

LANDSCAPE CALCULATIONS:
- EXISTING LANDSCAPE GREEN SPACE = 8745 SQ. FT
- 8745/42,538.43 = 20% EXISTING SITE GREEN AREA
- PROPOSED LANDSCAPE GREEN SPACE = 12,868 SQ. FT
- 12,868/42,538.43 = 30.25% PROPOSED SITE GREEN AREA



PROPOSED SITE PLAN - 2
SCALE: 1" = 20'-0"

Ambrose Design Group
PO BOX 1870
CRYSTAL LAKE, ILLINOIS
60039-1870
(847) 347-3721

NO	DESCRIPTION	DATE
	UPDATE PER SITE VISIT	10-16-14
	REVISE PER CITY REVIEW COMMENTS	1-23-15
	REVISE TRASH ENCLOSURE	1-27-15
	REVISE PER CITY REVIEW COMMENTS	6-4-15

REVISIONS

PROJECT NAME AND LOCATION:
PROPOSED SITE REMODEL FOR NEW CONVENIENCE STORE
2210-2250 W. OGDEN AT BELMONT
DOWNERS GROVE, ILLINOIS
PIN 08-01-402-006 (007, 008)

CLIENT:
truenorth energy, llc

SHEET TITLE:
NEW PROPOSED SITE PLAN - 2

DATE: 10-15-14
SCALE: 1" = 20'-0
DRAWN BY: RJA
PROJECT NO.: H1001
SHEET SP-2 OF TWO

Exhibit 5
Project Traffic Characteristics
Proposed Shell Gas Station C-Store - Downers Grove, Illinois

Part A. Trip Generations

Land Uses / Sizes, & ITE Code Number		Morning Peak Hour In	Morning Peak Hour Out	Morning Peak Hour Sum	Evening Peak Hour In	Evening Peak Hour Out	Evening Peak Hour Sum
Convenience Store w/1832 SF Retail Space	# 853	38	37	75	46	47	93
Fueling Positions increase from 14 to 16	# 944	12	12	24	14	14	28
Subtotals =		50	49	99	60	61	121
Less Combined Trips @ 50% =		-25	-24	-49	-30	-30	-60
New Trip Totals =		**25**	**25**	**50**	**30**	**31**	**61**

Notes:

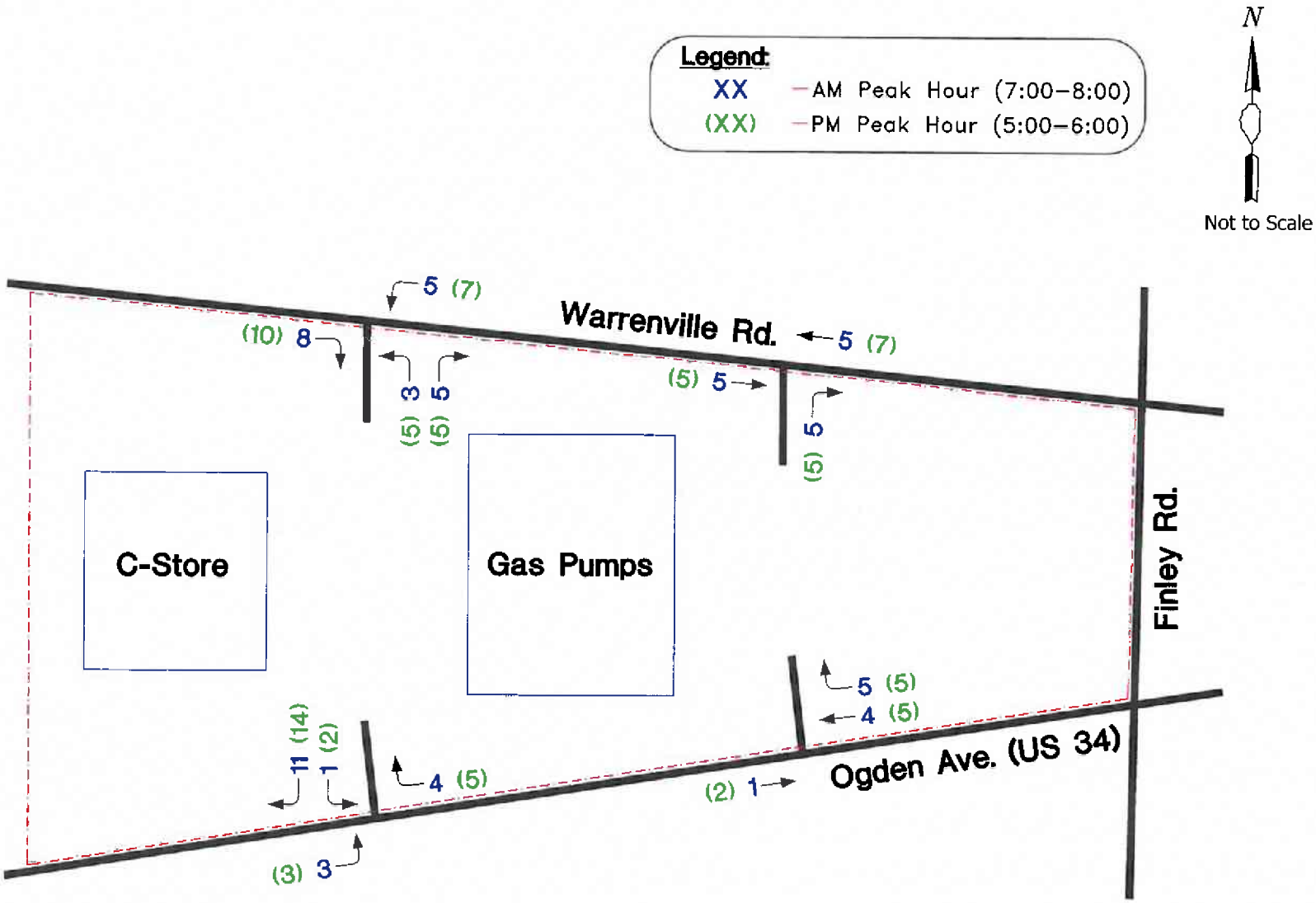
1) Source: ITE Trip Generation Manual; 9th Edition

2) GHA surveys found that combined trips at gas stations ranged from 50-70%.

Part B. Trip Distribution

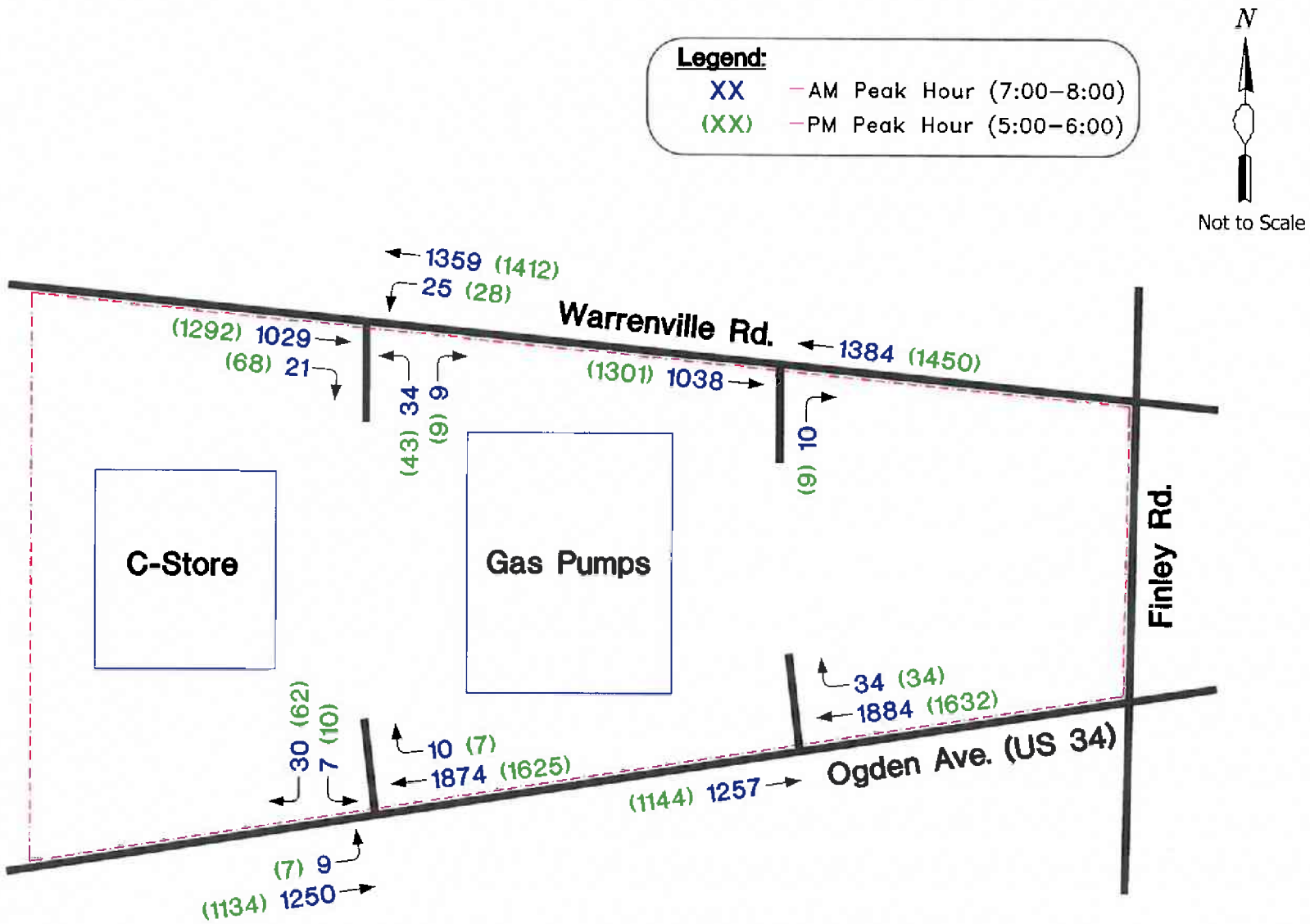
Route & Direction	Percent Use by Route: Arrive from	Percent Use by Route: Depart to
Ogden Avenue (US 34)		
East of Site	35%	5%
West of Site	10%	45%
Warrenville Road		
East of Site	20%	35%
West of Site	35%	15%
Totals =	**100%**	**100%**

GHA GEWALT HAMILTON ASSOCIATES, INC.



GEWALT HAMILTON ASSOCIATES, INC.

Exhibit 6
New Site Traffic



GHA GEWALT HAMILTON ASSOCIATES, INC.

Exhibit 7
Total Traffic

Exhibit 8
Intersection Capacity Analyses
Shell Gas Station - Downers Grove, Illinois

Part A. Parameters - Type of Traffic Control (Source: 2010 Highway Capacity Manual)

I. Traffic Signals

LOS	Delay (sec / veh)	Description
A	≤ 10	All signal phases clear waiting vehicles without delay
B	>10 and ≤ 20	Minimal delay experienced on select signal phases
C	>20 and ≤ 35	Some delay experienced on several phases; often used as design criteria
D	>35 and ≤ 55	Usually considered as the acceptable delay standard
E	>55 and ≤ 80	Very long delays experienced during the peak hours
F	>80	Unacceptable delays experienced throughout the peak hours

II. Stop Sign

LOS	Delay (sec / veh)
A	≤ 10
B	>10 and ≤ 15
C	>15 and ≤ 25
D	>25 and ≤ 35
E	>35 and ≤ 50
F	>50

Part B. Results

Roadway Conditions	LOS Per Movement By Approach (> = Shared Lane; - = Non Critical or not Allowed Movement)												Intersection / Approach	
	Eastbound			Westbound			Northbound			Southbound			Delay	
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	(sec / veh)	LOS
1. Ogden Ave. @ Full Access	**TWSC SB STOPS**												**EB Approach Delay**	
A. Weekday Morning Peak Hour														
Baseline Traffic (See *Exhibit 3B*)	• Current	C	-	-	-	-	-	-	-	-	F	-	<	67.2	F
Total Traffic (*See Exhibit 7*)	• Current	C	-	-	-	-	-	-	-	-	F	-	<	68.2	F
B. Weekday Evening Peak Hour														
Baseline Traffic	• Current	B	-	-	-	-	-	-	-	-	E	-	<	36.5	E
Total Traffic	• Current	B	-	-	-	-	-	-	-	-	E	-	<	41.2	E
2. Warrenville Rd. @ Full Access	**TWSC NB STOPS**												**SB Approach Delay**	
A. Weekday Morning Peak Hour														
Baseline Traffic	• Current	-	-	-	B	-	-	E	-	<	-	-	-	36.7	E
Total Traffic	• Current	-	-	-	B	-	-	E	-	<	-	-	-	46.3	E
B. Weekday Evening Peak Hour														
Baseline Traffic	• Current	-	-	-	B	-	-	E	-	<	-	-	-	48.9	E
Total Traffic	• Current	-	-	-	B	-	-	E	-	<	-	-	-	56.2	F

GHA GEWALT HAMILTON ASSOCIATES, INC.

APPENDIX A

Traffic Count Summaries

Time/Class Report

Device ID: 18819
Operator: JM
Begin: 10/16/2013 12:00 AM
End: 10/17/2013 12:00 AM
Hours: 24.00
Period (min): 60

Location: US 34 WEST OF BELMONT
Lane: COMBINED
Street: US 34
City: 0044
County: 022 0044
State: E

Raw Count: 31,871
AADT Count: 31,871
AADT Factor: 1
Speed Limit: 4

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,10/16/2013									
[00:00-01:00]	152	4	2	0	0	0	0	0	158
[01:00-02:00]	87	0	1	0	0	0	0	0	88
[02:00-03:00]	48	3	2	0	0	0	0	0	53
[03:00-04:00]	60	1	2	0	0	0	0	0	63
[04:00-05:00]	149	8	4	1	0	0	0	0	162
[05:00-06:00]	513	17	9	16	0	0	0	0	555
[06:00-07:00]	1393	35	67	89	0	0	0	0	1584
[07:00-08:00]	2261	83	114	137	0	0	0	0	2595
[08:00-09:00]	2072	87	110	165	0	0	0	0	2434
[09:00-10:00]	1413	79	66	111	0	0	0	0	1669
[10:00-11:00]	1253	86	65	109	0	0	0	0	1513
[11:00-12:00]	1502	82	70	119	0	0	0	0	1773
[12:00-13:00]	1668	82	72	137	0	0	0	0	1959
[13:00-14:00]	1581	71	70	123	0	0	0	0	1845
[14:00-15:00]	1601	89	65	125	0	0	0	0	1880
[15:00-16:00]	1724	73	66	152	0	0	0	0	2015
[16:00-17:00]	2222	54	91	181	0	0	0	0	2548
[17:00-18:00]	2460	45	86	177	0	0	0	0	2768
[18:00-19:00]	1826	39	56	136	0	0	0	0	2057
[19:00-20:00]	1124	23	31	90	0	0	0	0	1268
[20:00-21:00]	1064	17	32	44	0	0	0	0	1157
[21:00-22:00]	774	13	20	38	0	0	0	0	845
[22:00-23:00]	534	7	11	2	0	0	0	0	554
[23:00-00:00]	314	3	8	1	0	0	0	0	326
10/16/2013 12:00 AM 10/17/2013 12:00 AM	27795	1001	1120	1953	0	0	0	0	31869

Time/Class Report

Device ID: 5160
Operator: JM
Begin: 10/16/2013 12:00 AM
End: 10/17/2013 12:00 AM
Hours: 24.00
Period (min): 60

Location: US 34
Lane: AGAINST COMBINED WB
Street: US 34
City: 0044
County: 022 0044
State: E

Raw Count: 19,252
AADT Count: 19,252
AADT Factor: 1
Speed Limit: 4

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,10/16/2013									
[00:00-01:00]	79	1	0	0	0	0	0	0	80
[01:00-02:00]	40	0	1	0	0	0	0	0	41
[02:00-03:00]	21	1	1	0	0	0	0	0	23
[03:00-04:00]	32	0	2	0	0	0	0	0	34
[04:00-05:00]	87	3	2	0	0	0	0	0	92
[05:00-06:00]	329	8	3	1	0	0	0	0	341
[06:00-07:00]	1079	27	36	5	0	0	0	0	1147
[07:00-08:00]	1730	71	73	6	0	0	0	0	1880
[08:00-09:00]	1501	55	46	6	0	0	0	0	1608
[09:00-10:00]	1001	53	25	6	0	0	0	0	1085
[10:00-11:00]	806	64	21	4	0	0	0	0	895
[11:00-12:00]	1020	63	19	3	0	0	0	0	1105
[12:00-13:00]	1151	58	29	7	0	0	0	0	1245
[13:00-14:00]	1036	45	32	1	0	0	0	0	1114
[14:00-15:00]	1048	65	32	6	0	0	0	0	1151
[15:00-16:00]	1047	55	19	6	0	0	0	0	1127
[16:00-17:00]	1393	40	24	6	0	0	0	0	1463
[17:00-18:00]	1560	29	30	8	0	0	0	0	1627
[18:00-19:00]	1141	30	18	5	0	0	0	0	1194
[19:00-20:00]	636	15	12	2	0	0	0	0	665
[20:00-21:00]	540	7	3	1	0	0	0	0	551
[21:00-22:00]	389	8	8	2	0	0	0	0	407
[22:00-23:00]	238	5	3	0	0	0	0	0	246
[23:00-00:00]	125	0	6	0	0	0	0	0	131
10/16/2013 12:00 AM 10/17/2013 12:00 AM	18029	703	445	75	0	0	0	0	19252

Time/Class Report

Device ID: 4983
Operator: JM
Begin: 10/16/2013 12:00 AM
End: 10/17/2013 12:00 AM
Hours: 24.00
Period (min): 60

Location: US 34
Lane: WITH COMBINED EB
Street: US 34
City: 0044
County: 022 0044
State: E

Raw Count: 12,619
AADT Count: 12,619
AADT Factor: 1
Speed Limit: 4

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,10/16/2013									
[00:00-01:00]	73	3	2	0	0	0	0	0	78
[01:00-02:00]	47	0	0	0	0	0	0	0	47
[02:00-03:00]	27	2	1	0	0	0	0	0	30
[03:00-04:00]	28	1	0	0	0	0	0	0	29
[04:00-05:00]	62	5	2	1	0	0	0	0	70
[05:00-06:00]	184	9	6	15	0	0	0	0	214
[06:00-07:00]	314	8	31	84	0	0	0	0	437
[07:00-08:00]	531	12	41	131	0	0	0	0	715
[08:00-09:00]	571	32	64	159	0	0	0	0	826
[09:00-10:00]	412	26	41	105	0	0	0	0	584
[10:00-11:00]	447	22	44	105	0	0	0	0	618
[11:00-12:00]	482	19	51	116	0	0	0	0	668
[12:00-13:00]	517	24	43	130	0	0	0	0	714
[13:00-14:00]	545	26	38	122	0	0	0	0	731
[14:00-15:00]	553	24	33	119	0	0	0	0	729
[15:00-16:00]	677	18	47	146	0	0	0	0	888
[16:00-17:00]	829	14	67	175	0	0	0	0	1085
[17:00-18:00]	900	16	56	169	0	0	0	0	1141
[18:00-19:00]	685	9	38	131	0	0	0	0	863
[19:00-20:00]	488	8	19	88	0	0	0	0	603
[20:00-21:00]	524	10	29	43	0	0	0	0	606
[21:00-22:00]	385	5	12	36	0	0	0	0	438
[22:00-23:00]	296	2	8	2	0	0	0	0	308
[23:00-00:00]	189	3	2	1	0	0	0	0	195
10/16/2013 12:00 AM 10/17/2013 12:00 AM	9766	298	675	1878	0	0	0	0	12617

Time/Class Report

Device ID: 5113	**Location:** WARRENVILLE RD	**Raw Count:** 24,820
Operator: GHA	**Lane:** COMBINED	**AADT Count:** 24,820
Begin: 09/05/2012 12:00 AM	**Street:** WARRENVILLE RD	**AADT Factor:** 1
End: 09/06/2012 12:00 AM	**City:** 0287	**Speed Limit:** 4
Hours: 24.00	**County:** 022 0287	
Period (min): 60	**State:** E	

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,09/05/2012									
[00:00-01:00]	94	1	0	0	0	0	0	0	95
[01:00-02:00]	47	0	0	0	0	0	0	0	47
[02:00-03:00]	26	1	0	0	0	0	0	0	27
[03:00-04:00]	41	1	1	0	0	0	0	0	43
[04:00-05:00]	75	3	0	0	0	0	0	0	78
[05:00-06:00]	425	6	4	0	0	0	0	0	435
[06:00-07:00]	1335	17	11	2	0	0	0	0	1365
[07:00-08:00]	2357	35	8	0	0	0	0	0	2400
[08:00-09:00]	2306	26	11	1	0	0	0	0	2344
[09:00-10:00]	1280	27	9	0	0	0	0	0	1316
[10:00-11:00]	860	23	8	0	0	0	0	0	891
[11:00-12:00]	1054	10	10	0	0	0	0	0	1074
[12:00-13:00]	1253	18	10	1	0	0	0	0	1282
[13:00-14:00]	1122	26	7	2	0	0	0	0	1157
[14:00-15:00]	1177	25	4	0	0	0	0	0	1206
[15:00-16:00]	1557	22	8	0	0	0	0	0	1587
[16:00-17:00]	2294	34	17	0	0	0	0	0	2345
[17:00-18:00]	2667	30	8	0	0	0	0	0	2705
[18:00-19:00]	1630	18	5	0	0	0	0	0	1653
[19:00-20:00]	905	12	1	0	0	0	0	0	918
[20:00-21:00]	655	3	2	0	0	0	0	0	660
[21:00-22:00]	536	2	2	0	0	0	0	0	540
[22:00-23:00]	311	6	0	0	0	0	0	0	317
[23:00-00:00]	181	1	0	0	0	0	0	0	182
09/05/2012 12:00 AM									
09/06/2012 12:00 AM	24188	347	126	6	0	0	0	0	24667

Time/Class Report

Device ID: 3079
Operator: GHA
Begin: 09/05/2012 12:00 AM
End: 09/06/2012 12:00 AM
Hours: 24.00
Period (min): 60

Location: WARRENVILLE RD
Lane: AGAINST COMBINED WB
Street: WARRENVILLE RD
City: 0287
County: 022 0287
State: E

Raw Count: 13,102
AADT Count: 13,102
AADT Factor: 1
Speed Limit: 4

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,09/05/2012									
[00:00-01:00]	33	1	0	0	0	0	0	0	34
[01:00-02:00]	22	0	0	0	0	0	0	0	22
[02:00-03:00]	16	1	0	0	0	0	0	0	17
[03:00-04:00]	20	1	0	0	0	0	0	0	21
[04:00-05:00]	46	3	0	0	0	0	0	0	49
[05:00-06:00]	215	2	2	0	0	0	0	0	219
[06:00-07:00]	721	13	2	1	0	0	0	0	737
[07:00-08:00]	1344	22	5	0	0	0	0	0	1371
[08:00-09:00]	1286	18	7	0	0	0	0	0	1311
[09:00-10:00]	706	14	3	0	0	0	0	0	723
[10:00-11:00]	450	13	2	0	0	0	0	0	465
[11:00-12:00]	507	5	5	0	0	0	0	0	517
[12:00-13:00]	613	9	5	0	0	0	0	0	627
[13:00-14:00]	593	14	2	0	0	0	0	0	609
[14:00-15:00]	646	15	2	0	0	0	0	0	663
[15:00-16:00]	728	14	3	0	0	0	0	0	745
[16:00-17:00]	1091	17	7	0	0	0	0	0	1115
[17:00-18:00]	1408	12	5	0	0	0	0	0	1425
[18:00-19:00]	859	9	3	0	0	0	0	0	871
[19:00-20:00]	502	6	0	0	0	0	0	0	508
[20:00-21:00]	353	2	0	0	0	0	0	0	355
[21:00-22:00]	321	2	1	0	0	0	0	0	324
[22:00-23:00]	167	4	0	0	0	0	0	0	171
[23:00-00:00]	103	0	0	0	0	0	0	0	103
09/05/2012 12:00 AM									
09/06/2012 12:00 AM	12750	197	54	1	0	0	0	0	13002

Time/Class Report

Device ID: 5113	**Location:** WARRENVILLE RD	**Raw Count:** 11,718
Operator: GHA	**Lane:** WITH COMBINED EB	**AADT Count:** 11,718
Begin: 09/05/2012 12.00 AM	**Street:** WARRENVILLE RD	**AADT Factor:** 1
End: 09/06/2012 12:00 AM	**City:** 0287	**Speed Limit:** 4
Hours: 24.00	**County:** 022 0287	
Period (min): 60	**State:** E	

Date And Time Range	< to 22	23 to 39	40 to 99	100 to >					Total
Wed,09/05/2012									
[00:00-01:00]	61	0	0	0	0	0	0	0	61
[01:00-02:00]	25	0	0	0	0	0	0	0	25
[02:00-03:00]	10	0	0	0	0	0	0	0	10
[03:00-04:00]	21	0	1	0	0	0	0	0	22
[04:00-05:00]	29	0	0	0	0	0	0	0	29
[05:00-06:00]	210	4	2	0	0	0	0	0	216
[06:00-07:00]	614	4	9	1	0	0	0	0	628
[07:00-08:00]	1013	13	3	0	0	0	0	0	1029
[08:00-09:00]	1020	8	4	1	0	0	0	0	1033
[09:00-10:00]	574	13	6	0	0	0	0	0	593
[10:00-11:00]	410	10	6	0	0	0	0	0	426
[11:00-12:00]	547	5	5	0	0	0	0	0	557
[12:00-13:00]	640	9	5	1	0	0	0	0	655
[13:00-14:00]	529	12	5	2	0	0	0	0	548
[14:00-15:00]	531	10	2	0	0	0	0	0	543
[15:00-16:00]	829	8	5	0	0	0	0	0	842
[16:00-17:00]	1203	17	10	0	0	0	0	0	1230
[17:00-18:00]	1259	18	3	0	0	0	0	0	1280
[18:00-19:00]	771	9	2	0	0	0	0	0	782
[19:00-20:00]	403	6	1	0	0	0	0	0	410
[20:00-21:00]	302	1	2	0	0	0	0	0	305
[21:00-22:00]	215	0	1	0	0	0	0	0	216
[22:00-23:00]	144	2	0	0	0	0	0	0	146
[23:00-00:00]	78	1	0	0	0	0	0	0	79
09/05/2012 12:00 AM 09/06/2012 12:00 AM	11438	150	72	5	0	0	0	0	11665

APPENDIX B

Capacity Analysis Worksheets

TWO-WAY STOP CONTROL SUMMARY

General Information

Analyst	Bg
Agency/Co.	
Date Performed	7/29/2015
Analysis Time Period	AM Peak Hour

Site Information

Intersection	Ogden @ Shell Access
Jurisdiction	
Analysis Year	Baseline Traffic

Project Description

East/West Street: Ogden Avenue (US 34) | North/South Street: Site Full Access

Intersection Orientation: East-West | Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)	6	1250			1874	6
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	6	1250	0	0	1874	6
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Undivided					
RT Channelized			0			0
Lanes	1	2	0	0	2	0
Configuration	L	T			T	TR
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)				6	0	19
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	0	0	6	0	19
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	0	0	0	1	0
Configuration					LTR	

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L						LTR	
v (veh/h)	6						25	
C (m) (veh/h)	323						81	
v/c	0.02						0.31	
95% queue length	0.06						1.15	
Control Delay (s/veh)	16.4						68.2	
LOS	C						F	
Approach Delay (s/veh)	--	--					68.2	
Approach LOS	--	--					F	

HCS+™ Version 5.6 Generated: 7/29/2015 1:08 PM

TWO-WAY STOP CONTROL SUMMARY

General Information

Analyst	Bg
Agency/Co.	
Date Performed	7/29/2015
Analysis Time Period	AM Peak Hour

Site Information

Intersection	Ogden @ Shell Access
Jurisdiction	
Analysis Year	Total Traffic

Project Description

East/West Street: Ogden Avenue (US 34) | North/South Street: Site Full Access

Intersection Orientation: East-West | Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)	9	1250			1874	10
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	9	1250	0	0	1874	10
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Undivided					
RT Channelized			0			0
Lanes	1	2	0	0	2	0
Configuration	L	T			T	TR
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)				7	0	30
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	0	0	7	0	30
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)	0			0		
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	0	0	0	1	0
Configuration					LTR	

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L						LTR	
v (veh/h)	9						37	
C (m) (veh/h)	322						93	
v/c	0.03						0.40	
95% queue length	0.09						1.61	
Control Delay (s/veh)	16.5						67.2	
LOS	C						F	
Approach Delay (s/veh)	--	--					67.2	
Approach LOS	--	--					F	

HCS+™ Version 5.6 Generated: 7/29/2015 1:48 PM

TWO-WAY STOP CONTROL SUMMARY

General Information

Analyst	Bg
Agency/Co.	
Date Performed	7/29/2015
Analysis Time Period	PM Peak Hour

Site Information

Intersection	Ogden @ Shell Access
Jurisdiction	
Analysis Year	Baseline Traffic

Project Description

East/West Street: Ogden Avenue (US 34) | North/South Street: Site Full Access

Intersection Orientation: East-West | Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)	4	1134			1625	2
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	4	1134	0	0	1625	2
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Undivided					
RT Channelized			0			0
Lanes	1	2	0	0	2	0
Configuration	L	T			T	TR
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)				8	0	48
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	0	0	8	0	48
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	0	0	0	1	0
Configuration					LTR	

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L						LTR	
v (veh/h)	4						56	
C (m) (veh/h)	405						169	
v/c	0.01						0.33	
95% queue length	0.03						1.36	
Control Delay (s/veh)	14.0						36.5	
LOS	B						E	
Approach Delay (s/veh)	--	--				36.5		
Approach LOS	--	--				E		

HCS+™ Version 5.6 Generated: 7/29/2015 1:06 PM

TWO-WAY STOP CONTROL SUMMARY

General Information		Site Information	
Analyst	Bg	Intersection	Ogden @ Shell Access
Agency/Co.		Jurisdiction	
Date Performed	7/29/2015	Analysis Year	Total Traffic
Analysis Time Period	PM Peak Hour		

Project Description

East/West Street: Ogden Avenue (US 34)	North/South Street: Site Full Access
Intersection Orientation: East-West	Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)	7	1134			1625	7
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	7	1134	0	0	1625	7
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Undivided					
RT Channelized			0			0
Lanes	1	2	0	0	2	0
Configuration	L	T			T	TR
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)				10	0	62
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	0	0	10	0	62
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	0	0	0	1	0
Configuration					LTR	

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L						LTR	
v (veh/h)	7						72	
C (m) (veh/h)	403						169	
v/c	0.02						0.43	
95% queue length	0.05						1.92	
Control Delay (s/veh)	14.1						41.2	
LOS	B						E	
Approach Delay (s/veh)	--	--				41.2		
Approach LOS	--	--				E		

HCS+™ Version 5.6

Generated: 7/29/2015 1:49 PM

TWO-WAY STOP CONTROL SUMMARY

General Information		Site Information	
Analyst	Bg	Intersection	Warrenville @ Shell Access
Agency/Co.		Jurisdiction	
Date Performed	7/29/2015	Analysis Year	Baseline Traffic
Analysis Time Period	AM Peak Hour		

Project Description

East/West Street: Warrenville Road	North/South Street: Site Full Access
Intersection Orientation: East-West	Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)		1029	13	20	1359	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	1029	13	20	1359	0
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Two Way Left Turn Lane					
RT Channelized			0			0
Lanes	0	1	0	1	1	0
Configuration			TR	L	T	
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)	31	0	4			
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	31	0	4	0	0	0
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	1	0	0	0	0
Configuration		LTR				

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		L		LTR				
v (veh/h)		20		35				
C (m) (veh/h)		675		148				
v/c		0.03		0.24				
95% queue length		0.09		0.87				
Control Delay (s/veh)		10.5		36.7				
LOS		B		E				
Approach Delay (s/veh)	--	--	36.7					
Approach LOS	--	--	E					

 HCS+™ Version 5.6 Generated: 7/29/2015 1:33 PM

TWO-WAY STOP CONTROL SUMMARY

General Information

Analyst	Bg
Agency/Co.	
Date Performed	7/29/2015
Analysis Time Period	AM Peak Hour

Site Information

Intersection	Warrenville @ Shell Access
Jurisdiction	
Analysis Year	Total Traffic

Project Description

East/West Street: Warrenville Road | North/South Street: Site Full Access

Intersection Orientation: East-West | Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)		1284	21	25	1359	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	1284	21	25	1359	0
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Two Way Left Turn Lane					
RT Channelized			0			0
Lanes	0	1	0	1	1	0
Configuration			TR	L	T	
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)	34	0	9			
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	34	0	9	0	0	0
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	1	0	0	0	0
Configuration		LTR				

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		L		LTR				
v (veh/h)		25		43				
C (m) (veh/h)		537		129				
v/c		0.05		0.33				
95% queue length		0.15		1.33				
Control Delay (s/veh)		12.0		46.3				
LOS		B		E				
Approach Delay (s/veh)	--	--	46.3					
Approach LOS	--	--	E					

HCS+™ Version 5.6

Generated: 7/29/2015 1:41 PM

TWO-WAY STOP CONTROL SUMMARY

General Information		Site Information	
Analyst	Bg	Intersection	Warrenville @ Shell Access
Agency/Co.		Jurisdiction	
Date Performed	7/29/2015	Analysis Year	Baseline Traffic
Analysis Time Period	PM Peak Hour		

Project Description

East/West Street: Warrenville Road | North/South Street: Site Full Access

Intersection Orientation: East-West | Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)		1292	58	21	1412	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	1292	58	21	1412	0
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Two Way Left Turn Lane					
RT Channelized			0			0
Lanes	0	1	0	1	1	0
Configuration			TR	L	T	
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)	33	0	4			
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	33	0	4	0	0	0
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	1	0	0	0	0
Configuration		LTR				

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		L		LTR				
v (veh/h)		21		37				
C (m) (veh/h)		516		118				
v/c		0.04		0.31				
95% queue length		0.13		1.22				
Control Delay (s/veh)		12.3		48.9				
LOS		B		E				
Approach Delay (s/veh)	--	--	48.9					
Approach LOS	--	--	E					

 HCS+™ Version 5.6 Generated: 7/29/2015 1:35 PM

TWO-WAY STOP CONTROL SUMMARY

General Information		Site Information	
Analyst	Bg	Intersection	Warrenville @ Shell Access
Agency/Co.		Jurisdiction	
Date Performed	7/29/2015	Analysis Year	Total Traffic
Analysis Time Period	PM Peak Hour		

Project Description

East/West Street: Warrenville Road	North/South Street: Site Full Access
Intersection Orientation: East-West	Study Period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street	Eastbound			Westbound		
Movement	1	2	3	4	5	6
	L	T	R	L	T	R
Volume (veh/h)		1292	68	28	1412	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	0	1292	68	28	1412	0
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type	Two Way Left Turn Lane					
RT Channelized			0			0
Lanes	0	1	0	1	1	0
Configuration			TR	L	T	
Upstream Signal		0			0	

Minor Street	Northbound			Southbound		
Movement	7	8	9	10	11	12
	L	T	R	L	T	R
Volume (veh/h)	43	0	9			
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00
Hourly Flow Rate, HFR (veh/h)	43	0	9	0	0	0
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)		0			0	
Flared Approach		N			N	
Storage		0			0	
RT Channelized			0			0
Lanes	0	1	0	0	0	0
Configuration		LTR				

Delay, Queue Length, and Level of Service

Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		L		LTR				
v (veh/h)		28		52				
C (m) (veh/h)		512		120				
v/c		0.05		0.43				
95% queue length		0.17		1.88				
Control Delay (s/veh)		12.4		56.2				
LOS		B		F				
Approach Delay (s/veh)	--	--	56.2					
Approach LOS	--	--	F					

HCS+™ Version 5.6 Generated: 7/29/2015 1:43 PM

DRAFT

FILE 15-PLC-0017: A petition seeking approval of a Special Use for a Fueling Station to redevelop the existing gas station. The subject property is zoned B-3, General Services and Highway Business. The property is located on the west side of Finley Road between Ogden Avenue and Warrenville Road, commonly known as 2212 Ogden Avenue, Downers Grove, IL (08-01-402-006, -007, -008). Ambrose Design Group, LLC, Petitioner; TrueNorth Energy, LLC, Owner. (previous continued from the August 3, 2015 meeting)

Community Development Director Stan Popovich briefly introduced new senior planner Rebecca Leitschuh. Ms. Leitschuh summarized that the proposal was for the redevelopment of an existing gas station with a relocation of the current convenience store to the location of the car wash. The car wash would be removed, the existing fuel islands would be reconfigured, and other, overall site improvements would take place. Ms. Leitschuh reported that formerly the zoning for the site was a "by right" permitted use but with recent amendments to the zoning ordinance, was now a special use. The site plan was reviewed. Ms. Leitschuh confirmed that the signage met the zoning code.

Proposed changes to the site included a new sidewalk to the location of the new convenience store; 14 parking spaces (in compliance); a reduction in the canopy; and 3,000 square feet of land to be dedicated to the Illinois Department of Transportation. Remaining conditions included the current detention basin and the underground storage tanks. Ms. Leitschuh proceeded to explain that the width of an access point on the site would be reduced in order for trucks to enter in one direction, deliver the fuel, and then leave in another direction, thereby restricting the access points for the trucks on-site.

A landscaping plan was briefly reviewed. Ms. Leitschuh emphasized how the proposal met the village's Comprehensive Plan for the area and met the village's bulk standards. In addition, she overviewed how the proposal met the village's standards for the special use request.

For the record, Mr. Thoman pointed out that the curb-cut located on Ogden (closest to Finley Rd.) was determined by IDOT, to which Director Popovich confirmed and explained that the issue at that location was that the village did not want traffic exiting left to travel over two lanes in order to get to the left-turn lane. He confirmed that the Warrenville Road access point was reviewed by DuPage County. Ms. Leitschuh then confirmed that the access point being discussed was one-way in for both the fuel trucks and the cars. She also confirmed it was a right-in only access off of Ogden and a right-out only onto Warrenville. The western-most access points, vehicles could exit left onto Warrenville or a left onto Ogden Avenue.

Per the chairman's question regarding signage for the truckers, Dir. Popovich clarified that the curb cut entrance into the site would include a raised median with a hatched area to delineate the in-bound entrance and outbound exit. Mr. Cronin expressed concern as to how trucks would exit the site after making their deliveries during the rush hour and in the high density area. He suggested, for the future, that staff consider looking at regulating fuel deliveries since there were no set times for deliveries.

Petitioner, Mr. Ron Ambrose with Ambrose Design Group, P.O. Box 1870, Crystal Lake, IL, responded that tanker trucks generally schedule deliveries during off-peak hours to avoid the

congestion, as mentioned. Regarding the canopy, it would remain the same length in the north/south direction but would be reduced in the east/west direction due to removing an aisle.

Mr. Cozzo inquired as to who scheduled the fuel delivery, wherein Mr. Ambrose explained how electronic equipment on the trucks tracks and notifies TrueNorth Energy when fuel level is low. Deliveries are scheduled accordingly. Per a question, Mr. Ambrose did not know how old the existing tanks were. He did confirm, however, that the raised medians for ingress and egress would be constructed to allow trucks to drive over them. Per another question, Mr. Ambrose explained how the changes to the site would occur over time, noting the business would probably operate as long as possible and then close when work began on the canopy reduction.

Concern was raised that a right-out only exit would force trucks to head south, or south and then West onto Ogden, and that the same information needed to be communicated to the drivers, to which Mr. Ambrose agreed.

Chairman Rickard invited the public to speak on this matter.

Ms. Gail Tatterson, 1240 Gilbert Ave., Downers Grove, inquired as to what the percentage was for the stormwater detention, reminding the commissioners of a Walgreens (Main & Ogden) proposal, wherein Dir. Popovich explained there no new impervious areas were being added, no new detention was being created and best management practices were not required to be installed. He confirmed that the village's stormwater engineer reviewed the plans and had no concerns.

No further public comments followed.

Mr. Ambrose closed by responding to the above comment, explaining that currently there was 8,700 square feet of green space on-site and it was being increased to 12,800 square feet. Overall, he believed upgrading the facilities was a positive for the village.

Public comment was closed by the chairman.

Commissioner comments followed that the upgrades were nice to see since the site was a gateway into the village; the approval criteria/standards for the proposal were met; the proposal met the comprehensive plan; and the similar use was not negatively impacting the area.

WITH RESPECT TO FILE 15-PLC-0017, MRS. RABATAH MADE A MOTION THAT THE PLAN COMMISSION FORWARD A POSTIVE RECOMMENDATION TO THE VILLAGE COUNCIL, SUBJECT TO STAFF'S FOLLOWING THREE (3) CONDITIONS:

1. **THE PROPOSED SPECIAL USE REQUEST TO REDVELOP THE EXISTING FUELING STATION SHALL SUBSTANTIALLY CONFORM TO: THE SITE PLAN, TRUCK ACCESS AND FLOOR PLANS PREPARED BY AMBROSE DESIGN GROUP, LLC, DATED OCTOBER 15, 2014, AND LAST REVISED ON SEPTEMBER 15, 2015; THE ARCHITECTURAL ELEVATIONS PREPARED BY F.A. ROSS ARCHITECTURE, DATED JUNE 4, 2015, ATTACHED TO THIS REPORT, EXCEPT AS SUCH PLANS MAY BE MODIFIED TO CONFORM TO VILLAGE CODES, ORDINANCES, AND POLICIES;**

2. **ALL PROPOSED SIGNS SHALL CONFORM TO THE VILLAGE'S SIGN ORDINANCE; AND**
3. **AN ADMINISTRATIVE LOT CONSOLIDATION SHALL BE PREPARED THAT DEDICATES ADDITIONAL IDOT RIGHT-OF-WAY AND GRANTS AN EASEMENT OVER THE EXISTING STORMWATER MANAGEMENT BASIN.**

SECONDED BY MS. HOGSTROM. ROLL CALL:

AYE: MRS. RABATAH, MS. HOGSTROM, MR. COZZO, MR. CRONIN, MR. THOMAN, CHAIRMAN RICKARD
NAY: NONE

MOTION CARRIED. VOTE: 6-0

Director Popovich briefly reviewed the next meeting's agenda. He also introduced and welcomed new Senior Planner Rebecca Leitschuh, who started September 21, 2005. Her professional credentials followed. New commissioner, Mark Cronin, was also welcomed to the commission. Dir. Popovich announced that October is National Community Planning Month and he invited commissioners to attend the October 13th council meeting where a proclamation would be announced.

THE MEETING WAS ADJOUFRNED AT 7:40 P.M. ON MOTION BY MR. COZZO, SECONDED BY MR. THOMAN. MOTION CARRIED UNANIMOUSLY BY VOICE VOTE OF 6-0.

/s/ Celeste K. Weilandt
Celeste K. Weilandt
(As transcribed by MP-3 audio)