

VILLAGE OF DOWNERS GROVE
Report for the Village
10/17/2017

SUBJECT:	SUBMITTED BY:
Downtown Development Regulation Framework	Stan Popovich, AICP Director of Community Development

SYNOPSIS

Discussion of the proposed downtown development regulatory framework developed by the Comprehensive Planning Ad Hoc Committee and the Plan Commission in accordance with Village Council direction.

STRATEGIC PLAN ALIGNMENT

The Village Council has identified *Update to the Comprehensive Plan* and a *Review of the Downtown Zoning* as top priority action items for 2015-2017.

FISCAL IMPACT

N/A

UPDATE & RECOMMENDATION

This item was discussed at the September 12, 2017 Village Council meeting. The item is scheduled for continued discussion at the October 17, 2017 Village Council meeting. In response to the Council discussion on September 12th, the Village reached out to downtown business and property owners to obtain additional participation in the following ways:

- An interactive webpage (<http://www.downers.us/proposed-downtown-zoning>) that provided an opportunity for business and property owners to learn more about the proposed downtown regulatory framework.
- Two informational meetings for business and property owners that were led by Village staff to review the proposed Downtown Regulatory Framework. The meetings were held on Monday, October 2 between 5:00 pm and 7:00 pm and Wednesday, October 4 between 1:00 pm and 3:00 pm. A total of 17 people attended the meetings.

During the informational meetings, the following topics were discussed:

1. The proposed change from the existing DB (Downtown Business) zoning district to the proposed DT (Downtown Transition) zoning district may negatively affect property owners in the following ways:

- Height reduction of 24 feet
- Density reduction of 29 units per acre
- Additional setbacks would result in reduced buildable areas for both buildings and parking lots
- A number of uses will no longer be permitted

Specific areas discussed (and shown on the attached map) during the meetings included:

- East side of Main Street between Rogers Street and Franklin Street
- Southwest corner of Rogers Street and Washington Street
- Northeast corner of Warren Avenue and Washington Street
- Northeast corner of Maple Avenue and Washington Street
- Southwest corner of Main Street and Maple Avenue

2. The proposed reduction in height from the DB (Downtown Business) to the DC (Downtown Core) may have a negative impact on future resale value of properties.

3. The proposed change from the DB (Downtown Business) zoning district to the proposed DE-1 (Downtown Edge-1) zoning district results in additional rear and side yard setbacks which would result in reduced buildable area for both buildings and parking lots.

4. The elimination of drive-through uses throughout the downtown focus area may have a negative impact on existing businesses.

5. The Village's current six month allowance to re-establish a non-conforming use may be too stringent. Particular concern was expressed about the existing automobile repair shops.

Comment cards completed during the two meetings as well as other correspondence that staff has received since the September 12, 2017 Village Council discussion are attached.

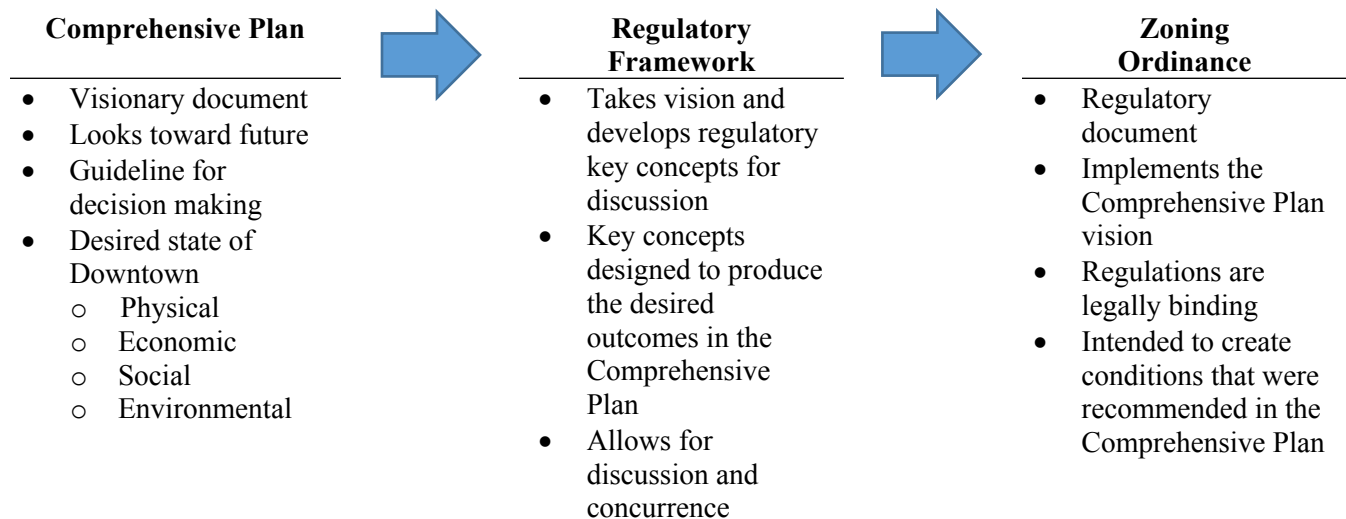
BACKGROUND

In March 2016, the Village Council created the Comprehensive Planning Ad Hoc Committee (CPC). The CPC was charged with updating the Village's 2011 Comprehensive Plan (Plan) and developing an updated

zoning regulation framework for the downtown. The Plan was adopted by the Village Council in June 2017. The regulatory framework has been developed by the CPC, reviewed by the Plan Commission and is ready for Village Council review and discussion.

Comprehensive Plan, Regulatory Framework and Zoning Ordinance

These three related documents play key roles in the development and redevelopment in the Village and in this case, the downtown. The documents play the following roles:



Comprehensive Planning Ad Hoc Committee (CPC)

The CPC met in January and February 2017 to review the existing downtown development regulations and develop a regulatory framework that considers modifications to the downtown development regulations based on the vision of the downtown identified in the Comprehensive Plan. The regulatory framework the CPC developed discusses bulk regulations, land uses and design guidelines. In February, the CPC forwarded their recommendations to the Plan Commission for their review.

Plan Commission

The Plan Commission met on June 26, August 7 and August 28 to review the CPC recommended downtown development regulatory framework. The Plan Commission concurred with many of the recommendations and offered differing recommendations in some instances. The Plan Commission also discussed increased public participation. At their August 28 meeting, the Plan Commission voted to forward their recommendations to the Village Council for review.

Downtown Regulatory Framework

The Comprehensive Plan identified three functional subareas in the Downtown: Downtown Core, Downtown Edge and Downtown Transition. The proposed regulatory framework consists of four zoning districts intended to implement the vision of the three functional subareas. The Downtown Edge functional subarea is split into two districts (Downtown Edge-1 and Downtown Edge-2) to ensure those edge properties closer to the core have core characteristics while the edge properties that are closer to the transition subarea and residential zoning areas have similar characteristics.

The Comprehensive Plan discusses the unique desired attributes of each subarea on pages 112-113. The following tables identify the significant changes to the existing downtown development regulations that are under consideration.

Downtown Core (DC) - Key Concepts		
Comprehensive Plan Recommendation	CPC Recommendation	PC Recommendation
<ul style="list-style-type: none"> Residential uses restricted to upper floors 	<ul style="list-style-type: none"> Permitted by-right above the 1st floor 	<ul style="list-style-type: none"> Permitted as a Special Use above the first floor in a mixed-use development where the first floor contains commercial service uses
<ul style="list-style-type: none"> Continuous street wall w/ zero foot setbacks 	<ul style="list-style-type: none"> Maintain setbacks <ul style="list-style-type: none"> 0' street setback 0' side yard setback 0' rear setback Build-to-zone of 0-5' 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Building height no more than 3-stories 	<ul style="list-style-type: none"> Maximum height 40' / 3-story Minimum height 24' / 2-story 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Parking 	<ul style="list-style-type: none"> No change 	<ul style="list-style-type: none"> Concur

Downtown Edge (DE) - Key Concepts		
Comprehensive Plan Recommendation	CPC Recommendation	PC Recommendation
<ul style="list-style-type: none"> DE as an area of greater residential density 		
<ul style="list-style-type: none"> Downtown Core density 	<ul style="list-style-type: none"> Maintain 54 units per acre (800 sq ft per dwelling unit) 	<ul style="list-style-type: none"> 48 units per acre (900 sq ft per dwelling unit)
<ul style="list-style-type: none"> Downtown Edge-1 (DE-1) density 	<ul style="list-style-type: none"> 14 units per acre (3,9000 sq ft per dwelling unit) 	<ul style="list-style-type: none"> 62 units per acre (700 sq ft per dwelling unit)
<ul style="list-style-type: none"> Downtown Edge-2 (DE-2) density 	<ul style="list-style-type: none"> 10 units per acre (4,000 sq ft per dwelling unit) 	<ul style="list-style-type: none"> 54 units per acre (800 sq ft per dwelling unit)
<ul style="list-style-type: none"> Downtown Edge-1 larger buildings, built near the front property line 	<ul style="list-style-type: none"> Maximum height - 72' / 6-story Minimum height 24' / 2-story 0' street setback 5' / 10% side yard setback 10' rear yard setback adjacent to R district Build-to-zone of 0-10' 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Downtown Edge-2 larger buildings with front and side setbacks to create more green space 	<ul style="list-style-type: none"> Maximum height - 60' / 5-story No minimum height 10' street setback 5' / 10% side yard setback 10' rear yard setback adjacent to R district No build-to-zone 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Residential development is predominant desired land use 	<ul style="list-style-type: none"> Apartment / Condo permitted by-right Attached house permitted by-right Two-unit house permitted by-right 	<ul style="list-style-type: none"> Apartment / Condo permitted as a Special Use Attached house permitted by-right Two-unit house permitted by-right

<ul style="list-style-type: none"> Consider appropriate commercial uses in Downtown Edge areas 	<ul style="list-style-type: none"> Update use list as shown 	<ul style="list-style-type: none"> Include Bed and Breakfasts as a Special Use in the DE-1 and DE-2 districts. Concur with other uses as shown.
<ul style="list-style-type: none"> Parking 	<ul style="list-style-type: none"> No change 	<ul style="list-style-type: none"> Concur

Downtown Transition (DT)- Key Concepts

Comprehensive Plan Recommendation	CPC Recommendation	PC Recommendation
<ul style="list-style-type: none"> All residential is appropriate in DT 	<ul style="list-style-type: none"> Apartment / Condo permitted by-right 	<ul style="list-style-type: none"> Apartment / Condo permitted as a Special Use
<ul style="list-style-type: none"> Buildings smaller than Downtown Core and Downtown Edge 	<ul style="list-style-type: none"> Maximum height 36' / 3-story 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Setbacks to create front and side yards 	<ul style="list-style-type: none"> 20' street setback 5' / 10% side yard setback 20' rear yard setback 	<ul style="list-style-type: none"> Concur
<ul style="list-style-type: none"> Consider service uses on a case-by-case basis 	<ul style="list-style-type: none"> Remove small art galleries and studios 	<ul style="list-style-type: none"> Maintain allowance for small art galleries and studios
<ul style="list-style-type: none"> Parking 	<ul style="list-style-type: none"> No change 	<ul style="list-style-type: none"> Concur

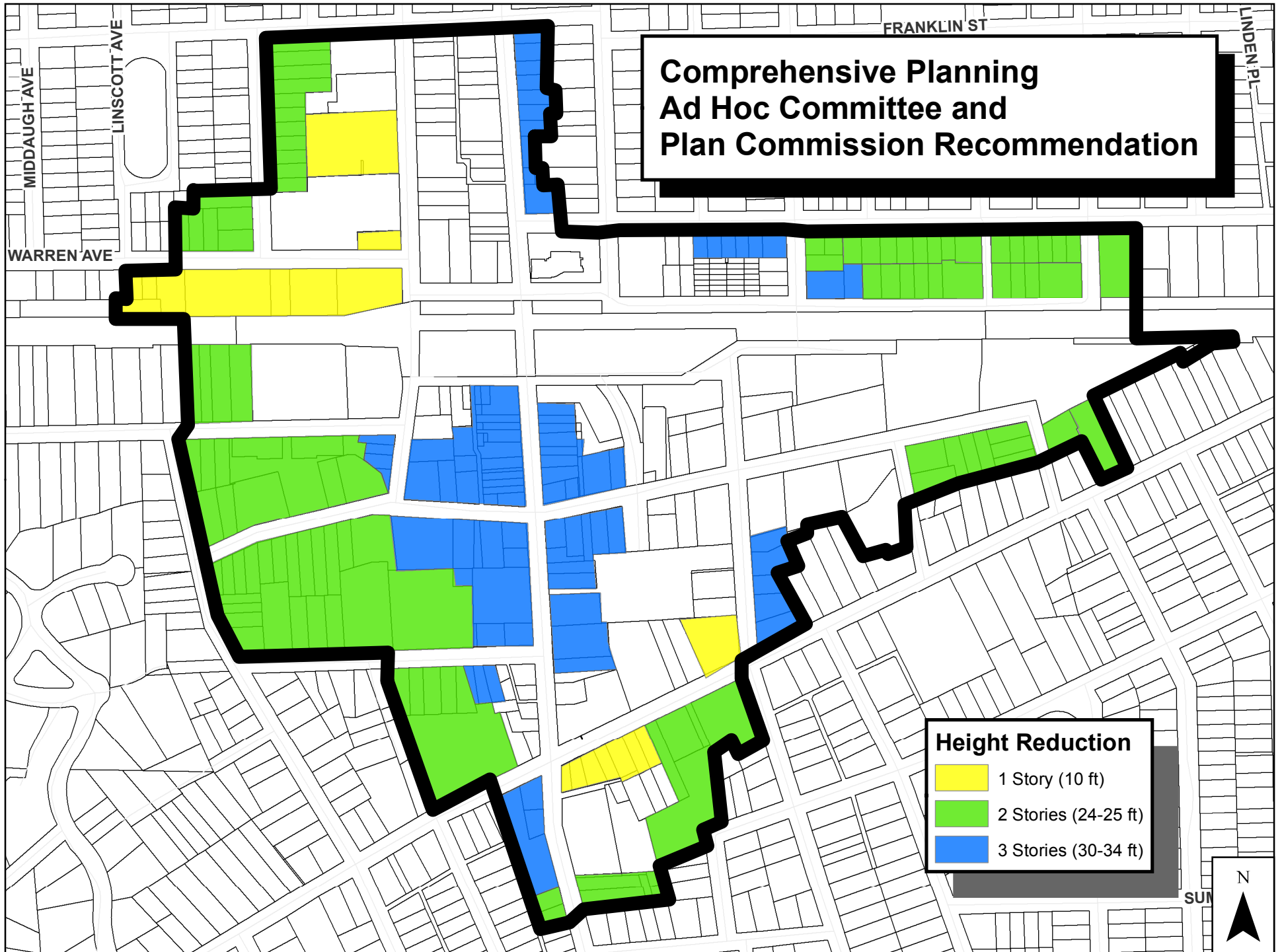
General Key Concepts

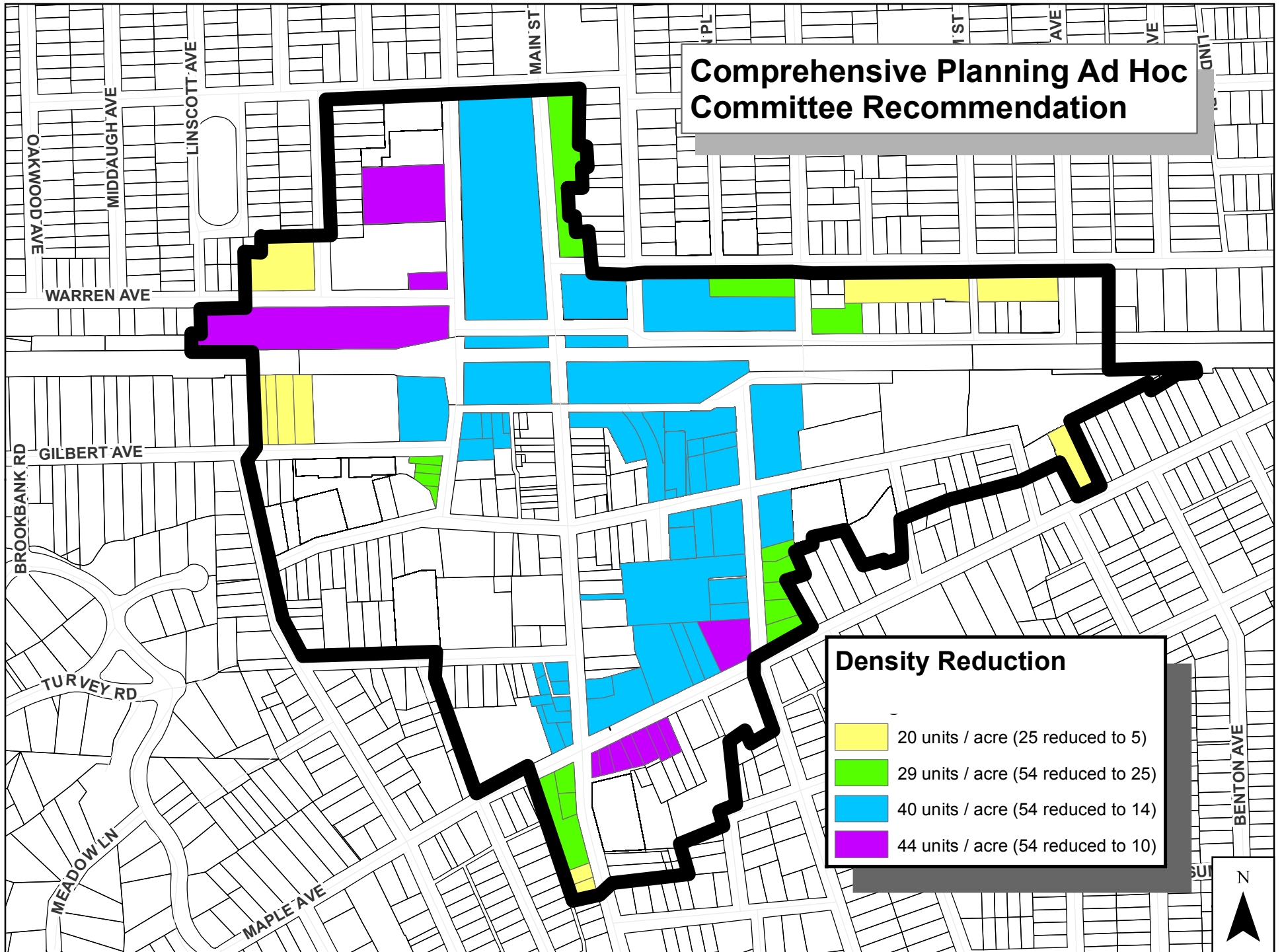
Comprehensive Plan Recommendation	CPC Recommendation	PC Recommendation
<ul style="list-style-type: none"> Promote Historic Preservation 	<ul style="list-style-type: none"> No comment 	<ul style="list-style-type: none"> Concur Historic Preservation is well documented in Comprehensive Plan Provide links on Village website to encourage historic preservation in the downtown
<ul style="list-style-type: none"> Maintain commitment to quality architecture through the development of tools and design guides for downtown Consider the establishment of a Downtown Design Review Board 	<ul style="list-style-type: none"> Continue current Design Guideline process 	<ul style="list-style-type: none"> Keep existing guidelines but explore updating guidelines Promote natural materials and limit use of undesirable materials Include parking lots, lighting and landscaping Tailor guidelines to each district (DC, DE-1, DE-2 and DT) Clarify what projects qualify to be reviewed for design guideline compliance Provide more links to

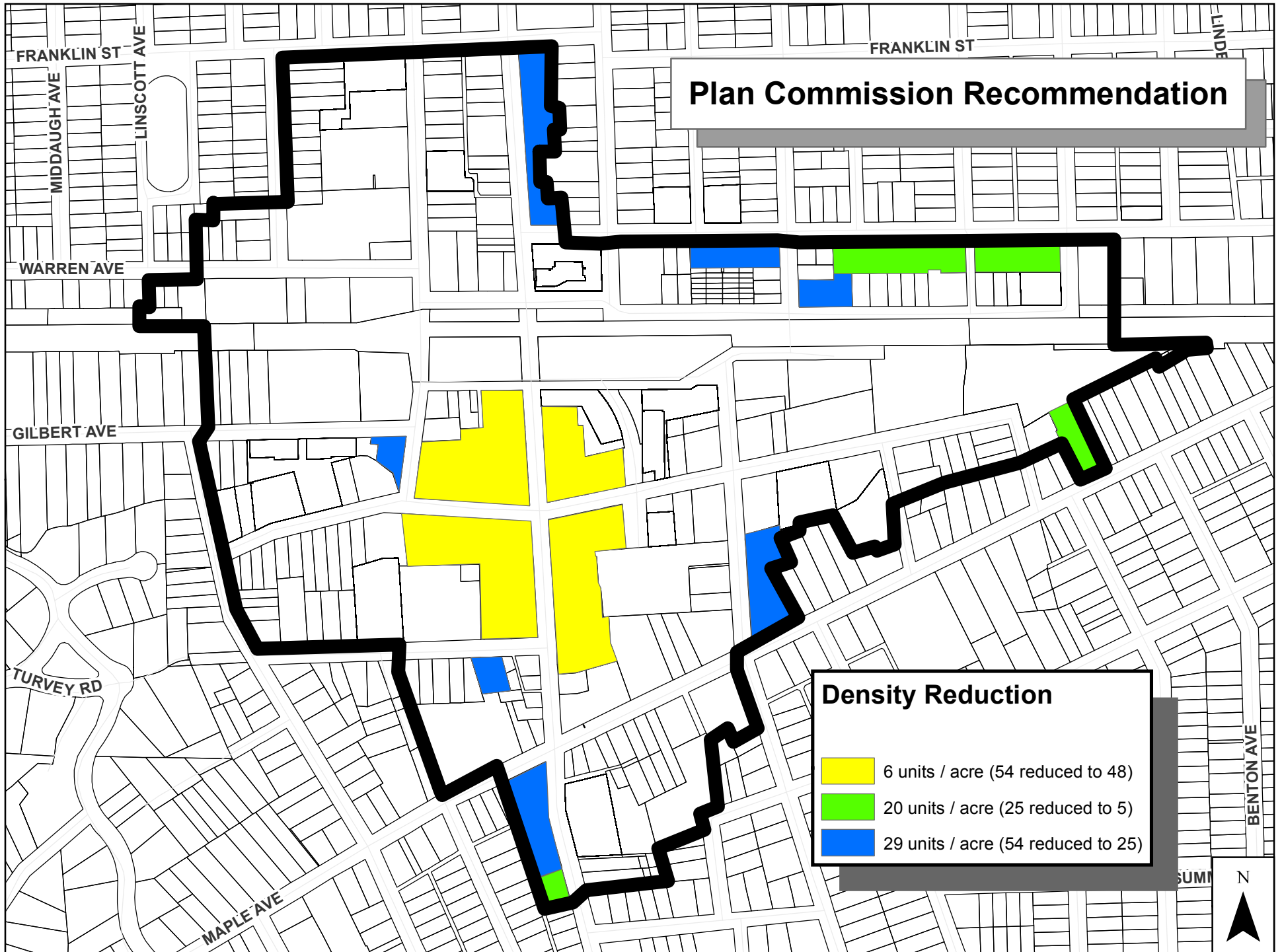
		guidelines on website
<ul style="list-style-type: none"> Review Public Participation Processes 	<ul style="list-style-type: none"> Was not discussed 	<ul style="list-style-type: none"> Allow for more public input early in the project planning process

ATTACHMENTS

Map of proposed height reductions
 Map of PC recommended density reductions
 Map of CPC recommended density reductions
 CPC recommended regulatory framework
 Plan Commission recommended regulatory framework revisions
 Plan Commission memos – June 26, 2017; August 7, 2017; and August 28, 2017
 Plan Commission meeting minutes
 CPC meeting minutes
 Downtown Zoning comparison sheet
 Comment cards/correspondence
 Map of Property/Business Owners areas of concerns





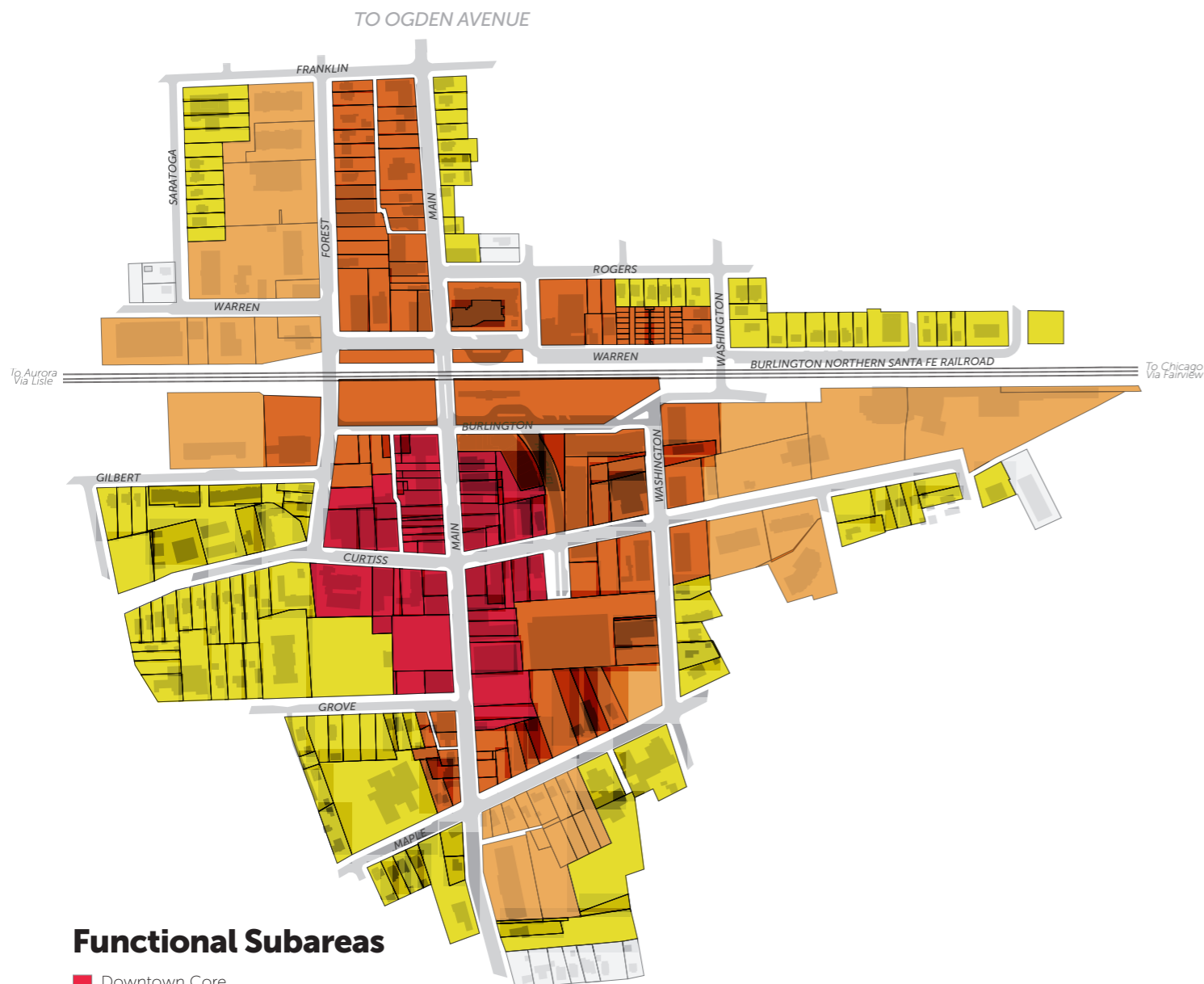


Comprehensive Planning Ad Hoc Committee - Regulatory Framework - February 2017

Key Focus Areas

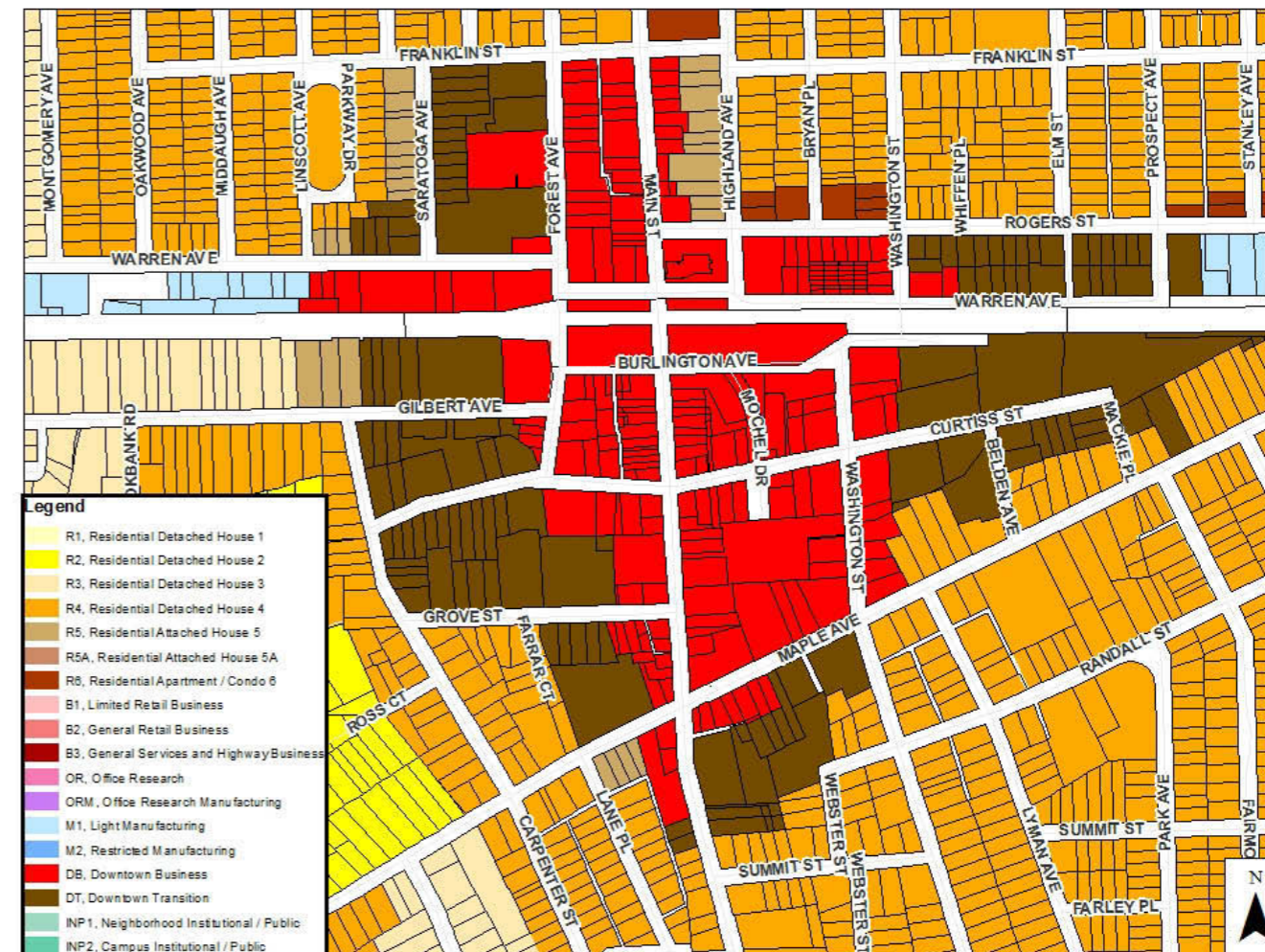
Downtown Functional Subareas

Downtown is often referred to, and treated as, a single place. While it is true that Downtown is a unique destination in the community, it is actually comprised of several distinct areas, with different form, uses, conditions, characteristics and potentials. Therefore, to better accommodate and encourage new appropriate development, new zoning districts and standards are being proposed to better align with the Downtown Plan. The new zoning addresses the specific needs of each "Functional Subarea" and establishes recommendations for the improvement and enhancement of each area in the future, including appropriate uses and intensities.



Functional Subareas

- Downtown Core
- Downtown Edge 1
- Downtown Edge 2
- Downtown Transition



Downtown Current Zoning

Downtown Core District (DC)

Downtown Core District (DC)

Desired/Planned Character:

Building Height: 2-3 stories (existing buildings range from 1-3 stories in height)

Setbacks: Buildings located along sidewalk with no setbacks

Uses: Mixed-use, retail/commercial on ground floor; residential and office on upper floors

Building Height:

- Current zoning allows maximum height of 70 feet for all parcels.
- Proposed maximum is 40 feet/3-stories (12'-15' first floor, 10'-12' upper floors)

Setbacks:

- Current Setback is 0' on side and rear; 0-10' on street
- Keep same setback standards.

Uses:

- Ground Floor: Commercial/retail/entertainment/restaurant on ground floor
- Upper Floor: multi-family residential or office on the upper floors
- Residential uses not permitted on ground floor

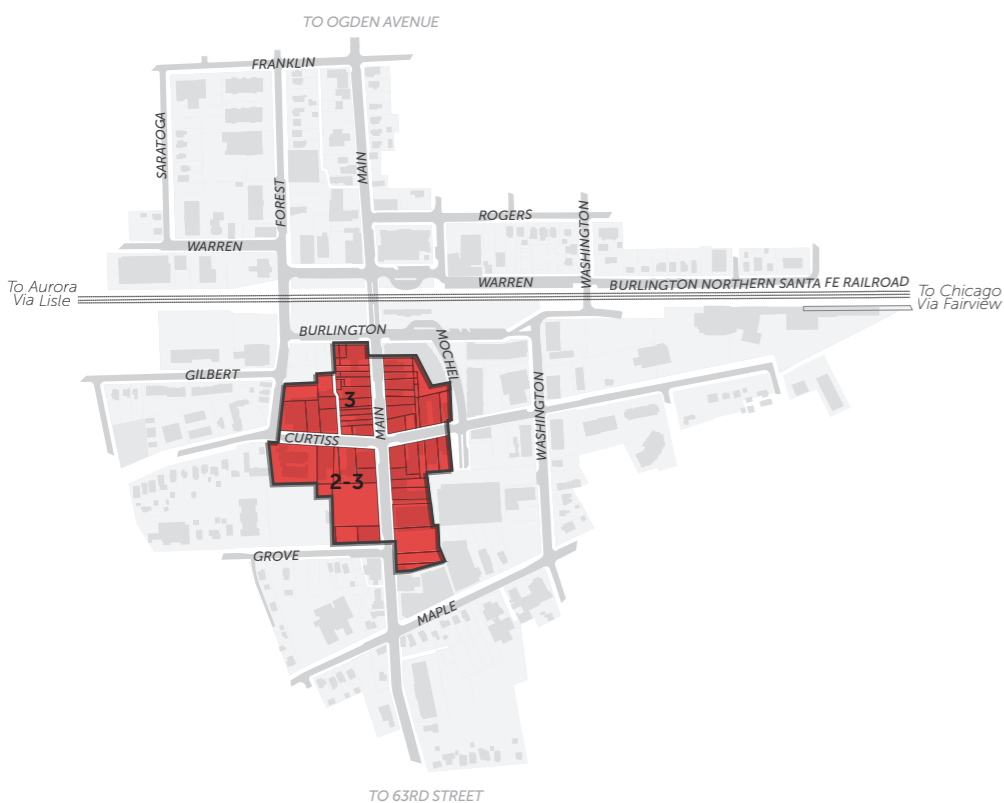
Existing Front Setback Requirement

■ 0ft Front Setback

Existing Building Height Maximum

■ 70ft Max Building Height

✕ Approximate Stories of Existing Buildings



Downtown Edge District (DE-1)

Downtown Edge District (DE-1)

Desired/Planned Character:

Building Height: 1-6 stories

Setbacks: variable – proportional to height and proximity to neighborhood and downtown core

Uses: wide ranging – mixed-use, residential, commercial, office, institutional, entertainment, civic, etc.

Building Height:

- Current zoning allows maximum height of 60-70 feet depending on parcel location
- Proposed maximum 72 feet/6-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, but a few sites require a 10' street setback
- Existing street setbacks vary within the district, from 0'-20+'
- Proposed 0' street setback throughout district

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater
- Proposed side yard setback for properties adjacent to the DC District and those properties along Main Street shall be 0'

Rear Setback:

- Proposed rear setback is 10' throughout the district
- Special rear setbacks standards apply to property abutting an R zoned lot (see table).

Uses:

- Commercial, retail, office, entertainment, service, restaurant, civic, and residential (apartment, condominium, attached single-family)

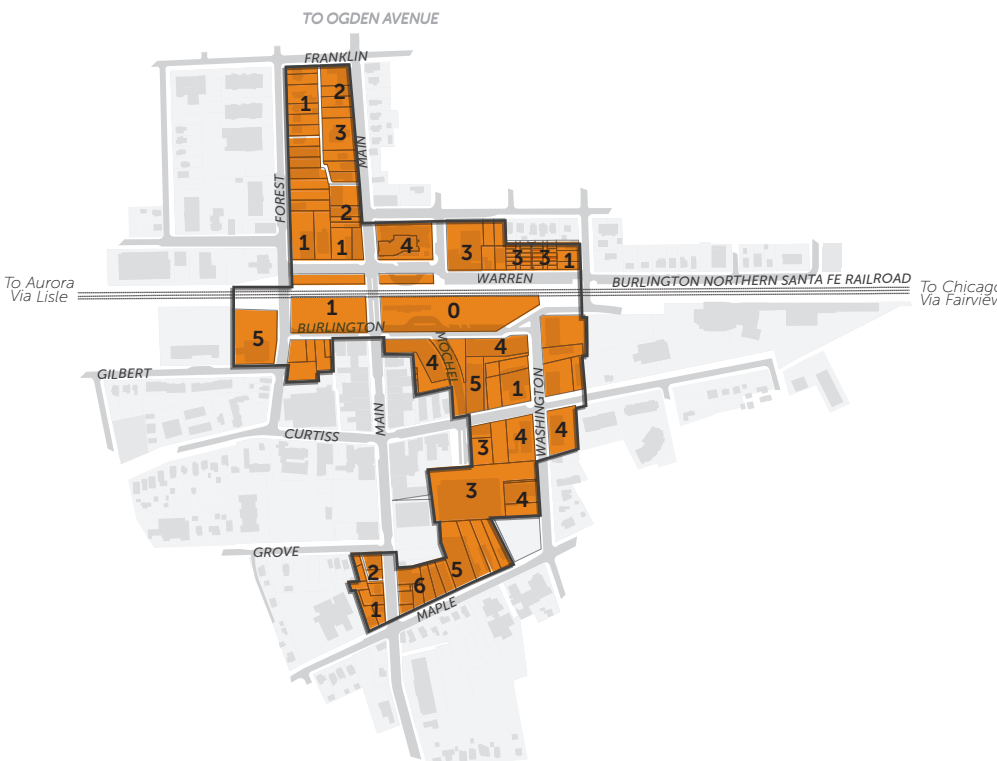
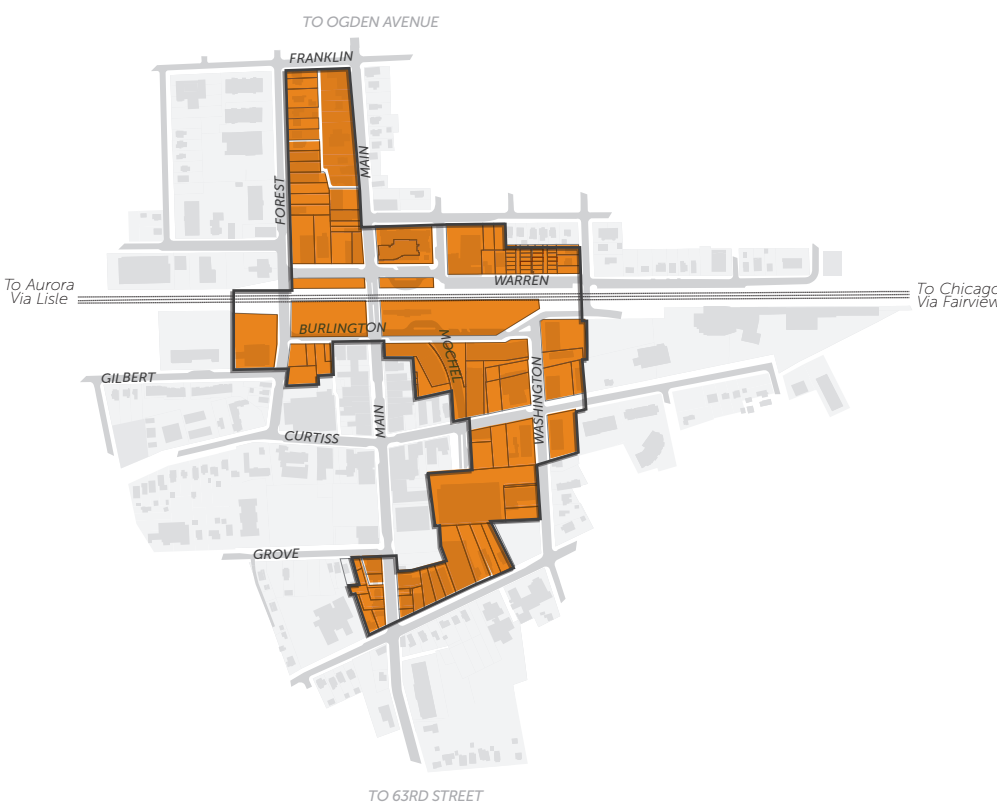
Existing Front Setback Requirement

■ 0ft Front Setback

Existing Building Height Maximum

■ 70ft Max Building Height

X Approximate Stories of Existing Buildings



Downtown Edge District (DE-2)

Downtown Edge District (DE-2)

Desired/Planned Character:

Building Height: 1-5 stories

Setbacks: variable – proportional to height and proximity to neighborhood and downtown core

Uses: wide ranging – mixed-use, residential, commercial, office, institutional, entertainment, civic, etc.

Building Height:

- Current zoning allows maximum height of 60-70 feet depending on parcel location
- Proposed maximum is 60 feet/5-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, but a few sites require a 10' street setback
- Existing street setbacks vary within the district, from 0'-20+'
- Proposed 10' street setback throughout district

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater

Rear Setback:

- Proposed rear setback is 10' throughout the district
- Special rear setback standards apply to property abutting an R zoned lot (see table)

Uses:

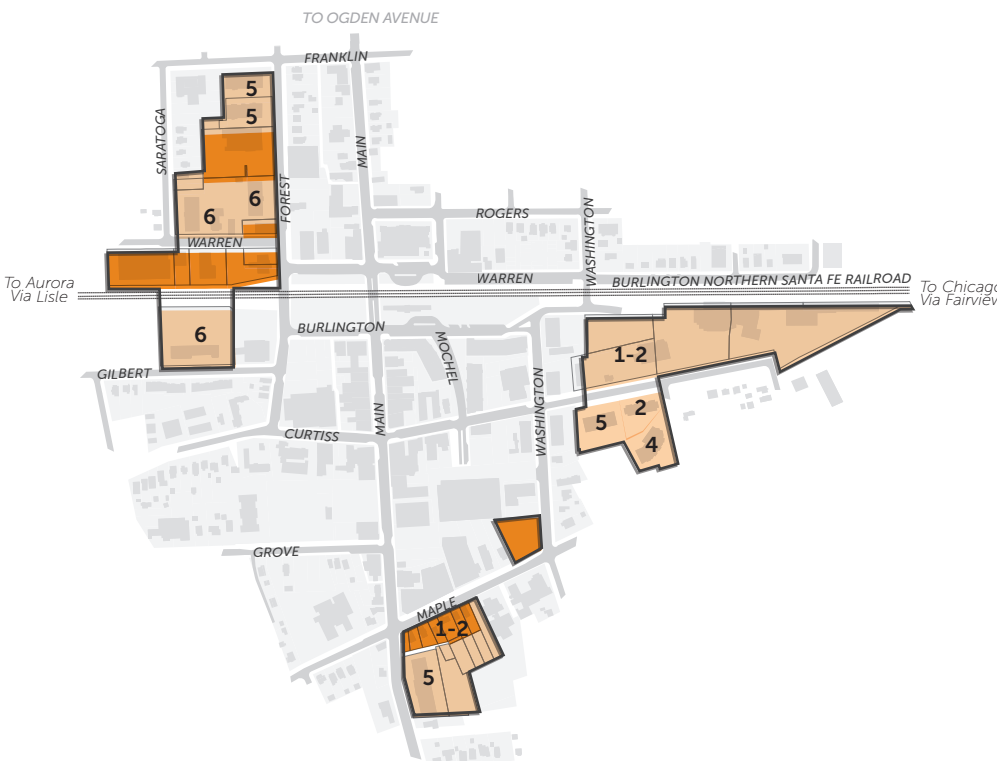
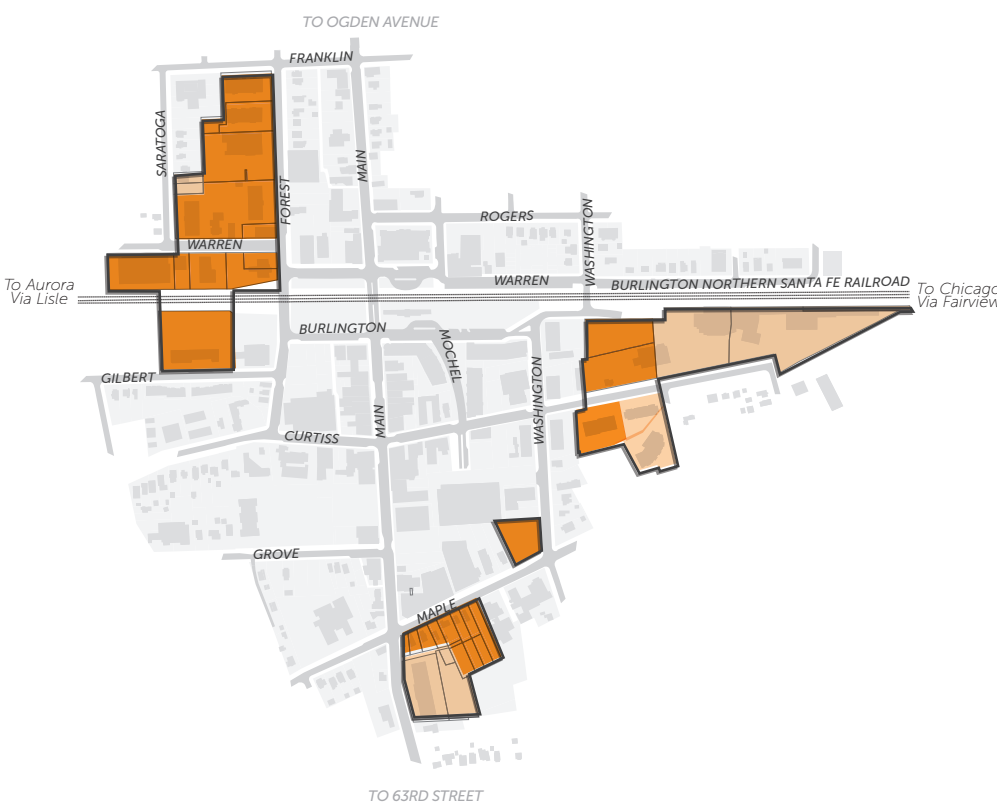
- Commercial, retail, office, entertainment, service, restaurant, civic, and residential (apartment, condominium, attached single-family)

Existing Front Setback Requirement

- 0ft Front Setback
- 10ft Front Setback

Existing Building Height Maximum

- 70ft Max Building Height
- 60ft Max Building Height
- ✕ Approximate Stories of Existing Buildings



Downtown Transition District (DT)

Downtown Transition District (DT)

Desired/Planned Character:

Building Height: 1-3 stories (existing buildings range from 1-6 stories in height)

Setbacks: Buildings with landscaped setbacks reflective of residential districts.

Uses: residential, office, service, home occupations, institutional (residential scale and compatibility)

Building Height:

Current zoning allows maximum height of 70' or 60' for most parcels, and 35' for a few parcels.

Proposed maximum is 36 feet/3-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, or 10' street setback required. A few have 25' setback required.
- Proposed 20' street setback throughout district, except for properties fronting Main Street, which will have a 10' required street setback.

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback.
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater.
- Special side setbacks standards apply to property abutting an R zoned lot (see table).

Rear Setback:

- Proposed rear setback is 20' throughout the district, except for properties fronting Main Street, which will have a 10' required rear setback.
- Special rear setback standards apply to property abutting an R zoned lot (see table).

Uses:

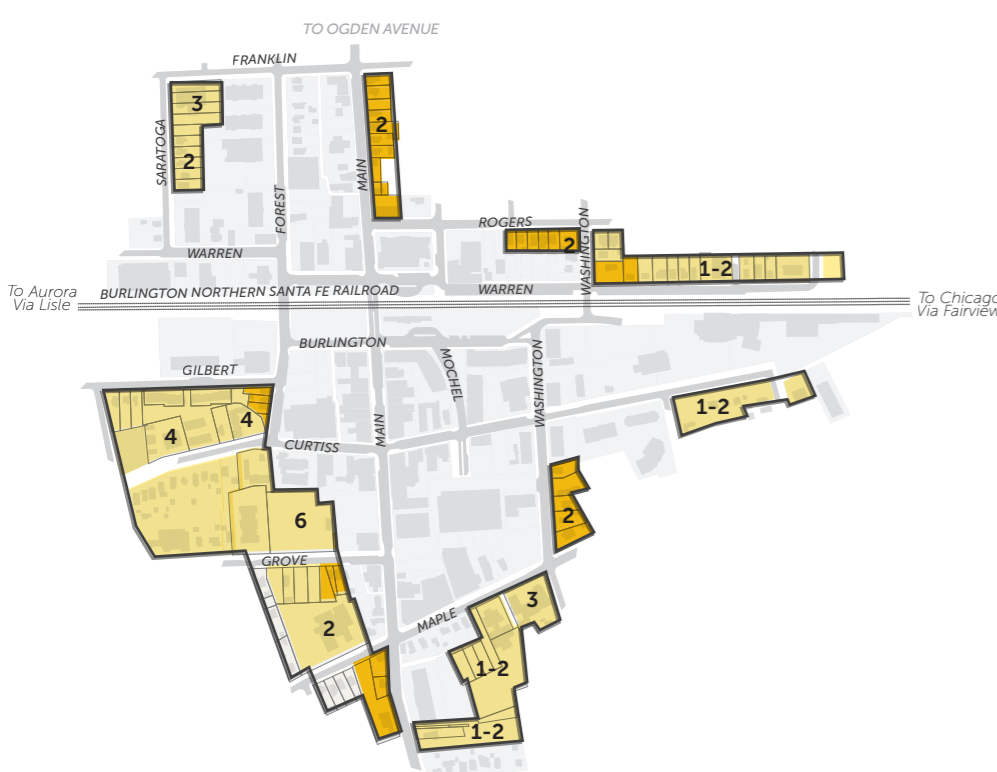
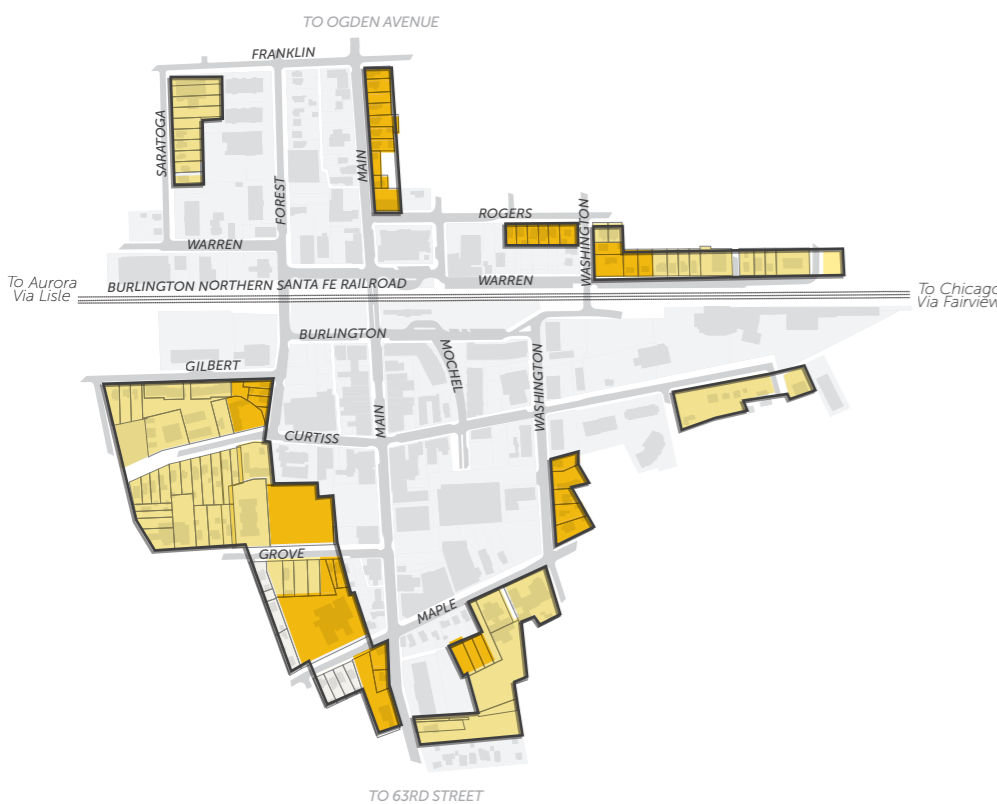
- Single-family, multi-family, office, service, home occupations, institutional (residential scale and compatibility), lodging (bed and breakfast only)

Existing Front Setback Requirement

- 0ft Front Setback
- 10ft Front Setback
- 25ft Front Setback

Existing Building Height Maximum

- 70ft Max Building Height
- 60ft Max Building Height
- 35ft Max Building Height
- ✕ Approximate Stories of Existing Buildings



Downtown Zoning Comparison Table

Regulation	DB	DT	DC	DE-1	DE-2	DT
Building Height						
Maximum	70'	60'[4]	40'/3-story	72'/6-story	60'/5-story	36'/3-story
Minimum	32'	-	24'/2-story	24'/2-story	-	-
Parking (per residential unit)						
	1.4	2	1.4	1.4	1.4	2
Minimum Lot Area per D.U. (square feet)						
	800	4,000[1]	800	3,000[1]	4,000[1]	4,000[1]
Maximum Floor Area Ratio						
Apartment/Condo	-	2.5	-	2.5	2.5	2.5
Non-residential	-	2.5	-	2.5	2.5	2.5
Minimum Lot Area (square feet)						
Detached House	-	7,500	-	-	7,500	7,500
Attached House (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Two -Unit House (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Apartment/Condo (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Other Non-residential Building Uses	-	7,500	-	7,500	7,500	7,500
Minimum Lot Width (feet)						
	-	50'	-	50'	50'	50'
Minimum Building Setback (feet)						
Street	0'	10'[2]	0'	0'	10'	20'[x]
Side (interior)	0'	5'	0'	5'/10%	5'/10%	5'/10%
Rear	0'[3]	20'	0'	10'[3]	10'[3]	20'[3]
Building to Zone						
Minimum/maximum (feet)	0/10'	-	0/5'	0/40'	-	-
Minimum percent of building in primary street Building to Zone	80	-	80	80	-	-
Minimum percent of building in secondary street Build to Zone	30	-	30	30	-	-
Maximum Building Coverage						
(% of Lot, Principal + Accessory)	-	-	-	-	-	-
Uses						
See Downtown Uses table on page 8						

[1] Does not apply to detached houses or apartment/condo projects.

[2] No street setback required if street lot line abuts DC zoning districts or if front lot line is located on Main Street

[3] A rear setback is required when abutting the side or rear lot line of an R-zoned lot. When abutting the rear lot line of an R-zoned lot, the setback must be at least 20 feet in depth, plus one foot of additional setback for each foot of building height in excess of 20 feet. When abutting the side lot line of an R-zoned lot, the setback must be at least as deep as the side setback required on the abutting R-zoned lot, plus one foot of additional setback for each foot of building height in excess of 20 feet. See Figure 4-3.

[4] Detached houses, attached houses and two-unit houses subject to 35-foot maximum height.

[x] 10' minimum setback required for properties fronting Main Street

[xx] Only permitted as part of a mixed-use development; Residential uses not permitted on ground floor

Existing Districts: DB=Downtown Business; DT=Downtown Transition

Proposed Districts: DC=Downtown Core; DE-1=Downtown Edge 1; DE-2=Downtown Edge 2; DT=Downtown Transition

Downtown Design Guidelines

Intent:

The Design Guidelines for development within the four new Downtown zoning districts (DC, DE-1, DE-2, DT) are intended to ensure quality, compatible, attractive, and market viable development that fits the overall Downtown, as well as the character of each of the different zoning districts that comprise the greater Downtown area. The guidelines are not standards or formulas but rather principles and approaches which could be applied with understanding and sensitivity in context of the use of the site, nearby buildings and the streetscape of adjacent public rights-of-way.

The Design Guidelines are intended to promote an attractive and pedestrian-friendly Downtown. The guidelines will work cooperatively with zoning to establish a flexible and creative environment that seeks to accommodate a wide range of uses and development types.

The Design Guidelines are not intended to dictate architecture or building design, but rather guide development to ensure the desired Downtown character and sense of place while facilitating appropriate new development and exterior renovations.

The Design Guidelines shall apply to all new construction and exterior renovations of all non single-family residential development within the DC, DE-1, DE-2, and DT Districts.

When and How to Use the Design Guidelines:

The Design Guidelines should be used to guide and evaluate new construction and exterior renovations of all non-single family residential developments within the four Downtown zoning districts. The following outlines the process for use of the Design Guidelines on a proposed development.

1. The developer is provided with the Design Guidelines. The intent is for the developer to follow the Design Guidelines as the development plans and designs are created. Village staff can answer any questions the developer may have regarding the Design Guidelines.
2. A new construction development or exterior renovation is proposed. Staff will determine if the proposed development meets the zoning standards for the district in which it is located, and whether or not the proposed development complies with the Design Guidelines.
3. Staff will work with the applicant or developer to encourage compliance with the Design Guidelines, understanding that the guidelines are not standards or formulas but rather principles and approaches to be applied to design.
4. If Village staff determines that the proposed improvements comply with all zoning standards, then the development or exterior renovation can be approved 'by-right'. In this case, Village staff will work with the developer to encourage compliance with the Design Guidelines. If all zoning standards are met, the development or renovation can continue without full compliance with Design Guidelines.
5. If Village staff determines that a proposed development does NOT comply with zoning, then the proposed development will require approval as a Planned Unit Development or a variation(s) can be sought for the necessary zoning relief. A Planned Unit Development would require a public hearing with the Plan Commission and a variation would require a public hearing with the Zoning Board of Appeals. As part of either a Planned Unit Development or a variation request, the Design Guidelines could be used as a standard of review to help determine the overall appropriateness of the proposed development.

Components/Features:

The Design Guidelines will address a wide range of development and architectural components/features. Different guidelines will likely need to be developed for the different districts, as the type and intensity of development may vary wildly from one district to the next, as well as within each district.

Identified below are many of the likely components/features to be addressed in the Design Guidelines:

Building Base

- Windows - % opening
- Windows - transparency
- Knee Walls (12' - 30')
- Entry Features (articulation, elaboration, materials)
- Horizontal expression to establish ground level
- Materials - complement existing
- Materials - restore/repair existing
- Materials - discourage covering existing features and materials
- Materials - brick, manufactured stone, terra cotta, metal accents, metal panels, wood, hardiboard
- Materials - discourage EIFS, utility brick, CMU, vinyl or aluminum siding
- Materials - differentiate importance of building features

Base - Façade Elements

- Awnings
- Outdoor cafes
- Protruding light fixtures
- Landscaping
- Benches / Seating
- Balconies
- Projecting sign

Building Middle

- Windows in rhythm w/ base - % of window openings?
- Visual interest - sills, lintels, divided lights and style
- Replacement windows - fill entire historic window opening
- Fill in historic window - use different material

Middle - Façade

- Proportionate shapes and patterns
- Visually appealing w/ detailing, openings & material
- Corner buildings - articulated comers and elaborated to reflect importance of corner

Middle - Materials

- Complimentary
- Materials - brick, manufactured stone, terra cotta, metal accents, metal panels, wood, hardiboard
- Materials - discourage EIFS, utility brick, CMU, vinyl or aluminum siding
- Materials - differentiate importance of building features

Building Top

- Distinctive corner and cornices
- Screen mechanical equipment

Downtown Land Uses

Uses Category	DB	DT	DC	DE-1	DE-2	DT
Residential						
Housing Household Living						
Detached house	-	P	-	-	-	P
Attached house	-	P	-	P	P	P
Two-Unit house	-	P	-	P	P	P
Apartment/condo	S	S	P[17]	P	P	P
Group Living <i>(except for the following uses)</i>	-	-	-	-	-	-
Group home, small (8-person max. occupancy)	-	-	-	-	-	-
Group home, large (9 or more occupants)	-	-	-	-	-	-
Nursing home	-	-	-	-	-	-
Sheltered Care	-	-	-	-	-	-
Public, Civic & Institutional						
Aircraft Landing Area	-	-	-	-	-	-
Cemetery	S[6]	-	S[6]	-	-	-
College or University	S	S	S	S	S	S
Community Center	S	S	S	S	S	S
Fraternal Organization	S	-	S	S	S	-
Governmental Facility	P	P	P	P	P	P
Hospital	-	-	-	-	-	-
Library	S	S	S	S	S	S
Museum or Cultural Facility	S	S	S	S	S	S
Natural Resource Preservation	P	P	P	P	P	P
Parks and Recreation	-	-	-	-	-	-
Religious Assembly	S[6]	S	S[6]	S	S	S
Safety Service	S	S	S	S	S	S
School	-	S	-	-	S	S
Utilities and Public Service Facility						
Minor	P	P	P	P	P	P
Major		S	S	S	S	S
Wireless Telecommunications						
Freestanding tower	S	S	S	S	S	S
Building or tower-mounted antenna	P	P	P	P	P	P
Commercial						
Adult Entertainment Establishment	-	-	-	-	-	-
Animal Service						
Boarding or shelter	-	-	-	-	-	-
Grooming	P	-	P	P	P	-
Veterinary Care	S	-	S	S	S	-
Assembly and Entertainment <i>(except for the following)</i>						
Auditorium	S	-	S	S	S	-
Cinema	S	-	S	S	S	-
Theater	S	-	S	S	S	-
Commercial Service						
Building service	S	-	-	S	S	-
Business Support Services	P	-	P	P	P	-
Consumer maintenance and repair	P	-	P	P	P	-
Personal improvement service	P	P[13]	P	P	P	P[13]
Fortune telling or psychic service	-	-	-	-	-	-
Massage therapy	-	-	-	-	-	-
Research service	S	-	S	S	S	-
Day Care						
Day care home	-	P	-	-	-	P
Day care center	S	S	-	S	S	S
Eating and Drinking Establishment						
Restaurant	P	-	P	P	P	-
Wine boutique	P	-	P	P	P	-

Uses Category	DB	DT	DC	DE-1	DE-2	DT
Financial Service	P	-	P	P	P	-
Funeral or Mortuary Service	-	-	-	-	-	-
Lodging	S	-	-	S	S	-
Bed and Breakfast			-	-	-	S
Office						
Business and professional office	P	S	P	P	P	S
Medical, dental and health practitioner	P/S[8]	S	P/S[8]	P	P	S
Parking, Non-Accessory	S	S	S	S	S	S
Retail Sales						
Convenience goods	P/S[12]	-	P/S[12]	P	P	-
Consumer shopping goods	P/S[12]	P[14]	P/S[12]	P	P	-
Guns and firearm supplies	-	-	-	-	-	-
Building supplies and equipment	P/S[12]	-	P/S[12]	P	P	-
Self-service Storage Facility	-	-	-	-	-	-
Studio, Instructional or Service	P	S	P	P	P	S
Trade School	S	-	S	S	S	-
Vehicle Sales and Service						
Commercial vehicle repair and maintenance	-	-	-	-	-	-
Commercial vehicle sales and rentals	-	-	-	-	-	-
Fueling station	S	-	-	-	-	-
Personal vehicle repair and maintenance	-	-	-	-	-	-
Personal vehicle sales and rentals	S	-	-	-	-	-
Vehicle body and paint finishing shop	-	-	-	-	-	-
Automobile dealership off-site vehicle storage	-	-	-	-	-	-
Wholesale, Distribution & Storage						
Equipment and Materials Storage, Outdoor	-	-	-	-	-	-
Trucking and Transportation Terminals	-	-	-	-	-	-
Warehouse	-	-	-	-	-	-
Wholesale Sales and Distribution	-	-	-	-	-	-
Industrial						
Artisan Industrial	-	-	-	-	-	-
Limited Industrial	-	-	-	-	-	-
General Industrial	-	-	-	-	-	-
Intensive Industrial	-	-	-	-	-	-
Junk or Salvage Yard	-	-	-	-	-	-
Recycling						
Recyclable Material Drop-off Facility	-	-	-	-	-	-
Recyclable Material Processing	-	-	-	-	-	-
Agriculture						
Animal Agriculture <i>(except as allowed under Chapter 5 of the Downers Grove Municipal Code)</i>	-	-	-	-	-	-
Crop Agriculture	-	-	-	-	-	-
Community Garden	P	P	P	P	P	P
Other						
Drive-in or Drive-Through Facility	-	S	-	-	-	-
Medical Cannabis Cultivation Center	-	-	-	-	-	-
Medical Cannabis Dispensing Organization	-	-	-	-	-	-

[1] Requires minimum lot area of 40 acres. Maximum 25% building coverage.

[2] Requires minimum lot area of 10 acres.

[3] Requires minimum lot area of 25 acres. Maximum 25% building coverage.

[4] Must be within 150 feet of a B district.

[5] Requires special use approval if above one dwelling unit per 4,000 square feet of lot area.

[6] Special Use only if use was in existence on or prior to June 7, 2005.

[7] Requires minimum seating capacity of 125 persons.

[8] Permitted as of right up to 3,000 sq. ft. (gross floor area); larger requires special use approval.

[9] Must be in a completely enclosed building.

[10] Maximum 10,000 sq. ft. (gross floor area).

[11] Permitted only if ancillary to the following principal uses: sporting goods stores, uniform supply stores and public safety equipment stores.

[12] Consignment stores and martial arts studios permitted as of right up to 3,000 square feet (gross floor area); larger requires special use approval. Other uses permitted as of right up to 15,000 sq. ft. (gross floor area); larger requires special use approval.

[13] Barber shops, beauty shops and salons only; must be on ground floor and may not exceed 2,500 square feet floor area.

[14] Art galleries and studios only; must be on ground floor and may not exceed 2,500 square feet floor area.

[15] Drive-through banks only.

[16] See Section 6.180.

[17] 2nd floor or above only as part of mixed-use development



DEPARTMENT OF COMMUNITY DEVELOPMENT DRAFT MEMO

To: Plan Commission
From: Stan Popovich, AICP
 Director of Community Development
Subject: 16-PLC-0019, Downtown Development Regulations Framework
Date: August 28, 2017

Synopsis

The Plan Commission will review the downtown development regulatory framework prepared by the Comprehensive Planning Ad Hoc Committee and provide comments and recommendations to the Village Council.

Plan Commission Recommendations

On June 26 and August 7, the Plan Commission reviewed the downtown development regulatory framework prepared by the Comprehensive Planning Ad Hoc Committee (CPC). Based on these discussions, staff has prepared the following tables which identify the Plan Commission's recommendations. These recommendations will be presented to the Village Council in September. Any additional comments, revisions or recommendations need to be discussed at the August 28th meeting so that staff can incorporate those comments into the report submitted to Village Council.

Two recommendations need to be finalized, both focused on density in the proposed DE-1 and DE-2 zoning districts. The Plan Commission discussed allowing a higher density in these areas, but did not identify a specific density recommendation. The Plan Commission should provide a density recommendation to the Village Council after discussing the existing density (1 unit per 800 square feet which is 54 units per acre) and the CPC proposed density.

Bulk Regulations

District	CPC	Plan Commission Recommendation
DE-1	<ul style="list-style-type: none"> Residential density is proposed to be 1 unit per 3,000 square feet of lot area. This is 14 units per acre. 	<ul style="list-style-type: none"> The 3,000 square feet per residential dwelling unit ratio should be reduced to meet the goals of encouraging higher density residential around the edges of downtown. The recommended density is ___ square feet per residential dwelling unit which equates to ___ units per acre.
DE-2	<ul style="list-style-type: none"> Residential density is proposed to be 1 unit per 4,000 square feet of lot area. This is 10 units per acre. 	<ul style="list-style-type: none"> The 4,000 square feet per residential dwelling unit ratio should be reduced to meet the goals of encouraging higher density residential around the edges of downtown. The recommended density is ___ square feet per residential dwelling unit which equates to ___ units per acre.

Uses

Use	CPC	Plan Commission Recommendation
Apartment / Condo	<ul style="list-style-type: none"> Allow apartment / condo uses as permitted uses in the DB zoning district only on 2nd floor or above in a mixed-use development 	<ul style="list-style-type: none"> Allow apartment / condo uses as permitted uses in the DB zoning district only on the 2nd floor or above as part of a mixed-use development where the first floor contains commercial service uses as defined in the use table.
Apartment / Condo	<ul style="list-style-type: none"> Allowed by-right in DE-1, DE-2, and DT zoning district 	<ul style="list-style-type: none"> Allowed as a Special Use in the DE-1, DE-2, and DT zoning districts
Bed & Breakfasts	<ul style="list-style-type: none"> Permit B&Bs in DT zoning district 	<ul style="list-style-type: none"> Permit B&Bs in DE-1, DE-2 and DT zoning districts
Art galleries and studios	<ul style="list-style-type: none"> No longer permit these uses in the DT zoning district 	<ul style="list-style-type: none"> Continue to permit these uses in the DT zoning district with the restrictions as noted in the use table

Design Guidelines

CPC	Plan Commission Recommendation
<ul style="list-style-type: none"> Continue the current process 	<ul style="list-style-type: none"> Further explore the dynamic of regulations vs. guidelines Create a balance between regulations and guidelines, current guidelines are not strict enough but mandatory regulations may be too strict Promote natural materials Limit use of undesirable materials Allow for more public input early in the design process Include design categories such as parking lots, lighting design and landscaping Tailor guidelines to DC, DE-1, DE-2 and DT zoning districts and expand to Fairview Avenue Focus Area Provide links to Design Guidelines in additional areas on website Better define what projects staff reviews for compliance with guidelines

Historic Preservation

CPC	Plan Commission Recommendation
<ul style="list-style-type: none"> Did not discuss historic preservation 	<ul style="list-style-type: none"> Historic Preservation is well detailed in the Comprehensive Plan Provide links to historic preservation tax credits on website

Schedule

The Plan Commission recommendation and all supporting documentation which is attached will be forwarded to the Village Council for their review in September.



DEPARTMENT OF COMMUNITY DEVELOPMENT MEMO

To: Plan Commission
From: Stan Popovich, AICP
 Director of Community Development
Subject: 16-PLC-0019, Downtown Development Regulations Framework
Date: August 7, 2017

Synopsis

The Plan Commission will review the downtown development regulatory framework prepared by the Comprehensive Planning Ad Hoc Committee and provide comments and recommendations to the Village Council.

Summary of June 24, 2016 Meeting

At the June 24, 2016 meeting, the Plan Commission discussed existing and proposed bulk regulations and reviewed the proposed use table modifications. Key discussion items included:

Bulk Regulations

Plan Commission Comment	Staff Response
In DE-1, the 3,000 square feet per residential dwelling unit would not facilitate the density that the Comprehensive Plan identifies.	Concur. Staff would recommend a lower square foot per residential dwelling unit number. See Table 1 below for a comparison of existing downtown densities and Table 2 below for information on how other communities determine density.
In DE-2, the 4,000 square feet per residential dwelling unit would not facilitate the density that the Comprehensive Plan identifies.	Concur. Staff would recommend a lower square foot per residential dwelling unit number. See Table 1 below for a comparison of existing downtown densities and Table 2 below for information on how other communities determine density.
In the DC, the 800 square feet per residential dwelling unit may be too dense.	Staff believes the current standard is appropriate. See Table 1 below for a comparison of existing downtown densities and Table 2 below for information on how other communities determine density.

Uses

Plan Commission Comment	Staff Response
Bed and breakfasts and inns should be included in the DE zoning classifications.	Under the current zoning classifications, both bed and breakfasts and inns are a form of lodging. In the 2014 Zoning Ordinance update, the use classification list was broadened and generalized to provide staff with additional flexibility when determining potential classifications for new businesses. Staff concurs that

	<p>a sub-group including bed and breakfasts be created because that is a unique type of lodging arrangement. An inn is similar to a hotel and thus should be maintained under the lodging classification.</p>
Continue to allow art galleries and studios in the DT district (see note 14 in use table).	Concur
Modify note 17 in the use table to read: Apartment/condo uses are permitted on the 2 nd floor or above only as part of a mixed-use development in which the first floor contains commercial service uses.	Concur
In the DC district, it was suggested to allow offices as special uses on the 1 st floor and permitted on the 2 nd floor or above.	<p>As seen below in Table 3, there are many offices currently located in the DB and DT zoning districts that would be impacted by a change in zoning classification or a re-classification from a permitted use to a special use. It is staff's recommendation to continue to allow offices as a permitted use in the DC, DE-1 and DE-2.</p> <p>A majority of the properties with an existing office that would require a special use result from rezoning from DB to DT. This is particularly true of the area on the east side of Main Street between Rogers and Franklin Streets.</p> <p>Additionally, there are some blocks designated for DT that are primarily older homes converted to offices and other DT blocks with older homes that remain primarily residential in nature. Office uses in the DT should be carefully examined to determine the appropriate action.</p>
In the DT district, one commissioner suggested the Village continue to allow drive-through uses as a special use while another commissioner supported removing auto-oriented uses from the downtown.	Staff's recommendation is to prohibit new auto-oriented uses in the downtown to encourage a pedestrian oriented downtown. The existing drive-through uses can continue as a lawful non-conforming use.
In all districts, it was suggested that apartment / condo uses remain special uses.	Staff's recommendation is to allow apartment / condo uses as a permitted use to encourage these uses in the downtown as identified in the Comprehensive Plan. To obtain a building permit, the developments would need to comply with all the bulk regulations and the design guidelines. Any request to not comply with these regulations would require a variance which has a higher standard of approval.

A commissioner inquired about the re-classification of the properties on the north side of Gilbert Avenue that are currently zoned DT and R-6, converting back to single family residential.	As shown in Table 4 below, this area would need to be examined further to determine the impact to the existing and proposed uses and what zoning classification is appropriate.
A commissioner inquired about the re-classification of properties on the south side of Rogers Street between Washington Street and Prospect Ave that are currently zoned DT, converting back to a residential zoning classification.	As shown in Table 4 below, this area would need to be examined further to determine the impact to the existing and proposed uses and what zoning classification is appropriate.

Table 1 Existing multi-family developments in the downtown and their densities (ordered by most dense to least dense):

Building	Number of Units	Lot Area (sq ft)	Lot Area / Unit	Density (units / acre)	Parking Ratio
Main & Maple	115	37,961	330	132	1.41
5100 Forest	89	48,136	541	81	1.19
Oak Tree Tower	164	102,366	624	70	0.40
Acadia on the Green	126	89,734	712	61	1.17
1008 - 1010 Curtiss	18	13,504	750	58	0.00
1122 Gilbert Avenue (Immanuel Residences)	120	90,605	755	58	0.30
Current Village Regulation			800	54	1.40
940 Maple	55	44,704	813	54	1.44
5202 Washington	27	24,394	903	48	1.30
Station Crossing	48	43,969	916	48	1.46
4910 Forest	32	30,056	939	46	0.44
4929 Forest	24	22,847	952	46	1.54
1110 Grove	50	52,272	1,045	42	1.36
1108-1114 Curtiss	19	20,473	1,078	40	1.05
922 Warren	28	30,816	1,101	40	2.00
5133 Washington (Grove Tower)	24	27,060	1,128	39	1.96
907 Curtiss	35	42,253	1,207	36	1.23
835-839 Curtiss	22	26,572	1,208	36	0.95
5329 Main Street (Morningside)	40	48,352	1,209	36	2.00
4900 Forest	32	40,075	1,252	35	1.22
822 Warren	6	8,276	1,379	32	1.00

715-719 Rogers	20	32,227	1,611	27	2.00
817-819 Curtiss	8	13,504	1,688	26	1.13
930 Rogers	10	18,295	1,830	24	1.88
1132 Curtiss	16	30,056	1,879	23	2.13
5146 Belden	21	42,253	2,012	22	1.52
5123-5137 Carpenter	8	16,553	2,069	21	1.00
918 Rogers	8	19,166	2,396	18	1.88
Georgian Courts	25	74,910	2,996	15	2.40

Table 2 – Density calculations in other communities

Community	Zoning District	How they calculate density	Parking Ratio
Naperville	Downtown Core	No density requirement, density is handled through FAR, height and parking. FAR maximum = 2.5 Height Maximum = 60'	2 / unit
	Downtown Transition	No density requirement, density is handled through FAR, height and parking. FAR maximum = 2.5 Height Maximum = 50'	2 / unit
Glen Ellyn	Central Retail Core	No density requirement, density is handled through height regulations. Height Maximum = 45'	n/a
	Central Service Subdistrict	No density requirement, density is handled through height and parking. Height Maximum = 55'	1 /efficiency 1.5 / 1 bedroom 2 / 2 bedroom +
Wheaton	Retail Core Business	No density requirement, density is handled through height regulations. Height Maximum = 50' or 4 stories	n/a
Hinsdale	Central Business	No density requirement, density is handled through FAR, height and parking. FAR maximum = 2.5 Height Maximum = 30' or 2 stories	Fee in lieu or 1 / efficiency 2 / 1-2 bedroom 3 / 3 bedroom plus 1 for each 20 required
Elmhurst	Central Business	Lot Area per dwelling unit = 1,500 sf (29 units / acre)	2 / 1-2 bedroom 2.5 / 3 bedroom
	North Downtown Business	Lot Area per dwelling unit = 1,500 sf (29 units / acre)	2.5 / dwelling unit
Lombard	Central Business	No density requirement, density is handled through height and parking. Height Maximum = 45'	1.5 / 1-2 bedroom 2 / 3 bedroom +

Lisle	Central Retail Core	Density is handled individually through Special Use process. No minimums / maximums All development is Special Use	1 / 1 bedroom 1.5 / 2 bedroom +
	Downtown Perimeter	Density is handled individually through Special Use process. No minimums / maximums All development is Special Use	1 / 1 bedroom 1.5 / 2 bedroom +

Table 3 – Uses which may be impacted by proposed zoning changes

Address	Existing Zoning	Proposed Zoning	Existing Use	Business Name	Existing Use Status (Permitted/ Special Use/ Prohibited)	Proposed Use Status (Permitted/ Special Use/ Prohibited)
1035 Grove St	DB	DT	Office: Business/ Medical	Expert Accounting / Counseling & Consulting Services	P	S
4901 Main St	DB	DT	Office: Business	Exit Real Estate Partners	P	S
4905 Main St	DB	DT	Office: Business	Charism Home Care Services, CHI LAW OFFICE, George Swimmer - Prudential Financial	P	S
4913 Main St	DB	DT	Office: Medical	Dr. Stephen T Jagielo D.D.S.	P (under 3000SF)	S
4913 Main St	DB	DT	Building Service	J P Concrete	S	--
4915 Main St	DB	DT	Office: Business	Wiedel Hudzik & Russ	P	S
4919 Main St	DB	DT	Office: Business	Allers Morrison, Quantum Insurance, Rhoades Brother Inc.	P	S
4920 Main St	DB	DE-1	Funeral Service	Toon Funeral Home	--	--
4923 Main St	DB	DT	Office: Medical	Alder Grove Counseling	P	S
4923 Main St	DB	DT	Office: Business	MG Computer	P	S
4927 Main St	DB	DT	Office: Business	Farmers Insurance, Country Financial	P	S
4941 Main St	DB	DT	Office: Business	Wenzel Select Properties	P	S
4945 Forest Ave	DB	DE-1	Commercial: Veterinary	All Creatures Great and Small - Veterinary Practice	P	S
4947 Main St, B	DB	DT	Personal Vehicle Maintenance and Repair	Langs Auto Service	--	--
4947 Main St, A	DB	DT	Consumer Maintenance and Repair	Crystal Formal Wear Co.	P	--

5100 Main St	DB	DC	Service: Financial Drive- Through	U.S. Bank	--	--
5114 Main St	DB	DC	Office: Business	Coldwell Banker Residential Brokerage	P	P (1st FL)
5116 Forest Ave	DB	DT	Office: Business/ Medical	Trinity Family Law, Patrick J. Smith Law Offices, MaryAnn Cushing Smith, Ann F. Murray	P	S
5120 Forest Ave	DB	DT	Office: Business	Heath & Associates	P	S
5122 Main St, A	DB	DC	Office: Business	Keller Williams Realty	P	P (1st FL)
5140 Main St	DB	DC	Service: Financial Drive- Through	Bank Financial	--	--
5145 Washington St	DB	DT	Office: Medical	Downers Grove Smiles Richard E. Patelski, DDS	P (under 3000SF)	S
5149 Main St	DB	DC	Office: Medical	Smile Downers Grove	P	P (1st FL)
5201 Washington St	DB	DT	Office: Business/ Medical	Solid Foundations Therapy Stabile Investment Westberg Robert	P	S
5205 Washington St	DB	DT	Office: Business	Our House in Town, Law offices of Paul J Fina, Gregory J Abbott Attorney At Law, Daniel Mc Cormick PC	P	S
5207 Main St, 6	DB	DC	Office: Medical	Family Health of Downers Grove	P (under 3000SF)	P (1st FL)
5300 Main St	DB	DT	Office: Business	Commercial Lending Consultants	P	S
5312 Main St	DB	DT	Office: Medical	American Family Chiropractic	P (under 3000SF)	S
5330 Main St	DB	DT	Service: Financial	West Suburban Bank	P	--
830 Warren Ave	DB	DT	Building Service	Customized FX	S	--
844 Warren Ave	DB	DT	Personal Vehicle Maintenance and Repair	Automotive Services	--	--

902 Maple Ave	DB	DT	Office: Business/ Medical	Fleming Marks & Iuorio, Donati Financial Services Inc, Harke Insurance Agency LLC, Glazko Julia a PhD	P	S
907 Rogers St	DB	DT`	Service: Studio	Image Makers	P	S
911 Rogers St	DB	DT	Office: Business	Studio 21	P	S
944 Curtiss St, 1	DB	DC	Office: Medical	Guy Atoinette DDS	P	P (1st FL)
1111 Warren Ave	DB	DE-2	Service: Financial Drive- Through	Community Bank of Downers Grove	--	--
1201 Warren Ave	DB	DE-2	Limited Industrial	Dicke Safety Products	--	--

Going from Permitted to Special Use

Going from Permitted or Special Use to Not Permitted

Lawful Nonconforming to Lawful Nonconforming

P = Permitted Use

S = Special Use

- = Not permitted

Table 4 – Uses which may be impacted by proposed zoning changes

Address	Existing Zoning	Proposed Zoning	Existing Use	Business Name	Existing Use Status (Permitted/Special Use/Prohibited)	Proposed Use Status (Permitted/Special Use/Prohibited)
720 Maple Ave	DT	R4	Multi-Family		P	--
5122 Mackie Pl	DT	R4	Single Family		P	P
5334 Main St	DT	R4	Single Family		P	P
5338 Main St	DT	R4	Single Family		P	P
1140 Gilbert Ave	DT	R4	Single Family		P	P
1144 Gilbert Ave	DT	R4	Multi-Family		P	--
1200 Gilbert Ave	DT	R4	Single Family		P	P
1204 Gilbert Ave	DT	R4	Single Family		P	P
1225 Warren Ave	DB	R4	Office: Business	Mayflower Tours	P	S
1219 Warren Ave	DB	R4 or M1	Limited Industrial	Dicke Safety Products	P	S or P
1212 Warren Ave	DT	R4	Single Family			
1208 Warren Ave	DT	R4	Unimproved		Unimproved	N/A
1210 Warren Ave	DT	R4	Unimproved		Unimproved	N/A
4942 Saratoga Ave	DT	R4	Unimproved		Unimproved	N/A
4952 Saratoga Ave	DT	R4	Single Family	Dicke Safety Products	P	P
4948 Saratoga Ave	DT	R4	Single Family		P	P
835 Rogers St	DT	R4	Single Family		P	P
829 Rogers St	DT	R4	Single Family		P	P
825 Rogers St	DT	R4	Single Family		P	P
821 Rogers St	DT	R4	Single Family		P	P
817 Rogers St	DT	R4	Single Family		P	P
813 Rogers St	DT	R4	Single Family		P	P
809 Rogers St	DT	R4	Single Family		P	P
803 Rogers St	DT	R4	Single Family		P	P
747 Rogers St	DT	R4	Single Family		P	P
745 Rogers St	DT	R4	Multi-Family		P	--
739 Rogers St	DT	R4	Single Family		P	P
735 Rogers St	DT	R4	Single Family		P	P
731 Rogers St	DT	R4	Multi-Family		P	--
727 Rogers St	DT	R4	Single Family		P	P
717 Rogers St	DT	R4	Multi-Family		P	--

Going from Permitted or Special Use to Not Permitted

P = Permitted Use

S = Special Use

- = Not permitted

Design Guidelines

The Village currently uses the [Downtown Design Guidelines](#) to guide the exterior appearance of downtown developments that require entitlement approvals and to make suggestions to by-right developments. The guidelines are not intended to dictate architecture or building style, but rather influence the design of the building so that they adhere to the Comprehensive Plan recommendations to create pedestrian-oriented development, and to maintain a commitment to quality architecture. The guidelines cover five sections: site design, building design, building base, building middle and building top. The guidelines describe elements that support good design and provide visual references to emphasize both encouraged and discouraged elements. Design guidelines are similar to the Comprehensive Plan in that both are visionary documents but are not regulatory.

Currently, staff works with petitioners and makes recommendations to meet the design guidelines during the review process. Compliance with the design guidelines is applied as a standard for approval. The CPC explored the use of design guidelines in the downtown and determined that the current guidelines and their application are appropriate for the downtown.

Historic Preservation

The new historic preservation ordinance was established to encourage preservation of historically significant structures throughout the Village. Since its adoption in late 2015, there have been no landmark applications from downtown property owners other than the Village owned train stations. Additionally, the Comprehensive Plan identifies several buildings and sites in the downtown which may be historically significant. The CPC did not discuss the historic preservation ordinance and its application to the downtown.

Plan Commission Schedule

The Village envisions three meetings to comment on the CPC recommended regulatory framework and develop a report and recommendation to the Village Council. A schedule of topics to be covered at each meeting is shown below:

- | | |
|-----------|---|
| June 26 | <ul style="list-style-type: none"> • Review process • Review roles and responsibilities of CPC, PC and Village Council • Review deliverables • Review Comprehensive Plan's Downtown Focus Area – pages 104-113 • Review and comment on CPC bulk regulation recommendations • Review and comment on CPC use table |
| August 7 | <ul style="list-style-type: none"> • Review June 26 meeting comments <ul style="list-style-type: none"> ○ Key Commission discussion items ○ Staff prepared information regarding bulk and use • Review existing design guidelines and their use • Review and comment on CPC design guideline recommendations • Discuss historic preservation in the downtown |
| August 28 | <ul style="list-style-type: none"> • Finalize report to the Village Council • Make recommendation to the Village Council |

Additional Links

Comprehensive Plan – adopted June 13, 2017

http://www.downers.us/public/docs/departments/com_dvlpmnt/CompPlan2017_Approved_061317.pdf

January CPC meeting agenda packet

http://www.downers.us/public/docs/board_commission/Comp_Plan_2016/Agenda-011617.pdf

February CPC meeting agenda packet

http://www.downers.us/public/docs/board_commission/Comp_Plan_2016/Agenda-022017.pdf

Downtown Design Guidelines

http://www.downers.us/public/docs/board_commission/architectural_design_review/Downtown_Design_Guidelines.pdf



DEPARTMENT OF COMMUNITY DEVELOPMENT MEMO

To: Plan Commission
From: Stan Popovich, AICP
 Director of Community Development
Subject: 16-PLC-0019, Downtown Development Regulations Framework
Date: June 26, 2017

Synopsis

The Plan Commission will review the downtown development regulatory framework prepared by the Comprehensive Planning Ad Hoc Committee and provide comments and recommendations to the Village Council.

Background

The Village Council created the Comprehensive Planning Ad Hoc Committee (CPC) to complete two items:

- Update the Village's 2011 Comprehensive Plan
- Develop a downtown development regulatory framework based on the policy detailed in the Comprehensive Plan.

The CPC completed their work on the Comprehensive Plan at their November 2016 meeting and the updated Comprehensive Plan was adopted by the Village Council on June 13, 2017. As shown below, the downtown focus area of the Comprehensive Plan was discussed at various meetings by the CPC, Plan Commission and the Village Council.

Comprehensive Plan Downtown Focus Area Meetings

Board	2016								2017					
	M	J	J	A	S	O	N	D	J	F	M	A	M	J
CPC	★	★					★							
PC		★						★	★					
VC			★			★				★	★	★	★	★

The development and review of a downtown development regulatory framework in support of the Comprehensive Plan will go through a similar process. The CPC developed the regulatory framework in January and February of 2017. The Plan Commission is charged with reviewing, commenting and making a recommendation to the Village Council on the regulatory framework in the next three months. The Village Council will review the regulatory framework and Plan Commission comments and recommendations beginning in September.

Downtown Development Regulatory Framework Schedule

Board	2017								
	J	F	M	A	M	J	J	A	S
CPC	★	★							
PC						★	★	★	
VC									★

The role that each Board has in the development of the regulatory framework in support of the Comprehensive Plan is shown in the table below:

Board	Role	Deliverables
CPC - 7 member Ad Hoc Board <ul style="list-style-type: none"> • 2 PC members • 2 ZBA members • 1 ADRB member • 1 EDC member • 1 Downtown Mgmt member • 1 SFPOC member • 1 TPC member 	Develop a downtown regulatory framework in support of the policies and recommendations in the Comprehensive Plan	Recommended regulatory framework
Plan Commission	Review the CPC regulatory framework Provide comments and recommendations to the Village Council	Meeting minutes and a report to the Village Council
Village Council	Review CPC regulatory framework Review PC comments and recommendations on the regulatory framework Approve the regulatory framework or direct that the PC or CPC make revisions to the framework	Approved regulatory framework

Comprehensive Plan, Regulatory Framework and Development Regulations

The Comprehensive Plan is a long-range guide for the maintenance and enhancement of existing areas and for improvements, developments and redevelopments within the Village. The Plan serves as a foundation for decision making in the community. It is intended to inform regulatory tools (such as the Zoning Ordinance) and also the Village's decisions as leaders determine courses of action and the most appropriate forms of development and growth in the community. The Plan's policies and guidelines should inform any revision to the development regulations to ensure consistency between the Plan and the development regulations.

Development regulations are intended to be used on a daily basis to ensure improvements to existing developments, redevelopments and new developments meet the standards of the Village. Development regulations protect the public health, safety and general welfare of the community while also implementing the policies and goals contained within the Comprehensive Plan. These regulations establish the types of uses allowed on specific properties and prescribe the overall character and intensity of development to be permitted.

It is essential that the development regulations are reviewed and updated as necessary to reflect the vision and policies in the Comprehensive Plan.

A regulatory framework is a bridge between the Comprehensive Plan and development regulations. The regulatory framework identifies Comprehensive Plan concepts and develops key regulatory topics for review and discussion that allows for concurrence before the regulations are drafted and adopted.

A summary of these three items is provided below:

Comprehensive Plan	Regulatory Framework	Development Regulations
<ul style="list-style-type: none"> • 15-20 year vision of the future beyond the day-to-day issues • Guideline for decision making • Plan is not legally binding • Desired state of Downtown <ul style="list-style-type: none"> ○ Physical ○ Economic ○ Social ○ Environmental 	<ul style="list-style-type: none"> • Includes key concepts for regulations that serve as the basis to draft zoning, subdivision, historic preservation or design ordinances • Key concepts should be designed to produce the physical, social and economic conditions described in the Comprehensive Plan • Allows for discussion and concurrence prior to spending resources to create legally binding regulations 	<ul style="list-style-type: none"> • Regulates day-to-day activities • Implements the vision of the Comprehensive Plan • Regulations are legally binding • Laws which govern development and use • Multiple ordinances and documents • Each document intended to create conditions that were recommended in the Comprehensive Plan • Developed by staff, including the Village Attorney

Proposed Downtown Development Regulatory Framework

The CPC's proposed downtown development regulatory framework includes:

- Creation of four zoning districts
 - Downtown Core
 - Downtown Edge-1
 - Downtown Edge-2
 - Downtown Transition
- Modifications to the Zoning Ordinance bulk requirements
- Modifications to the Zoning Ordinance land use table
- No modifications to existing Downtown Design Guidelines and their application
- No modifications to Historic Preservation as it relates to historic structures in the downtown

Zoning Districts

The CPC found that the three downtown subareas identified in the Comprehensive Plan would best be implemented by creating four zoning districts:

- *Downtown Core* is a place that has served as the social and civic heart of the downtown for 150 years and continues to attract people to gather as pedestrians.
- *Downtown Edge-1* is a place that is a combination of transit-oriented development and an area of greater residential density. Due to its proximity to the Downtown Core, this district should contain many of the characteristics of the core, including minimal building setbacks.
- *Downtown Edge-2* is a place that is a combination of transit-oriented development and an area of greater residential density. However, due to its location near the Downtown Transition and adjacent residential neighborhoods, this district should contain characteristics similar to the transition, such as building setbacks.
- *Downtown Transition* is an area that plays an important role in helping transition between more intensive uses in the downtown and the surrounding neighborhoods.

Bulk Requirements

Bulk requirements guide the physical development of a property primarily through height, parking, setback and density. These requirements determine where on the property a building can be placed, how big the building can be and how tall that building can be. The proposed bulk regulations in the regulatory framework reflect the Comprehensive Plan's vision for the downtown. Shown below are the significant proposed modifications to the bulk regulations. It should be noted that in some instances, the new downtown districts encompass one or more existing zoning classifications. As such, some existing regulations are shown with a varying range.

Downtown Core

Regulation	Existing	Proposed
Maximum Height	70 feet	40 feet / 3 stories
Minimum Height	32 feet	24 feet / 2 stories

Downtown Edge 1

Regulation	Existing	Proposed
Maximum Height	70 feet	72 feet / 6 stories
Minimum Height	32 feet	24 feet / 2 stories
Side Yard Setback	Varies from 0-5 feet	5 feet / 10%
Rear Yard Setback	0 feet	10 feet

Downtown Edge 2

Regulation	Existing	Proposed
Maximum Height	60 - 70 feet	60 feet / 5 stories
Minimum Height	32 feet	n/a
Street Yard Setback	Varies from 0 – 20 feet	10 feet
Side Yard Setback	Varies from 0-5 feet	5 feet / 10%
Rear Yard Setback	Varies from 0 – 20 feet	10 feet

Downtown Transition

Regulation	Existing	Proposed
Maximum Height	35 - 70 feet	36 feet / 3 stories
Minimum Height	32 feet	n/a
Street Yard Setback	Varies from 0 – 20 feet	10 feet
Side Yard Setback	Varies from 0-5 feet	5 feet / 10%
Rear Yard Setback	Varies from 0 – 20 feet	10 feet

Land Use Table

The location of industrial, institutional, retail, residential and service uses in the Village are regulated by a Use Table within the Zoning Ordinance. Based on CPC discussions, a proposed use table has been created which identifies allowable uses in each of the four proposed districts. Similar to the bulk regulations, some proposed districts encompass one or more existing zoning classifications and their uses. A few significant recommendations are noted below:

Downtown Core

- Apartment / condominium uses are allowed by-right on the 2nd floor or above as part of a mixed-use development
- Building service (contractor office, plumbing or landscape services), day care center, lodging, fueling stations and personal vehicle sales and rental uses are no longer permitted

Downtown Edge-1 and Edge-2

- Apartment / condominium uses are allowed by-right
- Convenience goods, convenience shopping goods and building supply uses permitted by right with no limitation on size
- Fueling stations and personal vehicle sales and rentals are no longer permitted

Downtown Transition

- Apartment / condominium uses are allowed by-right
- Bed and breakfast use is added as a Special Use
- Art galleries and studios no longer permitted

Design Guidelines

The Village currently uses the [Downtown Design Guidelines](#) to guide the exterior appearance of downtown developments that require entitlement approvals. The guidelines are not intended to dictate architecture or building style, but rather influence the design of the building so that they adhere to the Comprehensive Plan recommendations to create pedestrian-oriented development, and maintain a commitment to quality architecture. The guidelines cover five sections: site design, building design, building base, building middle and building top. The guidelines describe elements which support good design and provide visual references which identify both encouraged and discouraged elements. Design guidelines are similar to the Comprehensive Plan in that both are visionary documents but are not regulatory.

Currently, staff works with petitioners on meeting the design guidelines and makes recommendations to petitioners to meet the design guidelines during the review process. Compliance with the design guidelines is applied as a standard for approval. The CPC explored the use of Design Guidelines in the downtown and determined that the current guidelines and their application are appropriate for the downtown.

Historic Preservation

The new historic preservation ordinance was established to encourage preservation of historically significant structures throughout the Village. Since its adoption in late 2015, there have been no landmark applications from downtown property owners. Additionally, the Comprehensive Plan identifies several buildings and sites in the downtown which may be historically significant. The CPC did not discuss the historic preservation ordinance and its application to the downtown.

Upcoming Plan Commission Schedule

The Village envisions three meetings to comment on the CPC recommended regulatory framework and develop a report and recommendation to the Village Council. A schedule of topics to be covered at each meeting is shown below:

- | | |
|-----------|--|
| June 26 | <ul style="list-style-type: none"> • Review process • Review roles and responsibilities of CPC, PC and Village Council • Review deliverables • Review Comprehensive Plan's Downtown Focus Area – pages 104-113 • Review and comment on CPC bulk regulation recommendations • Review and comment on CPC use table |
| July 24 | <ul style="list-style-type: none"> • Review existing design guidelines and their use • Review and comment on CPC design guideline recommendations • Discuss Historic Preservation in the downtown |
| August 28 | <ul style="list-style-type: none"> • Finalize report to the Village Council • Make recommendation to the Village Council |

Attachments

Comprehensive Plan - Downtown Focus Area
CPC recommended regulatory framework
Downtown Design Guidelines
CPC meeting minutes (January and February)

Additional Links

Comprehensive Plan – adopted June 13, 2017

http://www.downers.us/public/docs/departments/com_dvelopment/CompPlan2017_Approved_061317.pdf

January CPC meeting agenda packet

http://www.downers.us/public/docs/board_commission/Comp_Plan_2016/Agenda-011617.pdf

February CPC meeting agenda packet

http://www.downers.us/public/docs/board_commission/Comp_Plan_2016/Agenda-022017.pdf

Downtown Design Guidelines

http://www.downers.us/public/docs/board_commission/architectural_design_review/Downtown_Design_Guidelines.pdf

Key Focus Areas

Downtown

For over 150 years, downtown Downers Grove has been the symbolic heart of the community and remains a key focal point for social and civic life. The downtown area is notable for many historic buildings and places such as the Tivoli Theatre, the Main Street Cemetery and the Masonic Temple among others. The importance of downtown to the identity of the community cannot be overstated. Downtown is the "place" where all of Downers Grove comes together; preserving that characteristic is the single most important aspect for downtown planning. Accordingly, a "placemaking" approach to planning, zoning and design should be adopted and implemented. Going forward, the challenge will be to maintain this traditional sense of place as well as the look and feel of a historic downtown in a manner that is economically sustainable.

Downtown is bisected by the BNSF railroad tracks and adjacent commuter parking lots. Main Street is the central business corridor in Downtown and has a coordinated streetscape from Franklin Street to Maple Avenue. In recent years, several new developments have occurred in Downtown providing new housing, parking and retail opportunities. Recommendations in this subarea plan take into consideration both the history of Downtown as well as these recent developments.

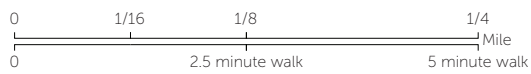
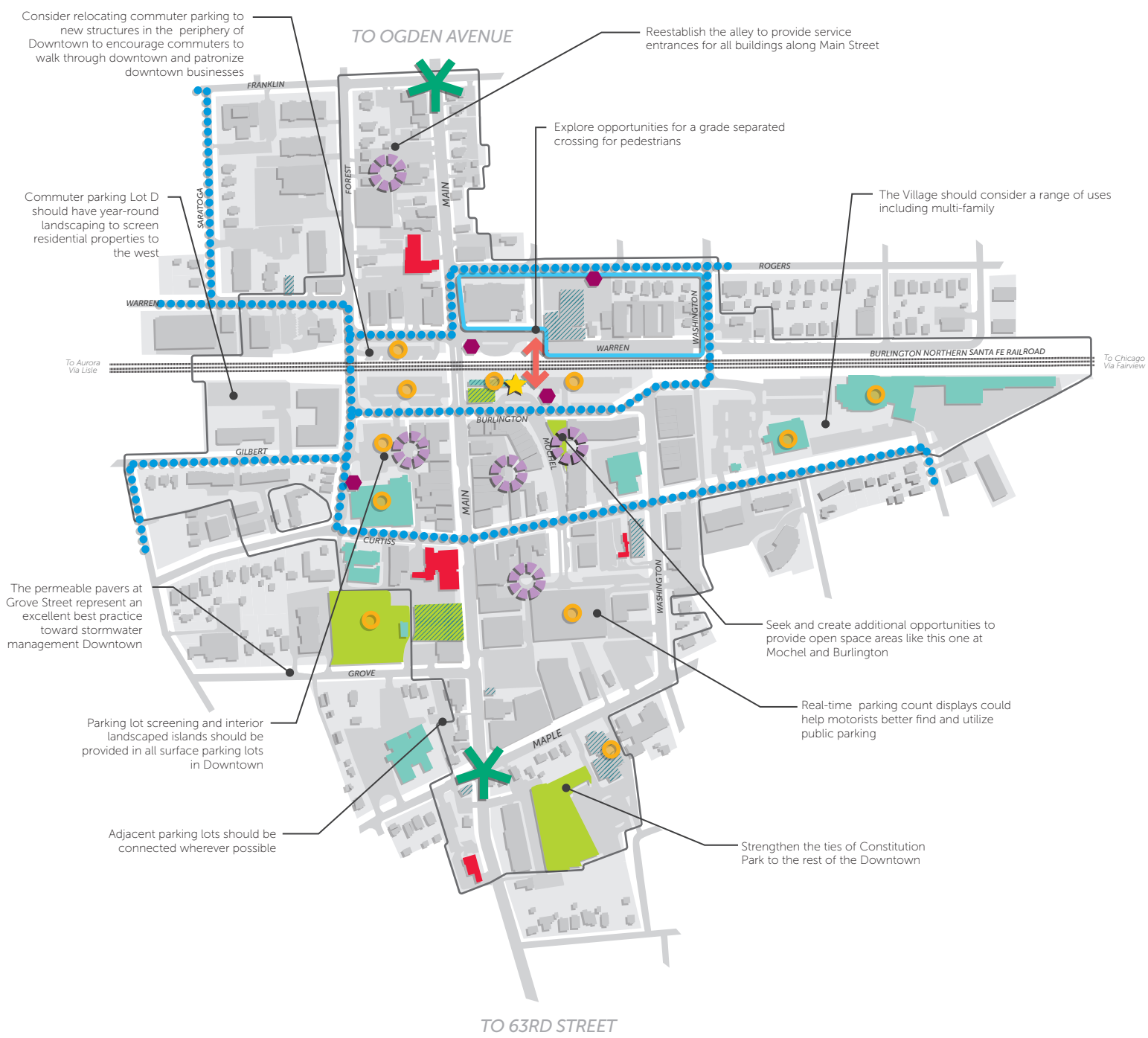
Key Concepts

- An improved Downtown wayfinding system should be a priority for Downtown. Wayfinding should include key destinations, public parking facilities, Village Hall, historic landmarks, Downtown parks and facilities, and Metra. Wayfinding can not only direct pedestrians and motorists to destinations in the Downtown, but can help promote the Downtown's unique amenities to commuters and visitors.
- Downtown's urban environment contains a lot of concrete and asphalt, which contributes to stormwater runoff. Where possible, best management practices should be constructed in order to improve stormwater management. Grove Street's permeable pavers serves as a good example of how this could be implemented in downtown.
- Developing boundaries for the Downtown's transition areas should be a priority for the Downtown. Opportunities to expand the boundaries of the Downtown are limited, however there are opportunities for intensification. The priority for the Downtown should be on infill development and redevelopment of key sites in order to maximize the Downtown's potential while strategically evaluating opportunities to expand the boundaries.
- Infill development and redevelopment should be pedestrian-oriented in order to complement the historic building pattern of the Downtown. Retail shops with attractive display windows and restaurants with sidewalk cafes maintain visual interest and generate foot traffic.
- Prohibit new and redevelop existing, non-pedestrian-oriented businesses including the strip commercial center on north Main Street and auto-oriented businesses, including drive-thru uses which should be relocated outside of the Downtown. Office uses should be encouraged to occupy space above the ground floor.



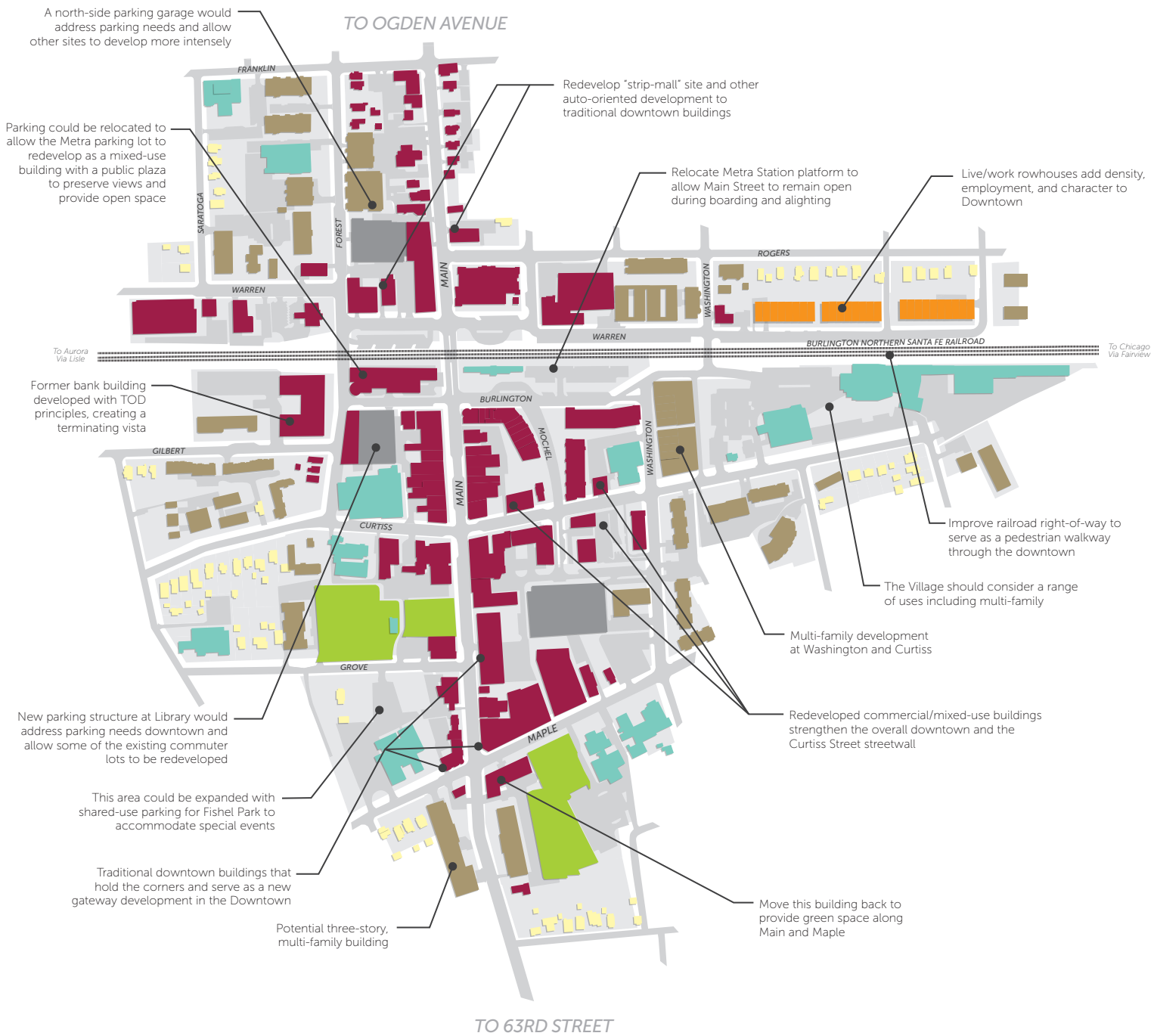
- To maintain the Downtown's unique identity and character, the Village should consider policies, programs and tools to identify and facilitate the protection of historic buildings and sites and encourage adaptive reuse of historic structures.
- The Village should maintain a commitment to quality architecture through the development of tools and design guides for the Downtown properties.
- As key properties redevelop, a sense of enclosure should be maintained to provide comfort to pedestrians. A sense of enclosure is attained through the combination of street widths and building height in proportion to the historic building pattern of the Downtown
- The importance of public uses (churches, Village Hall, parks, library, post office, and social services) cannot be overstated for the continued success of the Downtown. Preserving key streets as commercial corridors (e.g. Main Street) while also providing areas for public uses encourages visitors to make several stops during a trip to Downtown and encourages them to stay longer.
- Consider dedication of surface parking for shoppers and parking deck use for commuters. The Village should also consider parking counters at public lots and the Parking Deck that will provide drivers with real time information on the number of available parking spots as they navigate the Downtown.
- Reinforce the Downtown as the primary focal point in the community by working with Downtown Management to promote the activation of the Downtown's gathering spaces with special events, public art, and other temporary outdoor uses.
- Guided by the findings of the 2011 Parking Study, explore suitable locations on the north side of the railroad tracks for expanded parking, including the potential for a new parking deck. Not only would this provide businesses on the north end of the downtown and north side commuters with a parking option, but it could serve as a catalyst for north side investment by allowing new development to buy into the parking deck and allow them to fully utilize their property. On either Forest Avenue or Main Street between Franklin Street and Warren Avenue may represent a potential location.
- Identify areas for centralized garbage collection for businesses in the Downtown Core. A centralized dumpster area should be well screened and can remove this unsightly, yet necessary component of business operation to open up alleyways and the rear of buildings for storefronts and rear entrances.

- Encourage outdoor seating areas for restaurants and entertainment uses by streamlining the permitting process and reducing restrictions on sidewalk seating. Additionally, the Village should identify opportunities to expand sidewalks where appropriate and utilize existing set backs on buildings to create plazas and larger outdoor areas. The Village could consider a pilot program to expand outdoor seating into on-street parking spaces as some other communities have done.
- Promote business initiated/focused special events, such as sidewalk sales, to increase energy and activity in Downtown.
- Promote historic preservation as a means to preserve the existing building stock and historic architectural character of the village.
- With bus and train service, the Downtown is, and should remain, a multi-modal environment that fosters a sense of energy and vitality. As a complement to public transportation, active transportation and the use of bicycles should also be supported and encouraged. However, the Downtown should first remain a “walkable” area. The Village should support the installation of additional bike parking at both public and private facilities in the Downtown’s periphery to allow cyclists to park and secure their bikes and become “pedestrians” in the Downtown.
- The Village should explore the feasibility of constructing a pedestrian grade separated crossing near the Metra station. This significant capital investment would increase the safety of commuters and residents as they cross the railroad tracks. The Village should look into funding sources to help finance its construction, including grants from Burlington Northern Santa Fe (BNSF), the Illinois Commerce Commission, federal grants, and Metra.
- Ensure that adequate parking is provided for motorcycles and scooters in the Downtown.
- The Village should review circulation patterns, roadway capacities, and parking availability in the downtown after the completion of the Marquis on Maple development and the mixed-use and residential developments currently planned in the Downtown.
- Consider the establishment of a Downtown Design Review Board.
- Encourage greater residential density in the Downtown Edge to help facilitate a vibrant and energetic downtown. By allowing taller buildings and increased density in the Edge this will also increase the importance of the Downtown Transition area to buffer the neighboring residential areas.
- The success of downtown can be attributed to the coordination and combination of these key features: architectural detailing, a mix of land uses, pedestrian-oriented design, appropriate building heights, streetwalls and storefronts, a compact street grid, and public spaces and gathering areas. Future development or redevelopment should take into consideration these elements.
- Use the 2008 Downtown Pattern Book as a resource and guide for future development or redevelopment.
- As new development or redevelopment occurs, infrastructure improvements, such as the BNSF crossing at Washington Street, may be necessary to accommodate increased residential density.



Key Concept Recommendations

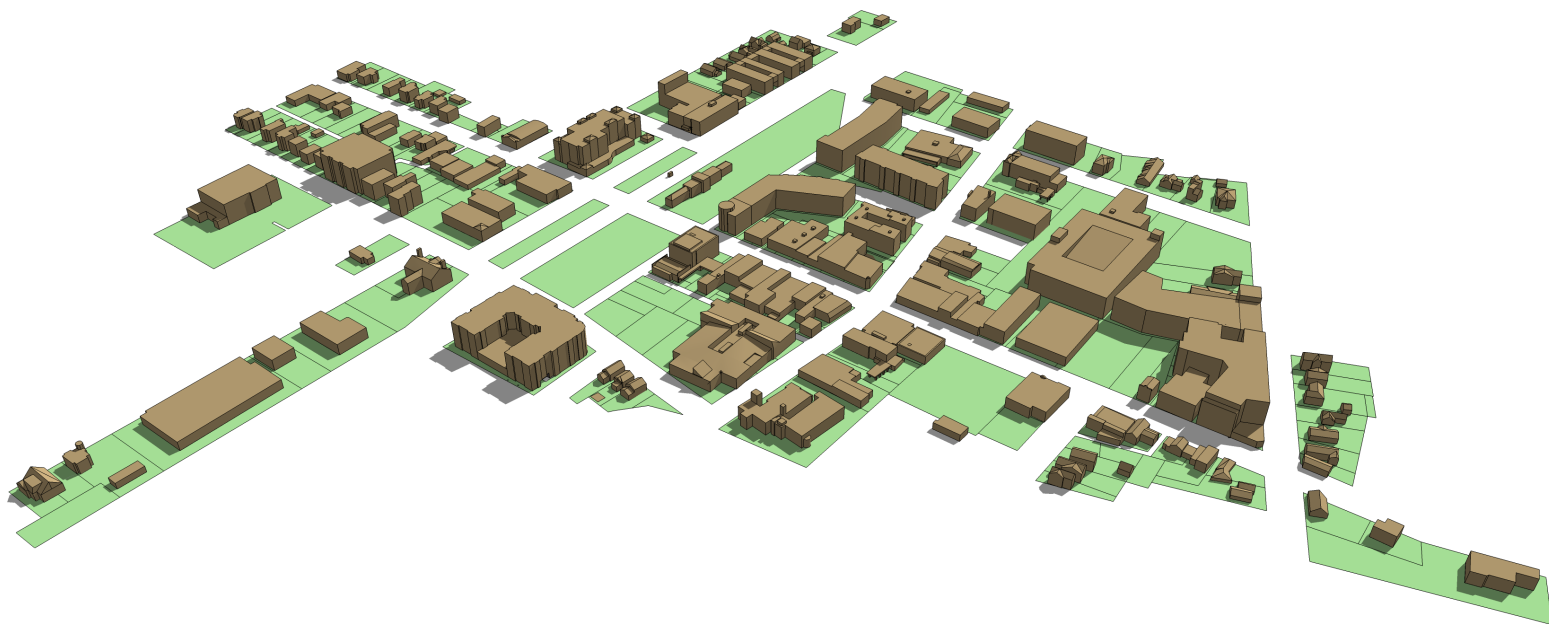
- Key Downtown Destination
- Gateway/Directory Signage
- Existing TIF District
- Existing Auto-Oriented Business
- Historic Structure or Site
- Centralized Garbage Collection
- North-side Parking Deficiency
- Pedestrian Underpass
- Park & Open Space
- Public Use
- Metra Commuter Station
- Village-Propose Bike Route
- Bike Parking



Land Use & Development

- Single Family Residential
- Multi-Family Residential
- Live/Work Units
- Public/Semi-Public
- Mixed-Use
- Park & Open Space
- Parking Garage

Existing Bulk Model



Key Focus Areas

Downtown Catalyst Sites**B1** AT&T Switching Station Parking Lot

This parking lot next to the AT&T Switching Station holds redevelopment potential. This site is ideally located for residential or for parking on the north side of the tracks to serve commuters and businesses. The parking lot is underutilized during the middle of the workday. The Village should explore how much of the lot is used by AT&T and see if it would be feasible to acquire.

B2 Main & Warren

This site presents an opportunity to redevelop an auto-oriented strip center and a one-story building into a multi-story, mixed-use development. Complementing this development, this site could accommodate a parking structure catering to the Downtown employees, shoppers and commuters along Forest Avenue.

B3 Washington & Warren

This area provides an important transition from residential areas to the north to the Downtown. Portion of this site designated 'Downtown Edge' may be taller and denser while those designated 'Downtown Transition' may contain less bulk. This site provides an opportunity to provide additional multi-family, office, mixed-use or parking.

B4 Main & Burlington

This site could be redeveloped as a mixed-use TOD development with residential above retail. A mixed-use building could provide a public plaza to preserve views and provide open space. A mixed-use development could provide retail opportunities along Main Street and Burlington Avenue and assist in connecting Main Street and Forest Avenue.

B5 Library Parking Lot

This site presents a unique opportunity for a mixed-use TOD development with an attached parking structure. Any building located on this site should be oriented towards Burlington Avenue so that it connects Main Street to Forest Avenue. The development would need to include structured parking to potentially serve the needs of many users, including building occupants, library patrons, commuter parking and downtown retail parking. The existing drive through on Main Street should be eliminated and redeveloped so that there is not a break in the Main Street streetwall.

B6 Post Office Operations

The post office provides an important civic function and a vital traffic-generator for the Downtown. Consideration should be given to splitting the retail and service functions from the delivery operations in order to minimize truck traffic Downtown. Removing the truck operations/parking would also create a potential redevelopment site on the west side of the post office site. The retail function of the post office should remain Downtown.

B7 Curtiss & Washington

Parcels on the northeast corner of Curtiss and Washington Streets could be redeveloped individually or assembled to create a 1.5-acre redevelopment site. Proximity to the train station makes this site an ideal location for a mixed-use TOD with residential above ground-floor retail.

B8 Curtiss Street

This site includes a bank drive-through lane and a one-story commercial building. This site has the potential to be redeveloped into a mixed-use TOD development that fronts onto Curtiss Street. A redevelopment here should respect the architectural features and bulk of the bank building at the southwest corner of Curtiss and Main Streets.

B9 Masonic Temple Parking Lot

The parking lot west of the Masonic Temple has development potential to add more housing units and commercial space in the downtown. Currently, the lot is underutilized and has a drive-thru structure that is no longer in use. Any building constructed on this site should be set back as to not disrupt the view of the Masonic Temple, which may have historic significance within the Downtown and should remain.

B10 Grove & Main

An underutilized, one-story building and the neighboring surface parking lot provides a redevelopment opportunity for a stronger relationship to the historic building pattern of Downtown. A multi-story building oriented towards Main Street maintains the streetwall, provides a sense of enclosure, and provides a terminating vista for Grove Street. Parking could be provided in the rear of the building where access presently exists, with a pedestrian arcade or alley providing access to Main Street. A building with a high-quality of architecture would provide a terminating vista for Grove Street.

B11 Maple & Washington

This area is an important transition from Downtown to the adjacent residential areas to the south and east. This site provides an opportunity for additional multi-family residential that is sensitive to the adjacent height and massing of nearby buildings.

B12 Maple & Main

This intersection is the southern gateway into the Downtown. Except for the historic building on the southwest corner, the condition, setback and/or orientation of the buildings surrounding this intersection do not contribute to creating a true gateway. Preservation of the blacksmith shop, which may have historic significance, should be a component of any proposed redevelopment at the southwest corner of this intersection. Redevelopment around the blacksmith shop should respect the massing of the blacksmith shop.



Recommendations

 Catalyst Redevelopment Opportunity

Key Focus Areas

Downtown Functional Subareas

Downtown is often referred to, and treated as, a single place. While it is true that Downtown is a unique destination in the community, it is actually comprised of several distinct areas, with different form, uses, conditions, characteristics and potentials. The Downtown Subarea Plan addresses the specific needs of each "Functional Subarea" area and establishes recommendations for the improvement and enhancement of each area in the future, including appropriate uses and intensities.

Downtown Core

The Downtown Core is a place that attracts people to gather as pedestrians. Placemaking should be the essential attribute and primary objective of planning for the Core.

In general, the concept of zoning by use should be subordinated by the objective of maintaining the built form of the Core. Maintaining a sense of place should be given priority over the importance of individual buildings and uses.

Land Uses

Residential. Residential uses should not be encouraged in the Downtown Core, however, residential uses should be restricted to upper stories of mixed-use buildings.

Retail. Retail uses should be promoted within all areas of the Downtown Core.

Entertainment. Entertainment uses, including restaurants, bars, and theatres and any other should be promoted within all areas of the Downtown Core. Opportunities for al fresco dining, including sidewalk seating, should be promoted throughout.

Office. Office uses should be largely restricted to the greatest extent feasible to upper stories of mixed-use buildings unless such office uses generate foot traffic and streetscape vitality.

Service. Commercial service uses that generate frequent customers and contribute to the energy and activity in Downtown and on the sidewalk should be integrated into the mix of ground floor uses in the Downtown Core.

Public Uses. Public uses that generate visitors and activity should be considered appropriate within the Downtown Core. Public uses consisting of primarily office functions should be treated like office land uses.

Built Form

The built form of the Downtown Core should support and facilitate the function of the Core, which is to establish and maintain a place that serves as the social and civic core of the community. The built form of the Core should foster a walkable environment that attracts and encourages people to gather, walk and mingle. A

continuous streetwalls and zero foot setbacks encourage fine granularity by increasing the density of doorways along a given segment of streetscape. However, alcoves and overhangs do allow for outdoor seating and places for pedestrians to linger in poor weather.

Building heights should not exceed three stories and be respectful of the existing structures. The Downtown Pattern Book should be used as a guide in designing buildings in the Core. Larger parking lots and decks should be located outside the Core with limited on-site parking accessed by alleyways in the rear of buildings. Eliminating existing drive-thru uses within the Core is a high priority objective of this Plan. Application of planning concepts such as triangulation should also be employed to facilitate placemaking.

Downtown Edge

While the Core provides a place that has served as the social and civic heart of the community for more than 150 years, the Downtown Edge should be understood as a combination of (1) transit-oriented development (TOD) - a mixed-use residential and commercial area that seeks to leverage access to public transportation, and (2) an area of greater residential density to facilitate a vibrant and energetic downtown while providing economic sustainability to the Core. As with the Core, land use regulation and the built form should be subordinate to the purpose and intended function of the area.

Land Uses

Residential. Residential development, generally of greater density than elsewhere in the Village, should be the predominant desired land use within the Downtown Edge. Within a particular development, a variety of mixed uses should be allowed if appropriate to facilitate the economic viability of such developments. Otherwise, uses that promote pedestrian traffic and / or triangulation with desired Core businesses should be encouraged to locate in the Core.

Retail. Retail uses should first be directed to the Downtown Core before developing within the Edge. Retail uses in the Edge should be focused near the Downtown Core. While retail, entertainment and restaurant uses should be most strongly encouraged in the Core, within particular Edge developments, a variety of mixed uses should be allowed if appropriate to facilitate the economic viability of such developments.

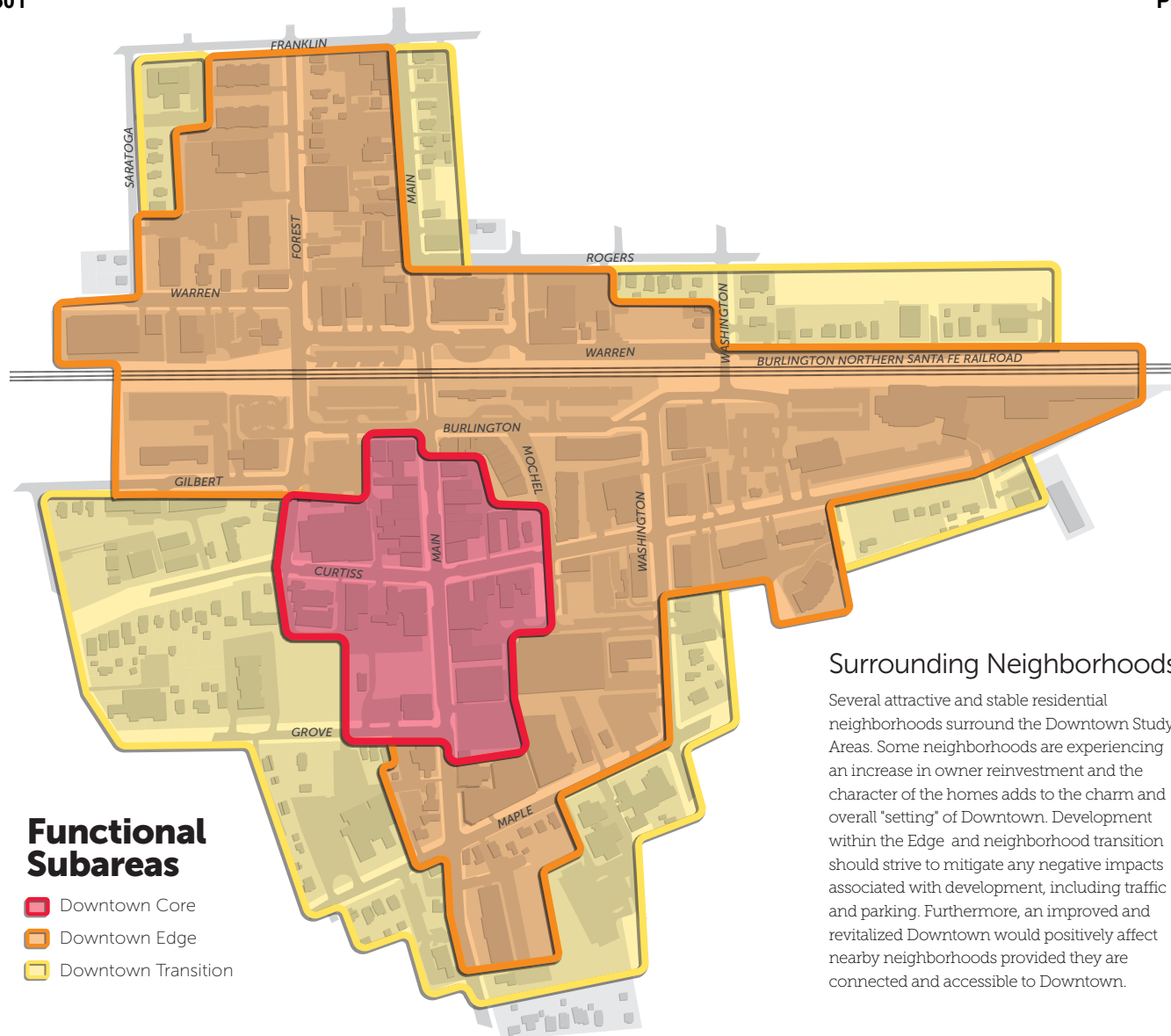
Service. Commercial service uses that generate frequent customers and contribute to the energy and activity in Downtown and on the sidewalk should be directed to the Downtown Core before developing within the Edge. Other types of commercial service uses should be considered appropriate in the Edge.

Office. Office uses should be encouraged as a component of mixed-use buildings within the Edge; however like residential, office uses on the ground floor should be permitted.

Public Uses. Public uses should be considered appropriate within the Edge.

Built Form

The Downtown Edge is uniquely located adjacent to the Downtown Core with denser, commercial development and the Downtown Transition and residential neighborhoods with residential characteristics. The built form of the Downtown Edge should be generally consistent with transit-oriented development. As such, the location of edge properties should play a role in the built form. For those Edge properties nearer the core, buildings should exhibit core characteristics, such as larger buildings at, or near the sidewalk and front property lines and a continuous streetwall. For those Edge properties near the Downtown Transition and surrounding residential neighborhoods, buildings should exhibit these characteristics. Buildings may be larger but should include front and side setbacks to create open green space around the buildings. Parking in these areas should be provided on-street or in the rear of the buildings accessed by entry drives or side streets. Surface parking lots should follow the Village's landscaping and screening requirements.



Functional Subareas

- Downtown Core
- Downtown Edge
- Downtown Transition

Surrounding Neighborhoods

Several attractive and stable residential neighborhoods surround the Downtown Study Areas. Some neighborhoods are experiencing an increase in owner reinvestment and the character of the homes adds to the charm and overall "setting" of Downtown. Development within the Edge and neighborhood transition should strive to mitigate any negative impacts associated with development, including traffic and parking. Furthermore, an improved and revitalized Downtown would positively affect nearby neighborhoods provided they are connected and accessible to Downtown.

Downtown Transition

The areas outside of the Downtown Core and Edge but within the Downtown Study Area comprise the Downtown Transition area. This area plays an important role in helping transition between more intensive uses in the Downtown Core and Downtown Edge into the neighborhoods that surround Downtown.

Land Uses

- Residential.** All types of residential uses are appropriate in the Downtown Transition Area.
- Retail.** Retail uses should not be encouraged in the Downtown Transition area. Retail uses should be directed to the Downtown Core and Edge.
- Service.** Commercial service uses that generate frequent customers should not be encouraged in the Downtown Transition area. Other types of commercial service uses with primarily an office function may be appropriate, but should be considered on a case-by-case basis.
- Office.** Low intensity and professional office uses, including lawyers, accountants, doctors, should be considered appropriate in the Downtown Transition area.
- Public Uses.** Public uses should be considered appropriate within the Downtown Transition area.

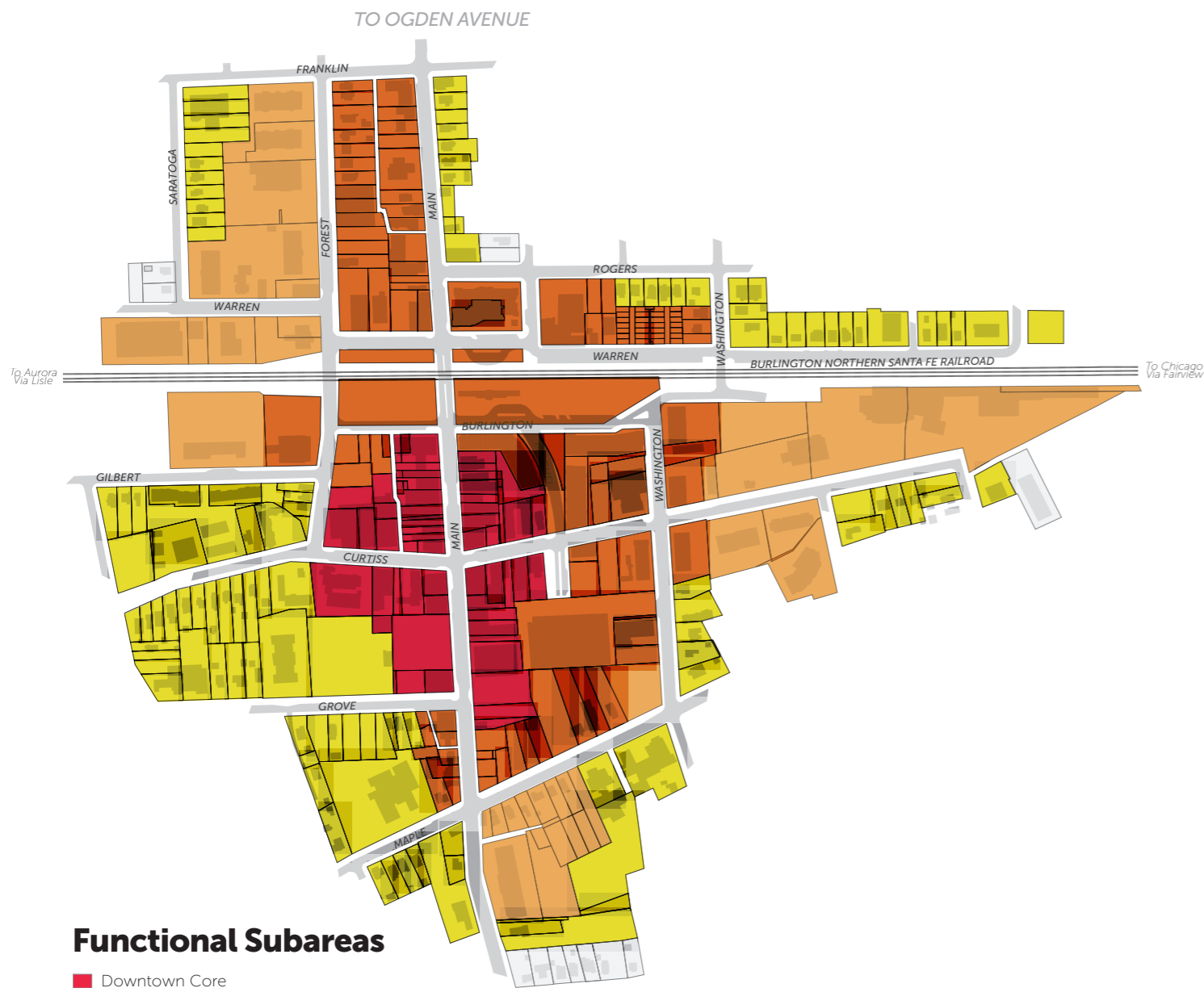
Built Form

The built form of the Downtown Transition area should buffer nearby residential areas from taller and denser developments and should consist of buildings that are smaller than what is found in the Core and Edge subareas. These buildings should not have a street wall and should be setback from the front lot line in a manner that creates a front yard with some open space. The building should also be setback from side property lines to create a side yard. This subarea should be denser compared to the surrounding neighborhoods outside of the downtown, but should be respectful of the height of surrounding neighborhoods.

Key Focus Areas

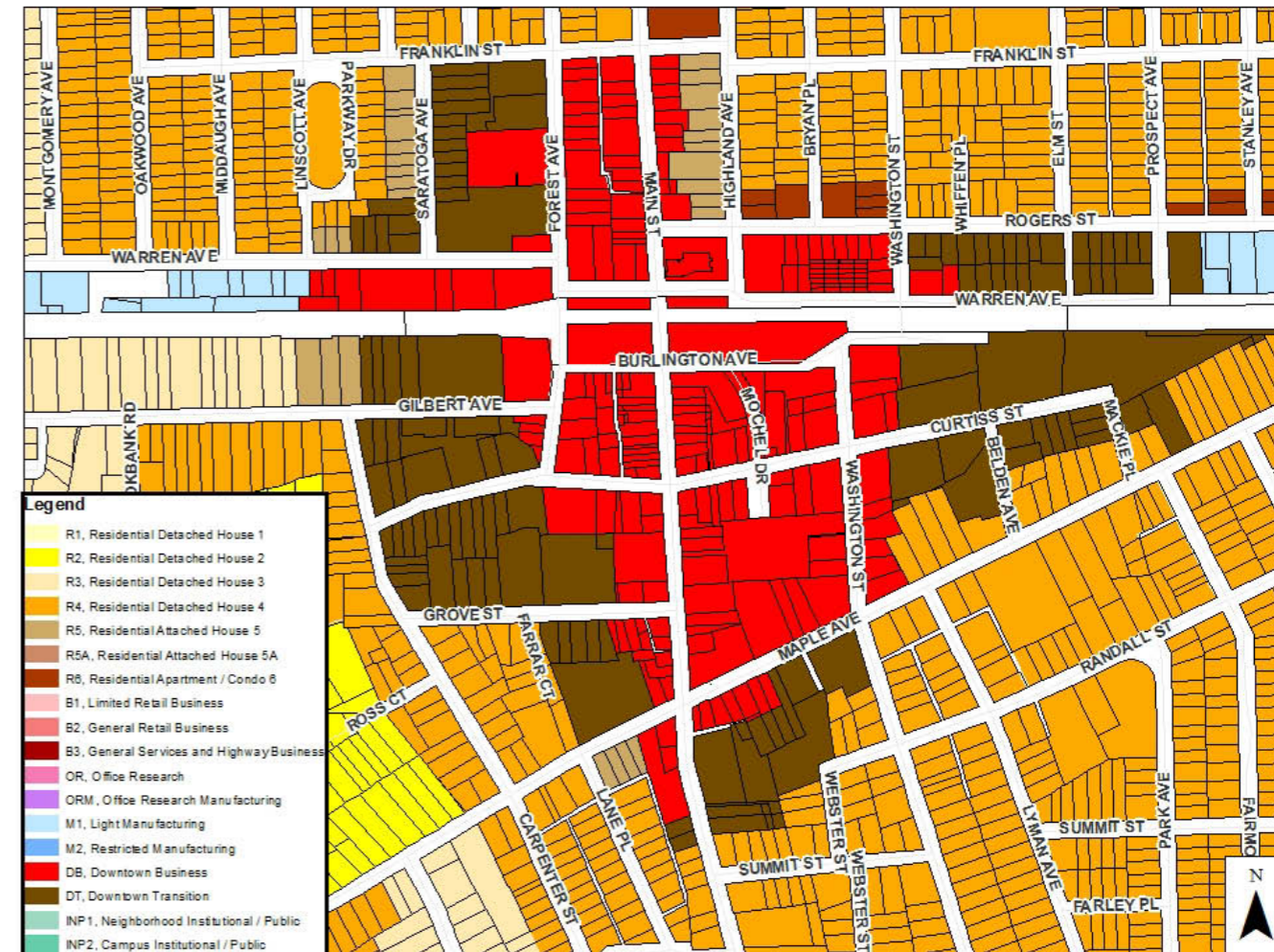
Downtown Functional Subareas

Downtown is often referred to, and treated as, a single place. While it is true that Downtown is a unique destination in the community, it is actually comprised of several distinct areas, with different form, uses, conditions, characteristics and potentials. Therefore, to better accommodate and encourage new appropriate development, new zoning districts and standards are being proposed to better align with the Downtown Plan. The new zoning addresses the specific needs of each "Functional Subarea" and establishes recommendations for the improvement and enhancement of each area in the future, including appropriate uses and intensities.



Functional Subareas

- Downtown Core
- Downtown Edge 1
- Downtown Edge 2
- Downtown Transition



Downtown Current Zoning

Downtown Edge District (DE-1)

Downtown Edge District (DE-1)

Desired/Planned Character:

Building Height: 1-6 stories

Setbacks: variable – proportional to height and proximity to neighborhood and downtown core

Uses: wide ranging – mixed-use, residential, commercial, office, institutional, entertainment, civic, etc.

Building Height:

- Current zoning allows maximum height of 60-70 feet depending on parcel location
- Proposed maximum 72 feet/6-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, but a few sites require a 10' street setback
- Existing street setbacks vary within the district, from 0'-20+'
- Proposed 0' street setback throughout district

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater
- Proposed side yard setback for properties adjacent to the DC District and those properties along Main Street shall be 0'

Rear Setback:

- Proposed rear setback is 10' throughout the district
- Special rear setbacks standards apply to property abutting an R zoned lot (see table).

Uses:

- Commercial, retail, office, entertainment, service, restaurant, civic, and residential (apartment, condominium, attached single-family)

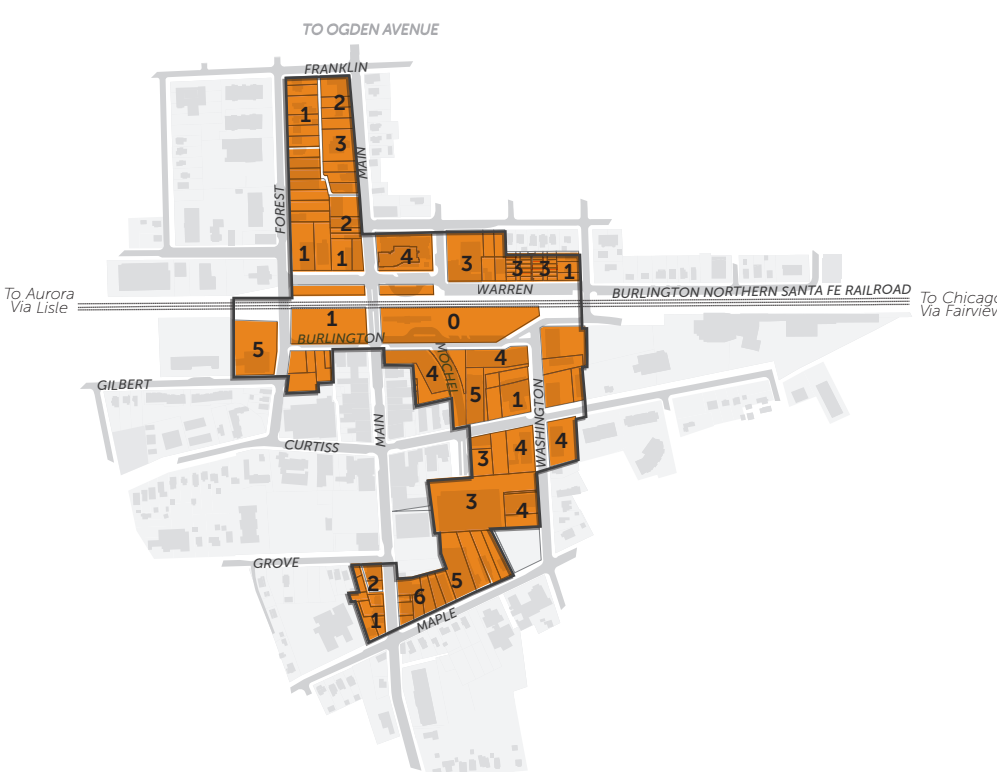
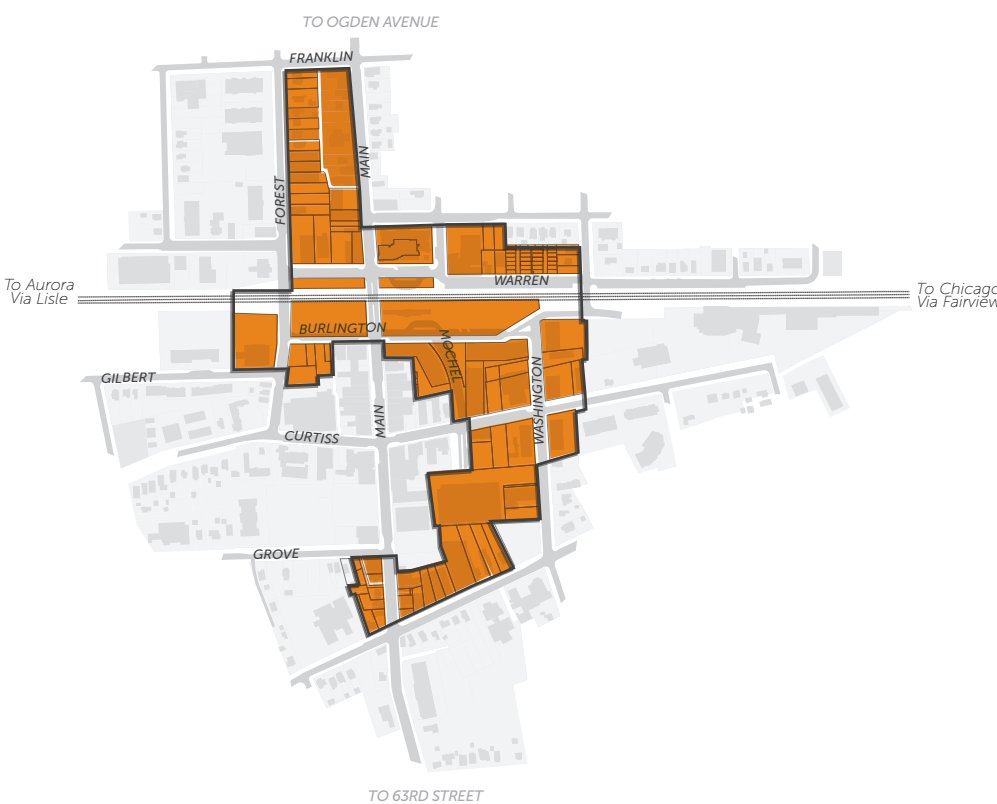
Existing Front Setback Requirement

■ 0ft Front Setback

Existing Building Height Maximum

■ 70ft Max Building Height

X Approximate Stories of Existing Buildings



Downtown Edge District (DE-2)

Downtown Edge District (DE-2)

Desired/Planned Character:

Building Height: 1-5 stories

Setbacks: variable – proportional to height and proximity to neighborhood and downtown core

Uses: wide ranging – mixed-use, residential, commercial, office, institutional, entertainment, civic, etc.

Building Height:

- Current zoning allows maximum height of 60-70 feet depending on parcel location
- Proposed maximum is 60 feet/5-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, but a few sites require a 10' street setback
- Existing street setbacks vary within the district, from 0'-20+'
- Proposed 10' street setback throughout district

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater

Rear Setback:

- Proposed rear setback is 10' throughout the district
- Special rear setback standards apply to property abutting an R zoned lot (see table)

Uses:

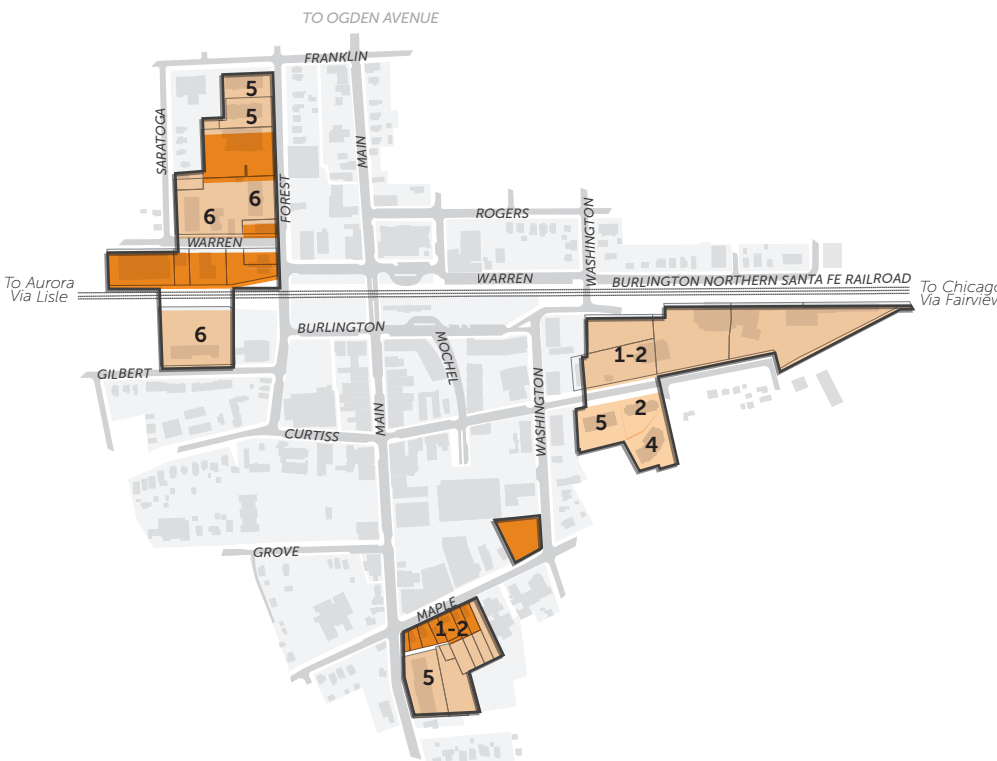
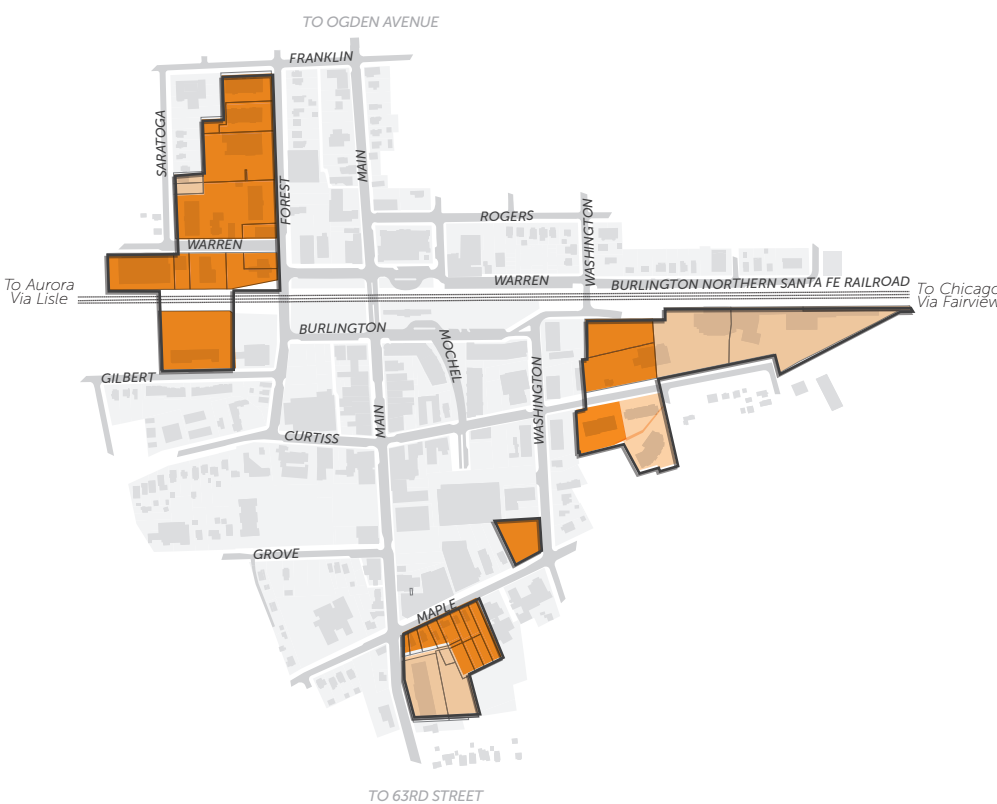
- Commercial, retail, office, entertainment, service, restaurant, civic, and residential (apartment, condominium, attached single-family)

Existing Front Setback Requirement

- 0ft Front Setback
- 10ft Front Setback

Existing Building Height Maximum

- 70ft Max Building Height
- 60ft Max Building Height
- ✕ Approximate Stories of Existing Buildings



Downtown Transition District (DT)

Downtown Transition District (DT)

Desired/Planned Character:

Building Height: 1-3 stories (existing buildings range from 1-6 stories in height)

Setbacks: Buildings with landscaped setbacks reflective of residential districts.

Uses: residential, office, service, home occupations, institutional (residential scale and compatibility)

Building Height:

Current zoning allows maximum height of 70' or 60' for most parcels, and 35' for a few parcels.

Proposed maximum is 36 feet/3-stories (approx. 12' per floor)

Setbacks:

Street Setback:

- Majority of parcels have no street setback required, or 10' street setback required. A few have 25' setback required.
- Proposed 20' street setback throughout district, except for properties fronting Main Street, which will have a 10' required street setback.

Side Setback:

- The majority of the existing properties have 0'-5' side yard setback.
- Proposed side yard setbacks shall be a minimum of 5' or 10% of lot width, whichever is greater.
- Special side setbacks standards apply to property abutting an R zoned lot (see table).

Rear Setback:

- Proposed rear setback is 20' throughout the district, except for properties fronting Main Street, which will have a 10' required rear setback.
- Special rear setback standards apply to property abutting an R zoned lot (see table).

Uses:

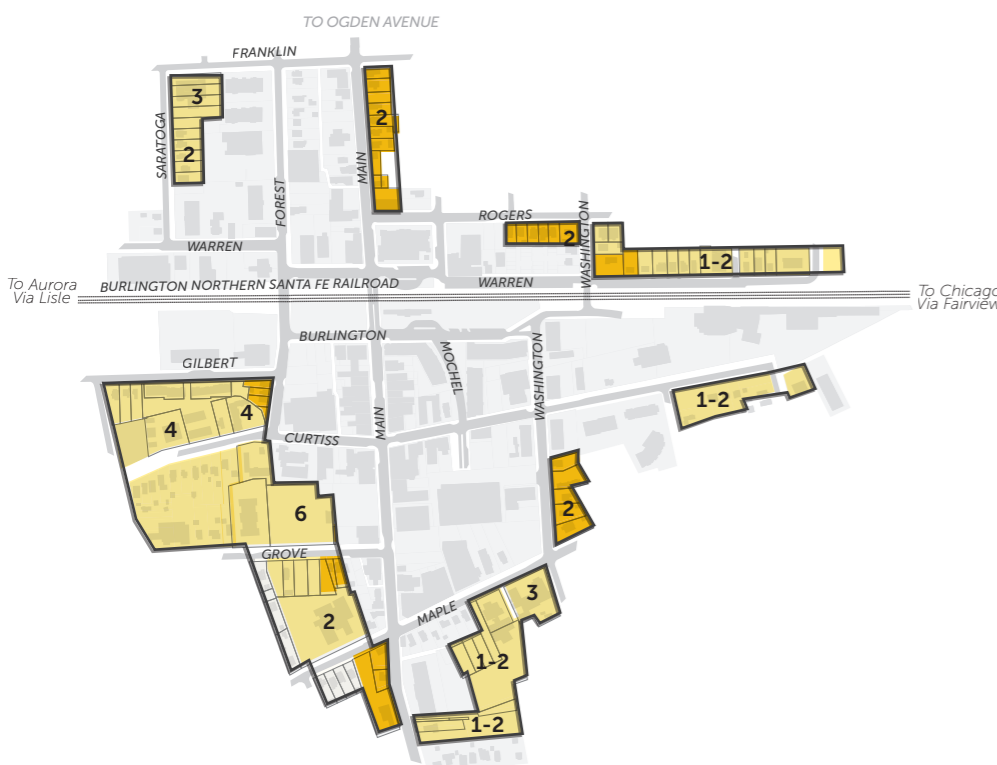
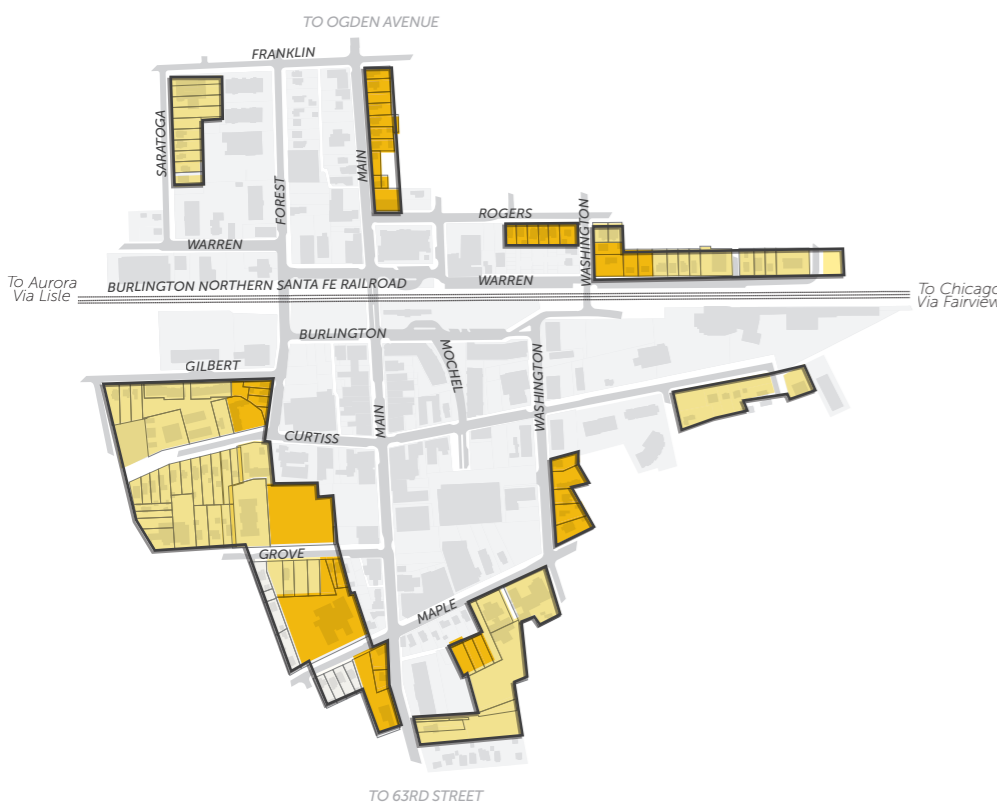
- Single-family, multi-family, office, service, home occupations, institutional (residential scale and compatibility), lodging (bed and breakfast only)

Existing Front Setback Requirement

- 0ft Front Setback
- 10ft Front Setback
- 25ft Front Setback

Existing Building Height Maximum

- 70ft Max Building Height
- 60ft Max Building Height
- 35ft Max Building Height
- ✕ Approximate Stories of Existing Buildings



Downtown Zoning Comparison Table

Regulation	DB	DT	DC	DE-1	DE-2	DT
Building Height						
Maximum	70'	60'[4]	40'/3-story	72'/6-story	60'/5-story	36'/3-story
Minimum	32'	-	24'/2-story	24'/2-story	-	-
Parking (per residential unit)	1.4	2	1.4	1.4	1.4	2
Minimum Lot Area per D.U. (square feet)	800	4,000[1]	800	3,000[1]	4,000[1]	4,000[1]
Maximum Floor Area Ratio						
Apartment/Condo	-	2.5	-	2.5	2.5	2.5
Non-residential	-	2.5	-	2.5	2.5	2.5
Minimum Lot Area (square feet)						
Detached House	-	7,500	-	-	7,500	7,500
Attached House (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Two -Unit House (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Apartment/Condo (applies to development not individual units)	-	10,500	-	10,500	10,500	10,500
Other Non-residential Building Uses	-	7,500	-	7,500	7,500	7,500
Minimum Lot Width (feet)	-	50'	-	50'	50'	50'
Minimum Building Setback (feet)						
Street	0'	10'[2]	0'	0'	10'	20'[x]
Side (interior)	0'	5'	0'	5'/10%	5'/10%	5'/10%
Rear	0'[3]	20'	0'	10'[3]	10'[3]	20'[3]
Building to Zone						
Minimum/maximum (feet)	0/10'	-	0/5'	0/40'	-	-
Minimum percent of building in primary street Building to Zone	80	-	80	80	-	-
Minimum percent of building in secondary street Build to Zone	30	-	30	30	-	-
Maximum Building Coverage	-	-	-	-	-	-
<i>(% of Lot, Principal + Accessory)</i>						
Uses						
See Downtown Uses table on page 8						

[1] Does not apply to detached houses or apartment/condo projects.

[2] No street setback required if street lot line abuts DC zoning districts or if front lot line is located on Main Street

[3] A rear setback is required when abutting the side or rear lot line of an R-zoned lot. When abutting the rear lot line of an R-zoned lot, the setback must be at least 20 feet in depth, plus one foot of additional setback for each foot of building height in excess of 20 feet. When abutting the side lot line of an R-zoned lot, the setback must be at least as deep as the side setback required on the abutting R-zoned lot, plus one foot of additional setback for each foot of building height in excess of 20 feet. See Figure 4-3.

[4] Detached houses, attached houses and two-unit houses subject to 35-foot maximum height.

[x] 10' minimum setback required for properties fronting Main Street

[xx] Only permitted as part of a mixed-use development; Residential uses not permitted on ground floor

Existing Districts: DB=Downtown Business; DT=Downtown Transition

Proposed Districts: DC=Downtown Core; DE-1=Downtown Edge 1; DE-2=Downtown Edge 2; DT=Downtown Transition

Downtown Design Guidelines

Intent:

The Design Guidelines for development within the four new Downtown zoning districts (DC, DE-1, DE-2, DT) are intended to ensure quality, compatible, attractive, and market viable development that fits the overall Downtown, as well as the character of each of the different zoning districts that comprise the greater Downtown area. The guidelines are not standards or formulas but rather principles and approaches which could be applied with understanding and sensitivity in context of the use of the site, nearby buildings and the streetscape of adjacent public rights-of-way.

The Design Guidelines are intended to promote an attractive and pedestrian-friendly Downtown. The guidelines will work cooperatively with zoning to establish a flexible and creative environment that seeks to accommodate a wide range of uses and development types.

The Design Guidelines are not intended to dictate architecture or building design, but rather guide development to ensure the desired Downtown character and sense of place while facilitating appropriate new development and exterior renovations.

The Design Guidelines shall apply to all new construction and exterior renovations of all non single-family residential development within the DC, DE-1, DE-2, and DT Districts.

When and How to Use the Design Guidelines:

The Design Guidelines should be used to guide and evaluate new construction and exterior renovations of all non-single family residential developments within the four Downtown zoning districts. The following outlines the process for use of the Design Guidelines on a proposed development.

1. The developer is provided with the Design Guidelines. The intent is for the developer to follow the Design Guidelines as the development plans and designs are created. Village staff can answer any questions the developer may have regarding the Design Guidelines.
2. A new construction development or exterior renovation is proposed. Staff will determine if the proposed development meets the zoning standards for the district in which it is located, and whether or not the proposed development complies with the Design Guidelines.
3. Staff will work with the applicant or developer to encourage compliance with the Design Guidelines, understanding that the guidelines are not standards or formulas but rather principles and approaches to be applied to design.
4. If Village staff determines that the proposed improvements comply with all zoning standards, then the development or exterior renovation can be approved 'by-right'. In this case, Village staff will work with the developer to encourage compliance with the Design Guidelines. If all zoning standards are met, the development or renovation can continue without full compliance with Design Guidelines.
5. If Village staff determines that a proposed development does NOT comply with zoning, then the proposed development will require approval as a Planned Unit Development or a variation(s) can be sought for the necessary zoning relief. A Planned Unit Development would require a public hearing with the Plan Commission and a variation would require a public hearing with the Zoning Board of Appeals. As part of either a Planned Unit Development or a variation request, the Design Guidelines could be used as a standard of review to help determine the overall appropriateness of the proposed development.

Components/Features:

The Design Guidelines will address a wide range of development and architectural components/features. Different guidelines will likely need to be developed for the different districts, as the type and intensity of development may vary wildly from one district to the next, as well as within each district.

Identified below are many of the likely components/features to be addressed in the Design Guidelines:

Building Base

- Windows - % opening
- Windows - transparency
- Knee Walls (12' - 30')
- Entry Features (articulation, elaboration, materials)
- Horizontal expression to establish ground level
- Materials - complement existing
- Materials - restore/repair existing
- Materials - discourage covering existing features and materials
- Materials - brick, manufactured stone, terra cotta, metal accents, metal panels, wood, hardiboard
- Materials - discourage EIFS, utility brick, CMU, vinyl or aluminum siding
- Materials - differentiate importance of building features

Base - Façade Elements

- Awnings
- Outdoor cafes
- Protruding light fixtures
- Landscaping
- Benches / Seating
- Balconies
- Projecting sign

Building Middle

- Windows in rhythm w/ base - % of window openings?
- Visual interest - sills, lintels, divided lights and style
- Replacement windows - fill entire historic window opening
- Fill in historic window - use different material

Middle - Façade

- Proportionate shapes and patterns
- Visually appealing w/ detailing, openings & material
- Corner buildings - articulated comers and elaborated to reflect importance of corner

Middle - Materials

- Complimentary
- Materials - brick, manufactured stone, terra cotta, metal accents, metal panels, wood, hardiboard
- Materials - discourage EIFS, utility brick, CMU, vinyl or aluminum siding
- Materials - differentiate importance of building features

Building Top

- Distinctive corner and cornices
- Screen mechanical equipment

Downtown Land Uses

Uses Category	DB	DT	DC	DE-1	DE-2	DT
Residentials						
Housinghold Living						
Detached house	-	P	-	-	-	P
Attached house	-	P	-	P	P	P
Two-Unit house	-	P	-	P	P	P
Apartment/condo	S	S	P[17]	P	P	P
Group Living <i>(except for the following uses)</i>	-	-	-	-	-	-
Group home, small (8-person max. occupancy)	-	-	-	-	-	-
Group home, large (9 or more occupants)	-	-	-	-	-	-
Nursing home	-	-	-	-	-	-
Sheltered Care	-	-	-	-	-	-
Public, Civic & Institutional						
Aircraft Landing Area	-	-	-	-	-	-
Cemetery	S[6]	-	S[6]	-	-	-
College or University	S	S	S	S	S	S
Community Center	S	S	S	S	S	S
Fraternal Organization	S	-	S	S	S	-
Governmental Facility	P	P	P	P	P	P
Hospital	-	-	-	-	-	-
Library	S	S	S	S	S	S
Museum or Cultural Facility	S	S	S	S	S	S
Natural Resource Preservation	P	P	P	P	P	P
Parks and Recreation	-	-	-	-	-	-
Religious Assembly	S[6]	S	S[6]	S	S	S
Safety Service	S	S	S	S	S	S
School	-	S	-	-	S	S
Utilities and Public Service Facility						
Minor	P	P	P	P	P	P
Major		S	S	S	S	S
Wireless Telecommunications						
Freestanding tower	S	S	S	S	S	S
Building or tower-mounted antenna	P	P	P	P	P	P
Commercial						
Adult Entertainment Establishment	-	-	-	-	-	-
Animal Service						
Boarding or shelter	-	-	-	-	-	-
Grooming	P	-	P	P	P	-
Veterinary Care	S	-	S	S	S	-
Assembly and Entertainment <i>(except for the following)</i>						
Auditorium	S	-	S	S	S	-
Cinema	S	-	S	S	S	-
Theater	S	-	S	S	S	-
Commercial Service						
Building service	S	-	-	S	S	-
Business Support Services	P	-	P	P	P	-
Consumer maintenance and repair	P	-	P	P	P	-
Personal improvement service	P	P[13]	P	P	P	P[13]
Fortune telling or psychic service	-	-	-	-	-	-
Massage therapy	-	-	-	-	-	-
Research service	S	-	S	S	S	-
Day Care						
Day care home	-	P	-	-	-	P
Day care center	S	S	-	S	S	S
Eating and Drinking Establishment						
Restaurant	P	-	P	P	P	-
Wine boutique	P	-	P	P	P	-

Uses Category	DB	DT	DC	DE-1	DE-2	DT
Financial Service	P	-	P	P	P	-
Funeral or Mortuary Service	-	-	-	-	-	-
Lodging	S	-	-	S	S	-
Bed and Breakfast			-	-	-	S
Office						
Business and professional office	P	S	P	P	P	S
Medical, dental and health practitioner	P/S[8]	S	P/S[8]	P	P	S
Parking, Non-Accessory	S	S	S	S	S	S
Retail Sales						
Convenience goods	P/S[12]	-	P/S[12]	P	P	-
Consumer shopping goods	P/S[12]	P[14]	P/S[12]	P	P	-
Guns and firearm supplies	-	-	-	-	-	-
Building supplies and equipment	P/S[12]	-	P/S[12]	P	P	-
Self-service Storage Facility	-	-	-	-	-	-
Studio, Instructional or Service	P	S	P	P	P	S
Trade School	S	-	S	S	S	-
Vehicle Sales and Service						
Commercial vehicle repair and maintenance	-	-	-	-	-	-
Commercial vehicle sales and rentals	-	-	-	-	-	-
Fueling station	S	-	-	-	-	-
Personal vehicle repair and maintenance	-	-	-	-	-	-
Personal vehicle sales and rentals	S	-	-	-	-	-
Vehicle body and paint finishing shop	-	-	-	-	-	-
Automobile dealership off-site vehicle storage	-	-	-	-	-	-
Wholesale, Distribution & Storage						
Equipment and Materials Storage, Outdoor	-	-	-	-	-	-
Trucking and Transportation Terminals	-	-	-	-	-	-
Warehouse	-	-	-	-	-	-
Wholesale Sales and Distribution	-	-	-	-	-	-
Industrial						
Artisan Industrial	-	-	-	-	-	-
Limited Industrial	-	-	-	-	-	-
General Industrial	-	-	-	-	-	-
Intensive Industrial	-	-	-	-	-	-
Junk or Salvage Yard	-	-	-	-	-	-
Recycling						
Recyclable Material Drop-off Facility	-	-	-	-	-	-
Recyclable Material Processing	-	-	-	-	-	-
Agriculture						
Animal Agriculture <i>(except as allowed under Chapter 5 of the Downers Grove Municipal Code)</i>	-	-	-	-	-	-
Crop Agriculture	-	-	-	-	-	-
Community Garden	P	P	P	P	P	P
Other						
Drive-in or Drive-Through Facility	-	S	-	-	-	-
Medical Cannabis Cultivation Center	-	-	-	-	-	-
Medical Cannabis Dispensing Organization	-	-	-	-	-	-

[1] Requires minimum lot area of 40 acres. Maximum 25% building coverage.

[2] Requires minimum lot area of 10 acres.

[3] Requires minimum lot area of 25 acres. Maximum 25% building coverage.

[4] Must be within 150 feet of a B district.

[5] Requires special use approval if above one dwelling unit per 4,000 square feet of lot area.

[6] Special Use only if use was in existence on or prior to June 7, 2005.

[7] Requires minimum seating capacity of 125 persons.

[8] Permitted as of right up to 3,000 sq. ft. (gross floor area); larger requires special use approval.

[9] Must be in a completely enclosed building.

[10] Maximum 10,000 sq. ft. (gross floor area).

[11] Permitted only if ancillary to the following principal uses: sporting goods stores, uniform supply stores and public safety equipment stores.

[12] Consignment stores and martial arts studios permitted as of right up to 3,000 square feet (gross floor area); larger requires special use approval. Other uses permitted as of right up to 15,000 sq. ft. (gross floor area); larger requires special use approval.

[13] Barber shops, beauty shops and salons only; must be on ground floor and may not exceed 2,500 square feet floor area.

[14] Art galleries and studios only; must be on ground floor and may not exceed 2,500 square feet floor area.

[15] Drive-through banks only.

[16] See Section 6.180.

[17] 2nd floor or above only as part of mixed-use development



**Design Guidelines
for the
Downers Grove
Downtown Business District**

January 20, 2009

SECTION 1 – INTRODUCTION

The Village has developed the following design guidelines for the downtown business district. The guidelines have been developed using the August 2008 Downers Grove Downtown Pattern Book prepared by Houseal Lavigne Associates & Topografis and a visual preference survey held in October 2008 with Village staff, Downtown Management and other interested downtown parties.

SECTION 2 – HOW TO USE THE DESIGN GUIDELINES

The design guidelines have been developed to assist in creating a vibrant and diverse downtown and should serve as a guide for downtown development. The guidelines are divided into five separate sections, site design, building design, building base, building middle, and building top. Each section describes elements which support good design and provide visual references which identify both encouraged and discouraged elements of each guideline. The Village encourages that all development in the downtown incorporate items from each section.



Fig. 1: Building areas

1. Building Base
2. Building Middle
3. Building Top

SECTION 3 - SITE DESIGN

The overall building design is important to create a sense of place, enclosure and activity. The following guidelines are offered:

- ✓ Building massing, height and lot coverage should be proportionate to adjacent buildings. Appropriate massing will assist in creating a sense of enclosure.

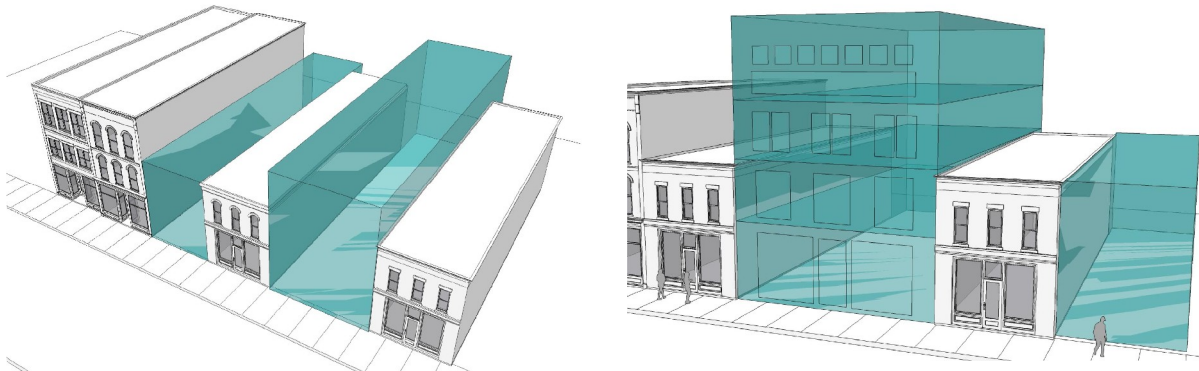


Fig. 1: Appropriate Building Massing

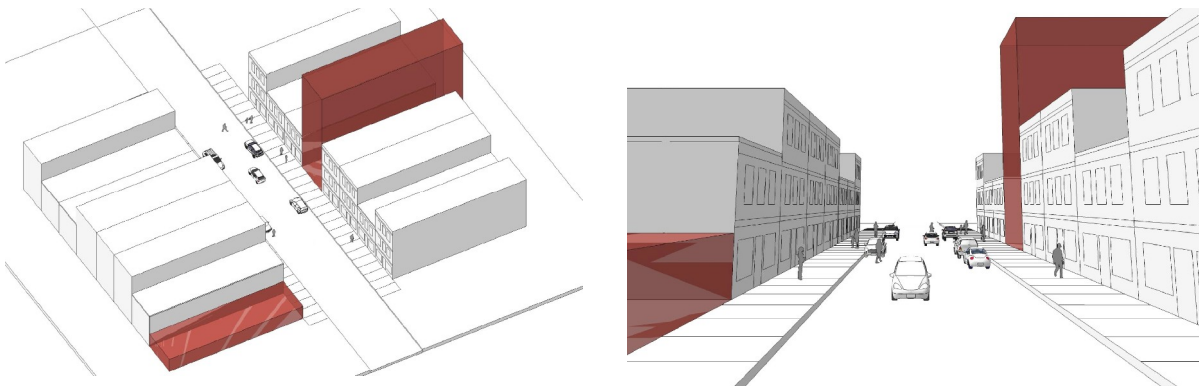


Fig. 2: Inappropriate Building Massing

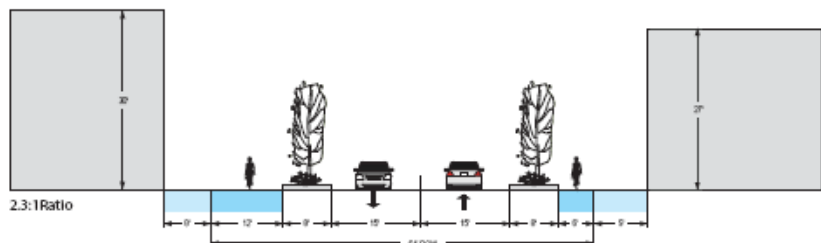


Fig. 3: Sense of enclosure

- ✓ Primary facades should be located near the property line. The urban quality of the street and the pedestrian experience are enhanced by buildings located at the street edge.
- ✓ Buildings should extend and establish a continual street wall.
- ✓ Parking lots are discouraged as they eliminate the street wall. Plazas and outdoor cafes are encouraged as they continue street walls.



Fig. 4: Example of strong street wall

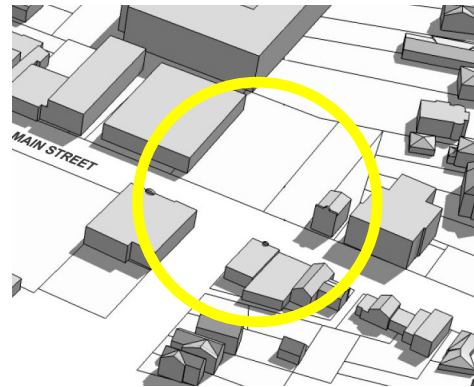


Fig. 5: Major gap in street wall should be avoided.

SECTION 4 - BUILDING DESIGN – GENERAL

In general, individual buildings should have a distinctive and aesthetically pleasing architecture. Although the Village does not have a distinct style in the Downtown, individual buildings should have a single architectural style.

Although the Village is not seeking to exclude materials, buildings should be constructed of high-quality materials. A maximum of three materials is strongly encouraged. A variety of finishes within the same material is acceptable.



Fig. 6: Examples of appropriate use of materials.

SECTION 5 - BUILDING BASE

Attractive storefronts can draw the attention of window shoppers, boost economic activities, enhance the image of the business and assist in marketing the goods and services of the business. To create attractive storefronts, certain design features can be used to create good storefronts:

Windows

- ✓ Windows should be designed to encourage retail uses. Generally, a majority of the first floor should be windows because they enliven streets and provide interest and activity at the street.
- ✓ Knee walls are encouraged to provide a strong base. Knee walls should be between 12 and 30 inches tall.
- ✓ Windows should be transparent, not opaque.



Fig. 7: Encouraged: knee walls and windows which make up a majority of the storefront



Fig. 8: Encouraged: knee walls and windows which make up a majority of the storefront



Fig. 9: Storefronts without a knee wall or base should avoided.



Fig. 10: Opaque windows and windows which do not make up the majority of the storefront are discouraged and should be avoided.

Entry Features

- ✓ Entries should be prominent features of the base. Entries should be different from the standard building bay through articulation, elaboration and materials.



Fig. 11: Encouraged: articulated entry through corner piers and signage



Fig. 12: Encouraged: articulated design and material details



Fig. 13: Entries should be prominent features. Entries with lack of detail and elements that are out of proportion with the building should be avoided.



Fig. 14: Entries without detail and definition are unattractive and do not invite customers into the establishment. If entries are setback, extra care should be given to the detailing as in Figure 11.

Building Materials

- ✓ Base materials should be consistent and new materials should compliment existing materials.
- ✓ Repair and restoration of original features and materials such as brick and stucco, etc., is encouraged. Covering original features and materials is discouraged.

- ✓ A horizontal expression should establish the ground level of the building from the rest of the building. The expression should compliment adjacent buildings and reinforce the street as a pedestrian friendly space.
- ✓ Building materials such as brick, stone, manufactured stone, terra cotta accents, metal accents and wood are encouraged as they provide visual interest and assist in creating a pedestrian friendly corridor.
- ✓ Building materials such as utility brick, concrete masonry units, and Exterior Insulating Finishing Systems (EIFS) are discouraged as they are perceived as cold and uninviting while do not create a pedestrian friendly scale.
- ✓ Materials should be used to differentiate between the importance of building features, and provide visual separations between material functions.



Fig. 15: Encouraged: brick and stone with horizontal expression of first floor



Fig. 16: Encouraged: brick, stone, and textured stone, quoins, projecting lights



Fig. 17: Materials such as EIFS can be used effectively as an accent. However, it is discouraged as a primary material

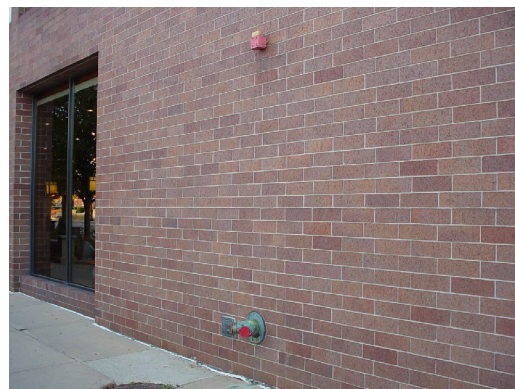


Fig. 18: Avoid blank facades with little detail and inappropriate materials such as utility brick

Façade Elements

Features which extend out from the building façade can contribute to the character, scale and visual interest of the street. These elements add value and are encouraged:

- ✓ Awnings create visual interest, shield pedestrians from weather and should be compatible in material and style with adjacent properties. Awning can be used to advertise goods or provide visual cues to the location of an entrance.



Fig. 19: Encouraged: awnings



Fig. 20: Avoid using materials that do not compliment the building and surrounding buildings. Awnings should be in scale with the other façade elements and with neighboring buildings.

- ✓ Outdoor cafes can create active streets. Cafes should be clearly defined and provide adequate space for the existing sidewalk to function.
- ✓ Protruding light fixtures also add visual interest while highlighting building details.
- ✓ Landscaping can create pedestrian friendly sidewalks by separating vehicles from pedestrians.
- ✓ Benches adjacent to landscape areas assist in creating pedestrian friendly areas.

- ✓ Balconies within the middle section of the building assist in providing the desired solid and void which are important to the middle sections of buildings. Additionally, balconies add visual interest to the street wall.
- ✓ Projecting signs create visual interest and can assist businesses in advertising.



Fig. 21: Encouraged: appropriately scaled and located signage



Fig. 22: Encouraged: awnings and flower box



Fig. 23: Encouraged: outdoor cafe



Fig. 24: Encouraged: landscaping and benches



Fig. 25: Encouraged: projecting light

SECTION 6 - BUILDING MIDDLE

The middle section of a building should be designed to tie the building base to the roof while creating visual interest. The middle of a building should be a series of solid and voids. Design features which compliment the base and roof are listed below:

Windows

- ✓ Windows should be in rhythm with the base level.
- ✓ Visual interest should be created through sills, lintels, divided lights and style. Double-hung windows provide more visual interest than casement windows.
- ✓ Replacement windows should fill the entire historic window opening. If historic window openings require closing, the opening should be a different material or texture to maintain the rhythm of the wall.



Fig. 26: Encouraged: double-hung windows, divided lights, lintels and sills



Fig. 27: Encouraged: replacement windows that fill the entire window opening



Fig. 28 Windows should be in scale with the other façade elements. Avoid windows which do not maintain rhythm of the wall as in this example



Fig. 29 Replacement windows should fill the entire historic opening. Avoid replacing windows where the window opening is not filled

Building Facade

- ✓ Facades should reflect proportionate shapes and patterns. Unarticulated walls create poor visual appearance, do not relate to the base nor the roof and are not allowed.
- ✓ Facades should be visually appealing through detailing, openings and materials.
- ✓ Corner buildings are exposed on two streets. As such, corners of these building should be articulated and elaborated to reflect this importance.



Fig. 30: Encouraged: detailing, window openings and material changes



Fig. 31: Encouraged: corner articulation



Fig. 32: Blank façades with no variation, windows or detailing should be avoided



Fig. 33: Façades with no detailing, as in this example, should be avoided

Building Materials

- ✓ New materials should be complimentary.
- ✓ Building materials such as brick, stone, manufactured stone, terra cotta accents, metal accents, stucco and wood are encouraged as they provide visual interest.



Fig. 34: Encouraged: brick and stone materials



Fig. 35: Encouraged: complimentary materials



Fig. 36: Vinyl and aluminum siding are discouraged materials and should not be used to cover historic building materials

SECTION 7 - BUILDING TOP

The building top should be an expression of form, ornament and detail as it meets the sky. The roof should give distinction to the entire building. To create an attractive roof, certain design features can be used to create good storefronts:

Roofs

- ✓ Distinctive corners and cornices can create visual interest.
- ✓ Per the Downers Grove Zoning Ordinance, mechanical equipment must be screened from street level view.



Fig. 37: Encouraged: cornice detailing



Fig. 38: Encouraged: stone cornice detailing



Fig. 39: Cornices should have detailing and should be in scale with the rest of the building. New cornices should not cover original features.



Fig. 40: It is important to provide details at the top of buildings. In this example, the parapet does not have any detail or cornice. Buildings without detail at the top should be avoided

Section 8 – Definitions

Concrete masonry unit (cmu) – A precast masonry unit, typically measuring 8” D x 8” H x 16” L, made mainly of portland cement, gravel, sand and water molded into various shapes.

Cornice – A continuous projection that crowns a wall or other construction, or divides it horizontally for compositional purposes.

Divided lights – A window with a number of smaller panes of glass separated and held in place by muntins or a single pane of glass with muntins placed on the surface of the glass to give the appearance of many smaller panes of glass.

Exterior Insulating Finishing Systems (EIFS) – A multi-layered exterior finish system that provide exterior walls with an insulated finish surface and waterproofing in an integrated composite material system.

Knee wall – A short wall upon which a window may sit.

Lintel – A horizontal member above a window opening

Massing –The three-dimensional bulk of a building: height, width and depth.

Pattern Book – The Village of Downers Grove Pattern Book is a document that contains the different patterns and components that create the fabric and context of Downtown Downers Grove and identifies the elements necessary for retaining and enhancing its character as new development, improvements, and changes are considered.

Sill – A horizontal member beneath a window opening.

Streetwall – A continuous built form of buildings at or near the front property line, with no or very small side yards.

Utility Brick – A brick which is larger in size than a standard brick. A standard brick measures 3 5/8” D x 2 1/2” H x 8” L.

Approved 02/20/17

VILLAGE OF DOWNERS GROVE
COMPREHENSIVE PLANNING AD HOC COMMITTEE MEETING

PUBLIC WORKS BUILDING
5101 WALNUT AVENUE
JANUARY 16, 2017 - 7:00 P.M.

Chairman Gorman called the January 16, 2017 meeting of the Downers Grove Comprehensive Plan Ad Hoc Committee meeting to order at 7:03 p.m. and led the meeting with the recital of the Pledge of Allegiance.

ROLL CALL

PRESENT: Chairman Dave Gorman, Carine Acks, Ed Kalina, John Luka, Daiva Majauskas
(7:15 p.m.) Jim Wilkinson

ABSENT: Members Irene Hogstrom

STAFF: Community Development Director Stan Popovich, Senior Planner Rebecca Leitschuh

VISITORS: John Houseal with Houseal Lavigne Associates, 188 W. Randolph, Chicago; Amy Gassen, 5320 Benton, Downers Grove; Linda Kunze, 933 Curtiss, Downers Grove; Rich Kulovany, 6825 Camden Rd., Downers Grove; Don Rickard, 4735 Main St., Downers Grove

APPROVAL OF MINUTES – NOVEMBER 14, 2016

MINUTES OF NOVEMBER 14, 2016 WERE APPROVED ON MOTION BY MR. LUCA, SECONDED BY MR. WILKINSON. MOTION CARRIED BY VOICE VOTE OF 5-0.

INTRODUCTION OF DOWNTOWN DEVELOPMENT REGULATIONS

Mr. John Houseal, with Houseal Lavigne Associates, recalled for the commissioners how the Downtown Development Regulations were approached, i.e., built on the understanding of the previously approved plan for the downtown which created three previously defined districts: 1) the Downtown Core, 2) the Downtown Edge, and 3) the Downtown Transition. Characteristics of each district followed.

Mr. Houseal discussed that tonight's discussion would focus on the content/breakdown of guidelines with the next meeting to discuss the procedures by which regulations would be administered or relief granted. Community Development Director, Mr. Popovich, added that with the regulations, the goal was to have the this Ad hoc Committee and the Plan Commission review the regulations and if the Village Council accepted the findings then staff would draft regulations to

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incorporate into the village's zoning ordinance. Input and discussion from tonight's meeting was important.

REVIEW DOWNTOWN CORE

A review of the Down Core Area followed on the overhead, noting there were many two- and three-story buildings at the sidewalk line, a few one-story buildings, and no setbacks between buildings. Referring to the plan diagram, Mr. Houseal summarized that according to the current plan, building height could be two to three stories. Buildings along the sidewalks had no setbacks and mixed-use was recommended (retail or commercial on ground floor with residential or office on the upper floors).

Per Mr. Houseal, the new maximum building height proposed for the Core area would be 40 feet or three stories, based off of a 12- to 15-foot first floor and a 10- to 12-foot upper floor. Setbacks would remain the same. Proposed uses for the ground floor included commercial, retail, entertainment, and restaurant-type uses. Proposed uses for the upper floors included multi-family residential, office, or service uses. No residential uses would be permitted on the ground floor.

Staff clarified that current drive-thrus were grandfathered in and if the use changed they would have to be removed within a certain period of time. If the use remained with a new tenant, the drive-thru could be used. However, Mr. Houseal recommended that the village continue to not allow drive-thrus in the Core area. He pointed out that parking would probably be the largest driver of development intensity in this area and recommended no changes to the village's parking standards either, as they were very good. (Ms. Majauskas arrives.)

Reviewing the use table depicted on Page 6, Mr. Popovich asked for comments regarding a 24-foot high, two-story minimum building height. No negative comments followed. Further review of the table followed. As for a constructing a building that came to the lot line but had an opening/courtyard or an outdoor dining area, Mr. Houseal stated the space could be factored in but the key was to have some sort of built structure next to the sidewalk, such as a fence.

Discussing office use, Mr. Popovich stated that office was a permitted use on the ground floor. However, Ms. Majauskas preferred to change that, believing that office should be allowed on the second floor only. Mr. Wilkinson asked if there was an option to promote retail without prohibiting office use, should vacancies start occurring, wherein Mr. Houseal suggested making retail and restaurant use permitted uses as of right and make office use a special use with the appropriate standards.

PUBLIC COMMENT

Ms. Linda Kunze, 933 Curtiss Street, Downtown Management, appreciated the comments, stating the downtown businesses "were pleading" with her about this, commenting that Anderson's Bookstore was thinking of leaving because Berkshire Hathaway moved next to Caldwell Banker which has hurt Anderson's sales significantly. She stated that pedestrians stopped walking at Caldwell Banker and would not travel beyond that point. Ms. Kunze thought the idea of a special use may be fine and could also give the village an opportunity to think about the use if retail could not be obtained. At the same time though, Ms. Kunze asked whether the change would discourage the outdoor cafes currently. Mr. Popovich explained that as with any existing cafes, the building

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line would not have to be moved and outdoor seating can be placed in the right-of-way with the proper license agreement. Anything new, however, would have to meet the 0 to 5-foot setback requirement. The Cellar Door was used as an example by staff.

Mr. Rich Kulovany, 6825 Camden, Downers Grove, asked if there was consideration for roof-top parking, citing a grocery store using it in Glenview, to which Mr. Houseal indicated other cities were doing that same thing and that currently, in the village, it was not prohibited. Mr. Popovich indicated that the maximum height for such scenario would be to the “flat part of the roof” with the screening of mechanicals or cars being required. Asked if there was consideration to have a single-family use above a store/restaurant, Mr. Popovich explained that single-family use would need multiple units above to be considered a multi-family unit in the district and so it would be permitted in the village’s code.

Asked if outdoor rooftop seating could be permitted currently or in the future, Mr. Popovich explained it could with the proper building code compliance.

REVIEW DOWNTOWN EDGE -1

Mr. Houseal recalled this area was initially one sub-area under the Downtown Edge, but now delineated as Downtown Edge 1 (DE-1) and Downtown Edge 2 (DE-2). He recalled members had talked about having the greater intensity located closer to the Core area and the slightly less intensity located toward the Transitional area. Two districts were then created: DE-1 and DE-2, which were very similar districts except for their allowed height difference. An overview of the two districts were shown on the overhead. Current building height, setbacks and uses for the Downtown Edge were described.

Mr. Houseal then shared that the proposed building height maximum for the DE-1 District would be 72 feet to allow six stories, approximately 12 feet per floor. Proposed setbacks would be 0 feet. Proposed side yard setbacks would be a minimum of 5 feet or minimum of 10% of lot width, whichever was greater. Proposed side yard setback for properties adjacent to the Downtown Core District would be 0 feet. Proposed rear yard setbacks would be 10 feet throughout the entire district and special setback standards would apply if a building abuts the residential districts.

A variety of proposed uses followed. Mr. Popovich also added that since this district was so close to the Core district, staff wanted to incorporate some of the characteristics of the Core to this district and for the DE-2 district, characteristics from the Transition/residential area would be incorporated.

REVIEW DOWNTOWN EDGE – 2

Mr. Houseal noted that this district was a bit further away from the Downtown Core area and was less intense. He pointed out that this area was where setback requirements were beginning to emerge and height was coming down. Much of the bulk standards were the same as DE-1 except the height was being decreased to 1 to 5 floors with maximum of 60 feet. Setbacks were proposed at 10-feet throughout the entire district to allow for some landscaping and trees. Proposed rear setbacks were the same as DE-1. A wide range of uses still existed.

Ms. Majauskas expressed concern about what was not written in the comprehensive plan. While she appreciated all the new developments that would come, she voiced concern that another parking

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lot was being removed and questioned how businesses were suppose to remain and thrive when no one could park. Convenience and accessibility were important. Mr. Houseal explained the challenge was that buildings occupy the majority of the site and cannot provide the parking for all of their uses. In order for any downtown to be successful, he explained the municipality has to provide public parking whether surface or structure. He pointed out that it is typically the Village that needs to identify locations for future parking.

However, one member believed surface lots were moving toward development and an additional parking structure was needed for the downtown area, but the ideal uses had to draw people to the downtown. Agreeing, Mr. Houseal cited other cities that had full parking structures and that more were being constructed. He elaborated that if businesses could get their employees to park in a parking structure versus in front of their business, it would free up a significant amount of parking.

Mr. Kalina asked if the DE-1 on Forest Avenue could be pushed 100 to 200 feet north, or even to Franklin Street. Mr. Popovich indicated the Village Council delineated the subareas, but staff would follow up. Referring to zoning and its impact on a parking structure in the DE-1 area, someone asked if the structure would have to conform to the existing, as proposed, or could the village seek an exception. Mr. Houseal explained the village could seek a variation with the hardship being that it is not a typical standard DE-1 use because the village is providing parking for the entire downtown so the village cannot be bound by existing zoning regulations because it is a benefit for the entire three districts, not just the village's private use. So it becomes a hardship because no one else is in the same situation and it is entirely unique.

Dialog followed regarding some map discrepancies between the pages of the comprehensive plan relating to buildings located at Rogers and Washington which Mr. Houseal would fix. Additionally, there were concerns about building setbacks at the northwest corner of Maple and Washington and that if the nearby church removed its parking lot there could be a six-story building on the corner with three of the corners being "quaint." Staff pointed out that two newer developments on Maple Avenue were located nearby and were at the property line currently. Another member voiced concern that a six-story building could be constructed katty-corner from a residential home at the southeast corner Maple and Washington. Mr. Popovich pointed out that if the committee thought the church property and the property north of it should be in DE-2 district, they could discuss it. After discussing the matter, members believed there should be a setback due to the height and recommended the church parking lot designation should be changed from DE-1 to DE-2.

PUBLIC COMMENT

Ms. Linda Kunze, with Downtown Management, appreciated the comments about parking and agreed parking was still an issue in the downtown. She expressed the challenges of keeping business owners when they complain that they have no customers or the customers complain about parking. While the village was helpful, Ms. Kunze believed it was time for the village to consider taking some older homes and converting them to a parking structure. She also voiced concern that some of the mayors attending conferences were not even being encouraged to build parking decks due to the new variety of ride-sharing occurring with services like Uber, etc.

Mr. Rich Kulovany agreed that the above corner should be redrawn to be included in the DE-2 district because the Main and Maple development was 70 feet in height and the roof at Marquee on Maple was 59-1/2 feet. The proposal would allow for a slight step down.

Approved 02/20/17

REVIEW DOWNTOWN TRANSITION

Mr. Houseal referenced this district and noted those areas where no front setbacks existed, those areas where setbacks were required, and those areas that had 25 ft. front setbacks. Current height requirements were referenced. Proposed for this district were buildings with a maximum height of 36 feet or three stories in height. Also proposed was a 20-foot front street setback throughout the district except for properties fronting on Main Street, which would have a 10-foot setback. Proposed side setbacks would be five feet or 10% of lot width. Special side setbacks would apply when abutting against a residential district. Proposed rear setbacks would be 20 feet throughout the entire district except for properties fronting on Main Street, which would have a 10 foot required setback. Proposed uses include single, multi-family, office, service, home occupation and institutional but on a residential scale and with residential compatibility. No retail or restaurant uses would be allowed.

Mr. Popovich then raised the topic of minimum lot area per dwelling unit and asked whether the village should be concerned with how many units are in a development if a developer can provide parking. Mr. Luka indicated that the village should care about it because the village did not want 100 square foot units being constructed. Then conversation followed regarding the rage with tiny houses among the Millennial market as well as the range of unit sizes being constructed by developers for the merging of the Millennials and the retirees, who both wanted walkability, rentability (not own homes) and to have nearby amenities. One member cautioned that the village should ensure that its rentable units were a saleable size in order to convert them to ownership units, if necessary, since it would give the village more options to people.

PUBLIC COMMENT

Mr. Kulovany recommended under “Usage” to add Inns and Bed & Breakfasts. Regarding the Millennial comments, Mr. Kulovany stated he was an Uber driver and shared some of the conversations he had while driving Millennials, who basically do not support owning cars in the city when a person can get to work for a relatively cheap fare. Millennials also rent cars and rent units because they do not want to fix anything. He stated that Millennials have a different mind set.

Further discussion followed how the Millennial demographics will change once children come into the picture and, as a demographic group, will eventually move out to the suburbs.

REVIEW DESIGN GUIDELINES

Mr. Popovich depicted various buildings in the village on the overhead, pointing out that their design was based off of the village’s Pattern Book. He described how the various buildings are broken up, the materials used on them, and the rhythm of the buildings, i.e., window patterns, corner treatments, and cornice/roof lines. He stated that EFIS, CMU brick, and vinyl siding are not encouraged in the village. Per a question, projecting signage or awnings can encroach into the right-of-ways but not balconies. Staff administers these guidelines.

Mr. Houseal explained that the intent of the guidelines is to use them for the different types of components or features the village anticipates addressing. However, once the district regulations

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are put into place, they are to be used as a reference. He recommended crafting design guidelines for each of the separate districts discussed above so developers know what to produce.

Asked if he has seen developers go green on their own or whether they need incentives, Mr. Popovich explained that he has seen developers go green when detention or stormwater practices are required. Details followed. Mr. Houseal also shared what he has seen in the field.

Of the three residential developments being constructed in the village, a question was asked whether the village had to change any of the designs wherein Mr. Popovich indicated that the designs were “tweaked” a bit, citing some of the design changes that were made at the Main and Maple project. Mr. Popovich inquired whether the guidelines should continue to be used as guidelines or whether they should be converted to regulations. He explained that currently, they are guidelines, whereby staff can enforce them. If they are regulations that require specific compliance, a review board would be needed. A review board could require developers spend more time to go through the process to receive approval from the board.

Members seemed to favor keeping the guidelines in place versus regulations. Mr. Houseal believed the guidelines needed to be drafted for each of the districts and the guidelines appeared to be successful currently. He reiterated that the guidelines were not meant to dictate architecture or dictate building design but to guide development to ensure the desired downtown character and sense of place is met.

PUBLIC COMMENT –

No additional public comment was provided.

Mr. Popovich closed the discussion by summarizing that he will revise the guidelines according to the changes discussed tonight and then at the next meeting he will discuss uses and procedures. The next meeting will be scheduled for February 20, 2017 at the Public Works Building.

ADJOURNMENT

THE MEETING WAS ADJOURNED AT 9:10 P.M. ON MOTION BY MR. LUKA, SECONDED BY MRS. ACKS. MOTION CARRIED BY VOICE VOTE OF 6-0.

Respectfully submitted,

/s/ Celeste K. Weilandt

(As transcribed by MP-3 audio)

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VILLAGE OF DOWNERS GROVE
COMPREHENSIVE PLANNING AD HOC COMMITTEE MEETING

PUBLIC WORKS BUILDING
5101 WALNUT AVENUE
FEBRUARY 20, 2017 - 7:00 P.M.

Chairman Gorman called the February 20, 2017 meeting of the Downers Grove Comprehensive Plan Ad Hoc Committee meeting to order at 7:06 p.m. and led the meeting with the recital of the Pledge of Allegiance.

ROLL CALL

PRESENT: Chairman Gorman, Ms. Acks, Ms. Hogstrom, Ms. Majauskas, Mr. Wilkinson

ABSENT: Members Kalina and Luka

STAFF: Community Development Director Stan Popovich

VISITORS: John Houseal with Houseal Levigne Associates, 188 W. Randolph, Chicago; Amy Gassen, 5320 Benton, Downers Grove; Linda Kunze, 933 Curtiss, Downers Grove; Rich Kulovany, 6825 Camden Rd., Downers Grove; Don Rickard, 4735 Main St., Downers Grove; Graham Mosey, 4925 Forest Ave., Downers Grove; Steve Kuhn, 4925 Forest Ave., Downers Grove

APPROVAL OF MINUTES – JANUARY 16, 2017

A typographical error was noted on page 5, second paragraph. On the same page, under Public Comment, fourth line, delete the word “fair” and insert “fare”. **MINUTES OF THE JANUARY 16, 2017 MEETING WERE APPROVED ON MOTION BY MR. WILKINSON, SECONDED BY MRS. ACKS. MOTION CARRIED BY VOICE VOTE OF 5-0.**

REVIEW DOWNTOWN DEVELOPMENT REGULATIONS

Community Development Dir. Stan Popovich referenced a memo prepared for members, a copy of the functional sub-areas and some emails. He reviewed the latest updates to the plan. However, he pointed out that tonight’s meeting would focus on the development regulations and how they would impact the development throughout the downtown area and how each of the sub-areas would function in terms of setbacks and bulk requirements. Those changes, he clarified, would be made at the village council level.

Mr. John Houseal, with Houseal Associates, reviewed what changes were made between the boundaries and what changes could not be made, i.e., those between the three primary districts. Ms. Acks, referring to the northwest corner of Maple and Main, voiced concern that a six-story

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building could be located there, creating a canyon. She further expressed her concern that the parking along the railroad tracks, north of Burlington, could be removed and replaced with six-story buildings and block out the downtown. She believed a cap in height should be required.

Mr. Houseal explained that the ad hoc committee could reduce the height at the northwest corner of Maple and Main (up to Grove Street) if desired. Ms. Acks, along with others, preferred to expand the northwest corner of Main and Maple, as well as the two parcels along the tracks (east and west of Main Street) to become part of the Downtown Core.

Mr. Popovich reviewed the other changes that were made from the last meeting. The group also discussed cell towers in the downtown area.

PUBLIC COMMENT

Mr. Graham Mosey, 4925 Forest Ave., Downers Grove, referenced his emails as well as another neighbor's email and shared his frustration that this committee was having its conversation and putting forth recommendations to council only to find out that the village council has made its decisions on district lines. Mr. Mosey stated his business has been in the village for forty years and the reason he purchased his business on Forest Avenue was because years ago the concept was to take off the load from the Downtown Central Business District and to bring it to secondary areas, such as Forest Avenue or Warren Avenue and to bring in service businesses, excluding retail, so that the downtown could have a vibrant retail area. He voiced frustration that council removed part of a footprint from Main Street at the corner of Maple and Main where parking existed and some very good opportunities existed for retail but instead residences were being constructed.

He said he was before this group because the footprint was being reduced, the real estate was being compromised with less services, less businesses and less opportunities for more multi-family high density to be developed in the downtown. Mr. Mosey summarized how Forest Avenue changed over the years and the variety of building types currently found on Forest Avenue. For the village to say that development could not increase in height was irresponsible and to make such changes on Forest Avenue would be economically impacted. He stated it was unfortunate that this committee did not have a say in such changes.

Mr. Mosey further explained he purchased his property with the knowledge that he could develop on Forest Avenue but that the proposed change hindered it and made it economically not feasible. He praised the downtown when a nearby business owner purchased three bankrupt properties with the idea that at some point all of the businesses could be doing a nice development on Forest Avenue. He was in the process of contacting other nearby property owners about this proposed change.

In response, Ms. Majauskas asked Mr. Mosey what he did want, wherein he stated he would not change anything on Forest Avenue. As an aside, he noted that prior to the changes being proposed, he and another property owner and tenant were discussing a proposed development with enclosed self parking for not only the building but "free range parking." His development would have been a mixed-use building.

Ms. Linda Kunze, Downtown Development Corporation, reiterated what the above speaker had said and also liked the comments that were made in Mr. Hinkel's letter to this committee – that some of the new Internet companies are hiring more employees because they want to be downtown and have

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access to Chicago as well as access to nearby amenities in the village. She supported having more offices on Forest Avenue.

Mr. Mosey returned and explained that people will begin to ask what was the reason for the village council to make such a proposal and he stated his job was to get in front of council to find out what was the reason and to make council responsible for that reason. Of the remaining businesses on Forest Avenue, Mr. Mosey explained they were tired because they could not afford to redevelop individually. He honestly thought that Forest Avenue would not have been discussed or compromised the way it had been and believed Forest Avenue should have been left alone.

Mr. Rich Kulovany, 6825 Camden, understood that Mr. Mosey's contention was tall buildings constructed on Forest Avenue as well as across the street and that the shorter buildings did not match the character of what already existed. Mr. Mosey returned and stated it did not match the character nor was it economical when considering the setbacks and the lower height because the ability for parking was being taken away – the taller the structure, the more ability there was to park internally. The lower buildings had no ability for parking. Mr. Mosey commented that the east side of Main Street could be compromised at some point as well, given that parking was an issue there.

Continuing, Mr. Mosey stated the downtown was slowly being “strangled” which made the transition area more important. However, now the transition area was being “strangled” especially on Forest Avenue. Asked if he needed 70 feet to make it feasible, Mr. Mosey indicated not necessarily so and proceeded to explain what his conversation was with staff from 20 years ago.

RECOMMENDATION OF DOWNTOWN DEVELOPMENT REGULATIONS

Mr. Popovich summarized for those in attendance all of the committees and commissions the plan was reviewed by, including village council. Next steps were explained as well as the motions the committee should consider forwarding to the Village Council: 1) motion to review the northwest corner of Maple and Main Streets, running up to Grove Street – to bring the properties into the Downtown Core; 2) motion to review two properties on Main Street, north of Burlington, located on the east and west sides of Main; and 3) motion to review Franklin Street, 7 blocks south on the east side of Forest Avenue.

MOTION BY MR. WILKINSON TO RECOMMEND THE DEVELOPMENT REGULATIONS REGARDLESS OF WHAT THE ZONING MAP LOOKS LIKE.

SECONDED BY MS. HOGSTROM.

MOTION CARRIED BY VOICE VOTE OF 5-0.

MOTION BY MS. ACKS THAT THE PLAN COMMISSION REVIEW THE NORTHWEST CORNER OF MAPLE AND MAIN STREETS, UP TO GROVE STREET AND TO CONSIDER BRINGING THE PROPERTIES INTO THE DOWNTOWN CORE.

SECONDED BY MS. MAJAUSKAS.

MOTION CARRIED BY VOICE VOTE OF 5-0.

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Discussion then followed regarding the two parking lots located on the east and west sides of Main Street along the railroad tracks. Comments followed that the parcels should be an expansion of the Downtown Core area from Main to Washington. Other comments followed that the train station should be included in the Downtown Core also.

Discussing the west side of Main Street, consensus was to leave it as Downtown Edge 1.

MOTION BY MS. ACKS THAT THE PLAN COMMISSION CONSIDER THE EXTENSION OF THE DOWNTOWN CORE TO THE AREA OF THE TRAIN STATION AND THE PARKING LOT NEXT TO IT (BETWEEN MAIN AND WASHINGTON, BURLINGTON AND THE TRAIN STATION).

SECONDED BY MS. MAJAUSKAS.

MOTION CARRIED BY VOICE VOTE OF 4-1 (NAY: MR. GORMAN)

Asked what members thought about Forest Avenue (east side) up to Franklin Street and it being currently depicted as Downtown Transition, Mr. Wilkinson felt it should remain in character with what currently existed. On the Main Street side he noted the various businesses that were currently there and said they could be incorporated into the type of development Mr. Mosey discussed above, i.e., service organizations that could be anchored. The Franklin to Forest to Main area should be kept as is. The taller buildings can be constructed with the caveat that parking be created underneath the building.

Ms. Majauskas, however, pointed out that with the new condo building in the area now, the village began to make the area “big buildings”, other than the houses that were businesses, and so, to cut it off mid-block did not make sense. Further dialog followed.

MOTION BY MR. WILKINSON THAT THE PLAN COMMISSION CONSIDER A RECOMMENDATION TO CONSIDER DOWNTOWN EDGE 2 ZONING FOR THE EAST SIDE OF FOREST AVENUE UP TO FRANKLIN.

SECONDED BY MS. ACKS.

MOTION CARRIED BY VOICE VOTE OF 5-0.

Mr. Popovich reviewed the next steps the comprehensive plan would follow. He thanked all of the members for volunteering their time and effort on the village’s comprehensive plan.

ADJOURNMENT

THE MEETING WAS ADJOURNED AT 8:14 P.M. ON MOTION BY MR. WILKINSON, SECONDED BY MS. MAJAUSKAS. MOTION CARRIED BY VOICE VOTE OF 5-0.

Respectfully submitted,
 /s/ Celeste K. Weilandt
 (As transcribed by MP-3 audio)



DEPARTMENT OF COMMUNITY DEVELOPMENT
MEMO

To: Comprehensive Planning Ad Hoc Committee
From: Stan Popovich, AICP
 Director of Community Development
Date: February 20, 2017

Based on the January 16, 2017 Comprehensive Planning Ad Hoc Committee meeting, the downtown regulatory framework has been revised. The revisions include:

Page # in document	Description
1	Updated functional subarea map to reflect discussed changes
3	Updated NW corner of Maple Avenue and Washington Street on the map
4	Updated NW corner of Maple Avenue and Washington Street on the map
6	Removed use section in the table as uses are more defined on page 7 in the current revision
7	Revised text of Downtown Design Guideline discussion to provide additional clarity on the application of the guidelines.
8	Included current Zoning Ordinance use table and proposed revised Zoning Ordinance use table based on each of the four subareas. Each use description can be found in Section 5 of the Zoning Ordinance. The Zoning Ordinance is on-line at this location: http://www.downers.us/public/docs/code/Chapter28.pdf

Staff is recommending the Comprehensive Planning Ad Hoc Committee forward the downtown regulatory framework to the Plan Commission for their review.



DEPARTMENT OF COMMUNITY DEVELOPMENT
MEMO

To: Comprehensive Planning Ad Hoc Committee
From: Stan Popovich, AICP
Director of Community Development
Date: January 16, 2017

Based on previous Ad Hoc Committee discussions and the Comprehensive Plan's Downtown Focus Area, the attached draft development regulations have been prepared. The regulations cover the four districts developed for the downtown:

- Downtown Core
- Downtown Edge – 1
- Downtown Edge – 2
- Downtown Transition

In addition to the draft development regulations, a discussion about design guidelines and an outline of some components and features of design guidelines are provided within the attached document.

DRAFT MINUTES

**VILLAGE OF DOWNERS GROVE
PLAN COMMISSION MEETING**

MINUTES FOR AUGUST 28, 2017

Chairman Rickard called the August 28, 2017 meeting of the Plan Commission to order at 7:01 p.m. and led in reciting the Pledge of Allegiance.

ROLL CALL:

PRESENT: Chairman Rickard, Ms. Gassen, Ms. Hogstrom, Ms. Johnson, Mr. Kulovany, Mr. Maurer

ABSENT: Mr. Boyle, Mr. Quirk, Ms. Rollins, Ex. Officio members Davenport, Livorsi & Menninga

STAFF: Director of Community Development Stan Popovich
Village Sr. Planner Rebecca Leitschuh

VISITORS: Scott Richards, 1130 Warren Avenue, Downers Grove

APPROVAL OF MINUTES: August 7, 2017 meeting

Ms. Gassen moved, seconded by Mr. Kulovany to approve the minutes for the August 7, 2017 meeting.

Chairman Rickard noted in the last paragraph on page 2, the EIFS acronym should be corrected. In the fifth paragraph on page 3, a reference is made to a 25' drive-through "land," which should read "lane."

AYES: Ms. Gassen, Mr. Kulovany, Ms. Hogstrom, Ms. Johnson, Mr. Maurer, Ch. Rickard

NAYS: None

The Motion to approve the minutes as corrected passed 6:0.

Chairman Rickard explained that the Public Hearing on this Agenda is a continuation from the June 26, 2017 and August 7, 2017 meetings to consider updates to the Downtown development regulations.

PUBLIC HEARING

FILE 16-PLC 0019 (Continued from June 26, 2017 and August 7, 2017): The purpose of this request is to consider updates to the downtown development regulations. Village of Downers Grove, Petitioner.

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Community Development Director Stan Popovich noted that the Plan Commission's last meeting on this topic was held on August 7, 2017. He indicated that the intention of this meeting is to finalize the Plan Commission's recommendations and report, and make its recommendation to the Village Council. He pointed out that in Staff's Memo to the Commission he summarized the Plan Commission's discussion in the previous meetings regarding bulk regulations, and historic preservation. He noted that two items require additional discussion so as to provide the Council with a clearer direction from the Plan Commission. The first concerns the density allowances in DE-1 and DE-2. In the Comprehensive Plan it is noted that these areas should have greater residential density to support the Core. He then displayed maps depicting both the DE-1 and DE-2 areas under consideration. As established in the Zoning Ordinance, a lower square footage equals a higher density. For example, allowing one unit per 800 square feet results in 54 units in a one-acre area. He requested discussion by the Commission as to what figure they would consider for the Core area in their recommendation. He referred to information provided in the Commissioners' packets showing the various developments in the general downtown area and the density of those developments.

Ms. Gassen noticed that in some of the communities Mr. Popovich referenced for comparison there is no density requirement. Mr. Popovich explained that some other communities measure density in different ways, such as floor area ratio, which refers to the size of the building and how many units are planned for that building. For example, with a 10,000 square foot lot and a floor area ratio of 2.5, a 25,000 square foot building could be constructed. The number of apartments in that building would be up to the developer.

Chairman Rickard said that regardless of what numbers are established for lot areas and densities, there may be other things that prevent reaching a specific number of units. Staff's opinion was that 800 was a good number for the Core with the logic that the edge areas would be where the bulk of the density occurred. Chairman Rickard said he thought a good number for all three areas would be to call the Core area 900, Edge-1 700, and Edge-2 800. That would average the 800 figure throughout the three zones.

Ms. Hogstrom said she likes those numbers as proposed because the 800 figure is ideal, as they want higher density in the downtown edge. The 3,000 square feet would decrease that and that is not the direction they want to go.

Ms. Gassen said since they are lowering the height in the buildings in the Core area it would result in a lower density than exists now. Mr. Popovich agreed saying that lowering the heights limits the available number of units for that area.

Chairman Rickard said that the 900 square feet equates to 48 units per acre, with Edge-1 at 62 units per acre at 700 square feet, and Edge-2 at 54 units per acre based on 800 square feet.

Mr. Kulovany said he likes what Naperville, Glen Ellyn and Wheaton have done. Downtown Naperville on a weekend evening is really vibrant. What the Village is trying to accomplish is to increase the number of people who want to walk to the downtown

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restaurants and shops. He thought the idea was to increase density, particularly in Edge-1. So he would go either with no density standards, or close to the Chairman's recommendations. He sees that as a reasonable compromise.

Mr. Maurer said he would also agree with keeping it at 800 or above. There are a lot of units coming on line, and there will be a big difference when three of the buildings currently under constructions are completed and filled.

Mr. Popovich reviewed Chairman Rickard's proposed figures, with the Core area at 900 square feet per unit, Edge-1 at 700 square feet per unit, and Edge-2 at 800 square feet per unit. In response to Chairman Rickard, Mr. Popovich said that the Transition Area is handled through a floor area ratio of 2.5, and not square footage per unit.

There was consensus by the Commission members to the figures proposed by Chairman Rickard.

Mr. Popovich moved the discussion to the Design Guidelines and the Plan Commission's Recommendations. He asked for clarification as to the issues they are attempting to address in reviewing the Design Guidelines.

Mr. Kulovany recalled two issues, one being not enough citizen input early in the process, and the second whether the Village has the proper tools if a developer chooses to follow the Building Code rather than the Design Guidelines. Mr. Kulovany reiterated his thoughts expressed at the last meeting that he didn't think this should be a Plan Commission decision, but should rest with the elected Village Council. He thinks it is a more serious policy issue that should be determined by the elected officials.

Regarding insufficient citizen input, Mr. Popovich asked whether that referred to the development of the Design Guidelines or the implementation of the Design Guidelines. Mr. Kulovany said he thinks it refers to both, and he noted the amount of frustration that's been expressed about Main and Maple. People have expressed that they never hear about these developments until it's "too late." Meetings of the Plan Commission are published about important issues and only a few people attend. He said he also thinks that developers would like to have more input early on as he thinks they would like to be welcomed by the community. He thinks they have to at least consider providing a format that would allow earlier input. He is recommending that there be public discussion, and not to decide in advance what the foregone conclusion would be.

Mr. Popovich replied that what he is hearing is public participation, but asked if it is about the building design, regulations and appearance, or are they trying to get more public involved when a project is coming forward so they can comment on it. Design Guidelines tend to be very material oriented. Are they trying to get more people actively involved to see what is changing, or actively involved in designing buildings?

Mr. Kulovany responded it's actually more to review a mechanism where there is a review process earlier on that could be opened to the public before a developer invests a lot of money in plans at the schematic stage. He referenced the amount of frustration by the community about what Walgreens is proposing at 63rd and Woodward, and the Plan Commission Meeting

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Village and Staff had no tools to say “wait, if you could take your durable materials a little higher up we think that would be better for the Village.” He noted that even the Village Council had nothing they could say about it as it was really up to Walgreens. Mr. Kulovany said his feeling is that the Staff and even the Village Council need more tools to be able to go back to a developer with guidelines that have more teeth in them, or some type of design review.

Mr. Popovich asked are whether he was referring specifically to the downtown area or Village-wide. Mr. Kulovany said he’s referring mostly to the downtown area and to catalyst sites.

Chairman Rickard said he sees several themes that people are concerned about in the downtown area. He said one of the complaints is lack of public input. People come to public hearings when developments are in the final engineering phase, and people feel as though everything is locked in and it is much more difficult to suggest changes at that point. People are trying to digest and consume a large, complicated proposal when they are often hearing about it for the first time. They are trying to understand the situation, don’t have time to get their thoughts and questions together, and it often results in them getting aggravated. He also agrees that it would be nice to get some input, or at least get a look at a proposal earlier on, except he thinks another idea to consider is, instead of coming to the Plan Commission at the final engineering phase, there instead be an intermediate step. He said he wasn’t recommending another committee or design review, but something similar to the Village Council’s First Reading or Workshop sessions. A developer could come in with their application and preliminary site plans, floor plans, etc. They would not as yet have spent money for studies for traffic, civil engineering, stormwater, etc. That would introduce everybody to the project, and the public would start to get a wide view of it, similar to First Readings. It would give the developer some type of assurance moving forward once they obtain everyone’s thoughts and comments on the concept, and there would not be any major bumps down the road. If there were comments, the developer would better be able to incorporate them at that level. He doesn’t think it would add additional time to the petitioner, as it would be during their normal course of work. He thinks that is worth considering and would give everyone more time to get their heads around these proposals as well.

Mr. Popovich said that Staff has the ability to provide for neighborhood meetings. He asked if this is something the Plan Commission would want to consider in terms of mandating neighborhood meetings either prior to or after submittal to Staff to encourage public participation. Chairman Rickard said that would be helpful, but he is looking to find something that would allow the Commission members the ability to look at the proposal earlier as well. It would also give the petitioner a better opportunity to address the comments before completing their projects. This would be especially beneficial to the Commission for larger projects that are difficult to digest in the 72 hours they have to review a petitioner’s proposal.

Ms. Johnson asked if the Chairman felt these sessions would be delivered by size of the project, and if so, what size is he considering.

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Ms. Gassen agreed that Main and Maple was a lot to digest. She asked how much detail they are supposed to determine tonight, in terms of what projects should have early input. Mr. Popovich replied he wasn't looking for that, but just wanted to understand the issue brought forward. Ms. Gassen noted that she agreed that the issue of public input is one of the biggest complaints they are receiving, because the public generally sees the projects for the first time when they are pretty much finished. Often it's shocking to the public, and she thinks if they saw the proposals earlier it might help solve the problem.

Chairman Rickard said with regard to the actual Design Guidelines, he reviewed them and made several comments on them. When he finished, he realized how minor his comments were. He wasn't suggesting changes as much as further clarifying some items. He didn't find anything wrong with the Design Guidelines themselves. People seem to complain more about issues such as bulk, rather than the design itself. His comments were geared more toward base materials, durability, etc.

Ms. Gassen agreed that for the most part Design Guidelines are sufficient. They could review them to create different Guidelines for specific areas. She said that the Plan Commission doesn't typically refer to them and it might be useful if they referred to them more.

Mr. Kulovany said that almost every petitioner refers to the Comprehensive Plan and takes its direction from that. He's not sure that the Design Guidelines have the same depth and breadth to them. A document from 2008-2009 should be reviewed again. More specifically, he said there was a huge uproar about the Main and Maple building, specifically concerns about having 70' at the curb. Trammell Crow put one setback in. He said another later recommendation to the developer came from the Plan Commission for another setback, and the developer replied not when it comes up at the eleventh hour, because by the time the project comes to the Plan Commission, they already have \$115,000 invested. If this request for an additional setback came earlier on in the process, it would have been positively considered. The developer said that Glenview has many public meetings and input. He thinks this is up to the Village Council to determine whether this should be studied in more detail.

Mr. Maurer commented that he likes the way many of the Design Guidelines are written. The photographs show recognizable historic structures from Downers Grove and suggest that is the direction they want to go; however, there is nothing in the language that specifically calls out a style they must meet. He thinks they are dancing a fine line between what is planning and what is architectural design review. He cares greatly about both of those issues, but tends to think that their focus is less on the direct aesthetics than it is on how big and how much.

Mr. Kulovany agreed that it speaks more to the form than the architecture. He's not suggesting that they get in the business of architecture, because in some ways it might preclude some other great ideas from architects and developers. He thinks the Village has been fortunate in that it has had cooperative developers who want to be good neighbors. But there is also the scenario of the developer who doesn't particularly care

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about the Village's Guidelines, and constructs what they want. That is why he thinks these considerations should be looked at.

Mr. Popovich then referred to Mr. Kulovany's comment as to whether the Village and Staff have the proper tools at their disposal to enforce specific standards. Mr. Kulovany said that the Ad Hoc Subcommittee on Historic Preservation took some time for thoughtful people to really discuss and examine the issue of historic preservation. He thinks that the Village is at a threshold with the opportunity to do that for possible downtown design through an Ad Hoc Committee.

Chairman Rickard agrees with that idea. He is comfortable with the Design Guidelines with some minor updates and leaving the process with Staff reviewing and interpreting the Guidelines. He thinks having another committee study whether it makes sense to get into binding architectural design is a big discussion and needs to happen. At this time he is comfortable with the process as it is.

Mr. Kulovany said the differential is that what happens downtown in the next 8-12 years will impact the way this Village looks for the next 50-100 years.

Mr. Maurer agreed because they are talking about the big developments that will be there for a very long time.

Mr. Popovich then asked if the recommendation is to have someone study the Guidelines closer.

Chairman Rickard said for the Design Guidelines, he thinks where it talks about the base segment of the building design on page 7, he would add something like "base level building materials should be durable, requiring little maintenance, and withstand street level pedestrian traffic and abuse."

Mr. Maurer added that maybe they can include a term such as "of a higher quality material" in addition to durable.

Mr. Popovich said he didn't think they would be getting into specifics regarding the Guidelines, but instead asked the question as to whether it's the application of the Guidelines or the Guidelines themselves. That is the primary question. They can always tweak the Guidelines. He asked whether the recommendation of the Commission is to review all the Design Guidelines, or the potential application of the Design Guidelines, or both, or neither.

Mr. Kulovany said he thinks the recommendation is both. The consensus seems to be that the Guidelines are good, but may need some tweaking. The overall issue of the kind of Village input, the tools available to Staff—he believes those issues require more study and perhaps an Ad Hoc Committee if the Village Council deems it wants to develop such a study.

Ms. Gassen asked whether Staff wanted specific recommendations on the Guidelines, and Mr. Popovich said those could be taken off line.

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Chairman Rickard commented that suggestions made at the last meeting were to further explore the dynamics of regulations versus guidelines. He said what he is hearing is a consensus to stay with the Guidelines and not create any new regulations that are binding and might trigger some additional process. All Commission members agreed.

As for Historic Preservation, Mr. Popovich said that one comment was to provide more links to historic preservation tax credits on the website. He asked if there were any additional comments on any other items discussed at previous meetings.

Chairman Rickard said he had no further comments. The remainder of the Commission had no further comments.

Chairman Rickard then called upon anyone in the audience who wished to speak on the topic. He then swore the audience member in.

1.Scott Richards of 1130 Warren Avenue, Downers Grove (Oak Tree Towers) said he has lived in the Village for about fifteen years, and lived in Lombard prior to that. He said that a good point was raised in terms of trying to reach the citizens. Many times when he attends meetings the information is over his head. He mentioned living in Lombard because they are in a different type of governing with Districts, and the public being represented by the Trustee elected for their specific District. He felt it worked much better than the system in Downers Grove because people thought they were part of the process and could participate better. He thinks the citizens of Downers Grove don't attend because it's a done deal, or the Village is going to do what it wants to do and will not listen to the public input.

Mr. Richards said he feels as though Downers Grove's downtown is in jeopardy with all of the huge projects that are going on right now. It doesn't seem as though anyone really cares about protecting what the Village has or in trying to capitalize on what the Village has in terms of its historic heritage. It is an old town that has an interesting downtown with some stores that are worth going to. One town that excels is Geneva in that it has a variety of well-maintained stores, with a feeling of universal connection in appearance. He thinks Glen Ellyn and Hinsdale have those same qualities, while Downers Grove does not have that feeling. There is not much in Downers Grove's downtown to draw people in. There are gyms opening up, realtors opening up next door to each other taking up viable space that should be going for stores. Those businesses could be anywhere in a strip mall in town and they would do fine.

He says one example of his feelings of losing the downtown is the monster at Main and Maple and people are still asking how that got through. It overwhelms the downtown, and he can't believe the two buildings got passed. Another aspect in town is parking which is ridiculous anymore. He often gives up after going around the block several times to find a parking space. Most people moving into the new buildings will probably have two cars. It's frustrating, and yet they keep building, building and building big. He is not against development but doesn't understand the direction the Village is going with massive development in a three-block downtown area. He never sees enough attention

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paid to traffic impact on these projects. He doesn't understand why nobody in this town seems to worry about traffic generated by these buildings. Mr. Richards said he feels as though the developer is in the driver's seat in Downers Grove, and they are telling the Village what to do. He thinks that is wrong. This is their home and as residents they have a right to some input as to what is and what is not allowed. If a developer chooses to go somewhere else that's fine because the Village does not have to take the first thing that comes in for consideration. Other towns stand their ground. He would like to see our downtown saved. There is some architectural interest in the buildings. He believes a developer should work with the town and not against it. Developers leave once the money is in their pocket and residents live with what they've left behind. Tax dollars are important but should not be the only thing that decisions are based upon. The Village needs to be in the driver's seat again.

There being no further comments, Chairman Rickard closed the public hearing.

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Ms. Gassen moved with respect to 16-PLC-0019 that the Plan Commission forward the recommendations from Staff's Memo dated August 28, 2017, and include the new density figures and other suggestions noted at this meeting. Ms. Hogstrom seconded the Motion. All in favor. The Motion carried unanimously.

Chairman Rickard noted that this might be Ms. Hogstrom's last meeting and he thanked her for her volunteer work for the Village. He wished her the best of luck.

Ms. Hogstrom moved to adjourn the meeting, seconded by Mr. Maurer. The Motion passed unanimously.

Chairman Rickard adjourned the meeting at 8:00 PM.

Respectfully submitted,

Tonie Harrington,
Recording Secretary

APPROVED 08-28-17

FILE 16-PLC-0019 (Continued from June 26, 2017): The purpose of this request is to consider updates to the downtown development regulations. Village of Downers Grove, Petitioner.

Community Development Director Stan Popovich presented the framework for the downtown development regulations, noting that the Plan Commission's last meeting on this topic was held on June 26, 2017. He reviewed the summary of the previous meeting and Staff's report. Discussion for this meeting will include a review of the existing design guidelines and their use, a review and comment on the CPC design guidelines recommendations, and historic preservation in the downtown area.

Mr. Popovich discussed Table 1 in Staff's report covering existing multi-family developments in the downtown and their densities. Table 2 describes density calculations in other communities, and Mr. Popovich noted that some communities do not use density calculations, but might use floor area ratios instead. He noted that in the DC area Staff finds the 800 square feet per dwelling to be appropriate.

Mr. Popovich then referenced the bed and breakfast recommendation from the Plan Commission, noting that Staff concurs that bed and breakfasts can be created as a subgroup because they are unique, while inns should fall under the lodging classification.

In the DC district, the Commission suggested allowing offices as special uses on the first floor, but permitted uses on the second floor or above. Mr. Popovich noted that there are many offices already located in the DB and DT zoning district that would be impacted by a change in zoning classification. Staff recommends continuing to allow offices as permitted uses in the DC, DE-1 and DE-2 districts. He further noted that some blocks in the DT area have older homes that have been converted to office use. He then explained that Staff's recommendation is to prohibit any new auto-oriented uses in the downtown area to encourage a pedestrian oriented downtown. Existing drive-through uses can continue as a lawful non-conforming use. If a lawful non-conforming drive-through business closes and reopens within six months, the drive-through can continue to be used. If no business reopens after six months, the drive-through would have to close as well.

Mr. Popovich explained Staff's recommendation to allow apartment/condo uses as a permitted use to encourage these uses in the downtown as identified in the Comprehensive Plan. The developments would have to comply with all the bulk regulations and design guidelines. Otherwise a variance would have to be approved. As for areas specified on Gilbert Avenue, and Rogers Street, Mr. Popovich said they would have to be examined further to determine their impact to existing and proposed uses, and to determine what zoning would be appropriate.

Ms. Hogstrom said the tables are very helpful. With regard to Gilbert Avenue and Rogers Street, she asked what would happen if a building changed from a multi-family unit to single-family. Mr. Popovich said they would be considered a lawful non-conforming use, and could maintain their existing multi-family use but could not expand it. If they were to convert to a single-family home they would lose their lawful non-

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conforming designation. Article 11 of the Zoning Ordinance discusses lawful non-conforming uses.

In further response, Mr. Popovich said if a multi-family building were demolished, it would have to go back to single-family construction.

Ms. Hogstrom asked for further clarification that if property on Gilbert were sold to a new party it would still remain multi-family. Mr. Popovich said that was correct.

Ms. Gassen asked about requiring buildings to conform to design guidelines. Mr. Popovich said Staff reviews projects internally to assure that design guidelines are met. Builders are required to meet bulk requirements, density, setback and design guidelines as determined by Staff. With the Marquis on Maple Staff found that they met all bulk regulations and design guidelines. Main and Maple, and 5100 Forest requested relief from the Zoning Ordinance in terms of density. Ms. Gassen said her concern is that there could be a situation where a building that is being constructed might impact neighboring properties, and if she understands this correctly, the public would not know about it until it is under construction. She thinks it is often not an issue, however, there are situations when there might be a concern to other people.

Mr. Kulovany said he agrees with those comments, saying that when Main and Maple was being presented there was not much notice to the community. At least it went to the Plan Commission and two Village Council meetings.

Ms. Gassen added that there could be some kind of compromise where neighbors were notified or a neighborhood meeting was scheduled to inform them of what was being proposed.

Chairman Rickard asked if there was a map of potential redevelopment sites available. Mr. Popovich said it is hard to say because there are a lot of smaller lots in the downtown area owned by one person, and they would have to be consolidated into one lot. With a consolidation they would not have to come before Council if they met all the standards. Marquis on Maple was one lot. The Main and Maple project was a consolidation, and 5100 Forest was one lot as well. Some other lots have been identified in the Comprehensive Plan as catalyst sites.

Mr. Boyle said he would lean toward community involvement earlier on. You would not want to stifle a possible development, but at the same time you would not want to have something built before the community has any opportunity for involvement.

Mr. Popovich said there are certain requirements that have to happen in terms of notification. For rezoning anything other than residential, they have to have a neighborhood meeting. In all public hearing cases, notice would be made to property owners within 250' of the site for the public hearing.

Chairman Rickard said the special use request would trigger the public hearing.

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Mr. Popovich said it sounds as though the Plan Commission would prefer to see this as a Special Use and the consensus was that was correct.

Mr. Popovich then discussed design guidelines, using a photo of an “idyllic” downtown. There are similar elements to the “ideal” downtown area such as base elements, cornices, etc. The Village's existing Design Guidelines were established in January of 2009 based on a pattern book from 2008, and Staff administers guidelines. He explained how Staff looks at the guidelines and how they apply to proposed buildings in terms of materials, entrances, building features, etc. He showed photographs of Acadia on the Green, Burlington Station, Main and Maple with design variations depicting the rhythm of the buildings. He described the variety of rooflines and cornices used in the downtown area. Staff also encourages façade elements such as light fixtures, awnings, benches, landscaping, outdoor cafes, etc. He also displayed examples of downtown buildings in other communities. Staff does not review renovations of interior spaces. The Comprehensive Plan Committee recommended maintaining the current system with Staff reviewing the design elements, and ultimately bringing it to the Plan Commission and Village Council.

Mr. Kulovany referenced the third paragraph of the Design Guidelines which states: “The Design Guidelines are not intended to dictate architecture or building design, but rather guide development to ensure the desired Downtown character and sense of place while facilitating appropriate new development and exterior renovations.” He said he thinks Staff does a fine job in working with developers and providing their input as to how things should look. His question concerns what recourse the Village has if a developer says they are not interested in following the design guidelines but will build according to Code. Mr. Popovich replied that in most cases developers want to work with Staff and present something that will be supported by Staff. If someone did not meet the design guidelines, Staff would note that in its report. It could be dealt with as a special use by saying that the developer does not meet the intent of the Comprehensive Plan. It's difficult because many communities have design guidelines but don't codify them into regulations. Most applicants do want to get a positive Staff recommendation.

Mr. Kulovany said his concern is what would prevent a developer from saying, in the middle of the downtown, that this is what he wants to build. He doesn't think this should be the purview of the Plan Commission, but he is not comfortable at this point saying that the guidelines should just be guidelines, and he is also not comfortable with saying these should be mandatory. He thinks this is something the Village Council should look into further. Some communities have a combination of guidelines and requirements.

Ms. Rollins asked whether the guidelines are discussed for some of the other key focus areas, as that would probably help some of the other catalyst areas in the Comprehensive Plan. Mr. Popovich said they are applied primarily to the downtown area, which is pedestrian oriented versus auto-oriented.

Ms. Leitschuh said when people do come in with something unacceptable, Staff does have conversations about the design guidelines and in some cases the developers incorporate some of the design elements.

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Chairman Rickard said he has some thoughts similar to Mr. Kulovany. He referenced a building near his home saying he has a concern with a developer who comes in with something that is unacceptable design-wise. He asked if there is any recourse on the part of the Village to force them into compliance. Mr. Popovich said that they can look at the Comprehensive Plan and say that they have to meet the Comprehensive Plan that has specific text related to design. The developer has to market an attractive product.

Chairman Rickard asked if it would be a reach to take wording from the Comprehensive Plan to correlate it to the design of a building. He thinks that it is reasonable to deny some developments if it can be tied into the Comprehensive Plan. Mr. Popovich said Staff would have to craft arguments that would be defensible. Design Guidelines are considered a policy of the Village. He said about the Forest Avenue building was approved but the builder changed materials during the project. Staff works on behalf of the Plan Commission and the residents. There has to be an understanding that the Village wants to facilitate redevelopment; however, it also has to be understood that the developer's bottom line is to come up with a product that will sell.

Mr. Kulovany said his point is that this needs more study to really determine what is the right mix for Downers Grove. How do we get developers to want to build here as opposed to other suburbs, and how do we get the public's input as to the downtown as well.

Ms. Hogstrom asked about the Marquis materials used on the bottom of the building, and Mr. Popovich said the finish on the first floor is a limestone block.

Mr. Boyle said he thinks the buildings downtown look amazing. One thing he mentioned was the complex just south of the Lemon Tree grocery, where he felt they could promote natural materials as much as possible. Cedar starts to degrade. Mr. Popovich said there is an International Property Maintenance Code that Staff follows that would require maintenance of the building. Code Enforcement officers will go out to tell people they have to maintain certain aspects of their property. Mr. Boyle also noted the pathway to the parking garage that could benefit from some additional maintenance, particularly as a way to promote the parking garage as having connectivity with the downtown community. Mr. Boyle also referenced the northeast corner space at Main and Curtiss. Mr. Popovich said the tenants will be making some improvements to the façade. Mr. Boyle asked whether Staff will have a chance to review that. Mr. Popovich said Staff has looked at it and will review it again.

Ms. Gassen said it would be nice if there was more input in some of the designs that come to the Village. She asked if the Plan Commission would be making suggestions regarding guidelines. Mr. Popovich said if they think there are specific elements that should be included in design guidelines, then that should be noted. Ms. Gassen said she has some general categories such as decorative lighting fixtures, using warmer colored lamps, landscaping, and special recommendations for buildings with historic significance. She also suggested providing a link regarding tax credits available for some of the buildings. She suggested placing another link to the design guidelines besides just the Architectural Design Review Board page.

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Mr. Popovich then reviewed the Historic Preservation Ordinance that was adopted in 2015. He noted that there are several buildings in the downtown that are historic properties, such as the Main Street train station. He asked if there were any other ideas on historic preservation in the downtown.

Ms. Gassen asked whether the Architectural Design Review Board will be reviewing these updates, and Mr. Popovich said they would not at this point. The Village Council will determine whether they will review this.

Mr. Kulovany said the new Comprehensive Plan does a great job in promoting historic preservation downtown. He doesn't know what more the Plan Commission can add. There is a gap for commercial properties with the 10% federal investment tax credit for buildings over 1936 and the 20% only comes into play if you have a national historic registry district. So it's going to have to be individual landmarking.

Ms. Gassen said really all they have right now is the 10%.

Mr. Kulovany said that Staff is also doing a great job in working with developers and he was not referring to Staff when he mentioned the difficult developer. Mr. Popovich said before a case is brought to the Plan Commission it has typically been in Staff's hands for a couple of months for review to ensure compliance with all planning documents. In some cases staff will work with developers for some time before the developer creates a final plan for a formal submittal.

Chairman Rickard said he would never suggest that buildings have to be a certain color or architectural style. He has had some experience with architectural boards as a petitioner and in some cases they are nightmares. The question is how do you codify architectural style that everyone would agree with. Another question is the enforceability of design. Main and Maple has been a case in point where people have said to him that they don't approve of the bulk. He hasn't heard anyone come up with exactly what they would want to see.

Mr. Popovich said that Staff will come back with an additional report on what was discussed this evening. The August 28th meeting will focus on finalizing the report to the Village Council. This should go before Council sometime in September. He hasn't had any feedback from Council so far.

There being no further question or comments, Chairman Rickard called for a Motion to Continue the Public Hearing.

Ms. Hogstrom moved to continue the Plan Commission Public Hearing to August 28, 2017. Ms. Gassen seconded the Motion. All in favor. The Motion carried.

Mr. Popovich gave an update on several projects in the Village. He noted that there would be a National Planning Conference in Naperville in September. He will check to see if funding is available, and asked if anyone is interested to let Staff know.

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**Ms. Gassen moved, seconded by Mr. Kulovany to adjourn the meeting.
All in favor. The Motion carried.**

Chairman Rickard adjourned the meeting at 8:43 PM.

Respectfully submitted,

Tonie Harrington,
Recording Secretary

APPROVED MINUTES

**VILLAGE OF DOWNERS GROVE
PLAN COMMISSION MEETING**

MINUTES FOR JUNE 26, 2017

Chairman Rickard called the June 26, 2017 meeting of the Plan Commission to order at 7:02 p.m. and led in reciting the Pledge of Allegiance.

ROLL CALL:

PRESENT: Chairman Rickard, Mr. Boyle, Ms. Gassen, Ms. Hogstrom, Mr. Kulovany, Mr. Maurer, Mr. Quirk, Ms. Rollins

ABSENT: Ms. Johnson, Ex. Officio members Davenport, Livorsi & Menninga

STAFF: Director of Community Development Stan Popovich
Village Sr. Planner Rebecca Leitschuh

VISITORS: Jim Wilkinson, 1125 Black Oak, Downers Grove
Scott Richards, 1130 Warren, Downers Grove

Chairman Rickard reviewed the procedures to be followed for the meeting, explaining that the Plan Commission is a recommending body. He noted that the Village of Downers Grove is the Petitioner for the public hearing on the Agenda. This Public Hearing will span a total of three meetings, and at the end of the third meeting the Plan Commission will make its recommendation to the Village Council. He asked anyone who intended to speak during the Public Hearing to rise and be sworn in.

PUBLIC HEARING

FILE 16-PLC-0019: The purpose of this request is to consider updates to the downtown development regulations. Village of Downers Grove, Petitioner.

Community Development Director Stan Popovich presented the framework for the downtown development regulations, noting that the Village previously approved the Comprehensive Plan over a series of many meetings. The Comprehensive Plan Committee (hereinafter "CPC") developed the regulatory framework in the beginning of 2017, and the Plan Commission is charged with reviewing, commenting on and making a recommendation to the Village Council on the regulatory framework as part of its review over the next three months. For those meetings, the Plan Commission will meet for a second session each month so as not to delay any other petitions brought before the Commission. Director Popovich reviewed the background on updating the Comprehensive Plan ("the Plan") and developing regulatory framework.

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Mr. Popovich explained the differences between the Plan, downtown regulatory framework, and the development of downtown regulations. The Plan is a long-range document, which is visionary. It is not legally binding, and in this particular case focuses on the downtown area including the physical, economic, social and environmental aspects of the downtown. Development regulations regulate day-to-day activities. Development ordinances are used on a daily basis to implement the visionary Comprehensive Plan. Once the Village Council approves the regulatory framework, which acts as a bridge between the regulations and Comprehensive Plan, Staff will draft regulations with the Village Attorney and present them to the Plan Commission for review and the Council for final consideration and approval.

The Village Council adopted the Plan on June 13, 2017 and it is the official policy of the Village with no proposed revisions. Mr. Popovich reviewed the membership of the CPC representing various boards and commissions of the Village. The Plan Commission will review the CPC's regulatory framework, and provide comments and recommendations to the Village Council. Finally the Village Council will review the CPC's regulatory framework, review the Plan Commission's comments and recommendations regarding the regulatory framework, and approve the regulatory framework, or direct either the Plan Commission or the CPC to make revisions to the framework.

Director Popovich reviewed the downtown Key Focus area, and key concepts related to that area. He reviewed materials included in the Plan Commission members' packets including the key concepts for the downtown focus areas, the downtown catalyst sites, and downtown functional subareas as to boundaries, etc.

Mr. Popovich referred to the downtown Core Area with an existing maximum height of 70 feet, and a proposed height of 40 feet or three stories. That would allow for a fifteen foot first floor and twelve feet for the other two stories. He said that the minimum height requirement was 32 feet and is proposed to be changed to 24 feet or two stories.

The existing Build-to zone is 0'-10' to the lot line with a proposed 5' setback area. There is no change to parking, and the minimum lot area per dwelling unit also has no change recommended.

Mr. Boyle asked how this works with zoning. Mr. Popovich replied that if approved, it would be a regulation. If a proposal for 41 feet in height were submitted, the petitioner would have to get a variance or change the plan. He said that the framework is first review to see if this is the way the Village would like to proceed.

Chairman Rickard noted this would affect what property owners in this area can do, and limits them to half of what they would be able to build. He asked whether they were notified by public notice or general notice about these proposed changes. He was surprised that some of those property owners were not present at the meeting. Mr. Popovich said this is published as framework, as it is not law at this point. It will be published to property owners later as a specific notification.

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Ms. Rollins asked whether there has been any feedback from business owners, and Mr. Popovich said they had feedback from owners at Forest and Warren.

In response to Mr. Quirk, Mr. Popovich said that the 800 square feet would allow 54 units per acre. If the Plan Commission sees this as an opportunity to provide for more density, then they should include that in their recommendation. With an 8,000 square foot lot, a developer could put in ten units. Mr. Quirk asked whether there is an opportunity to increase that size. Mr. Popovich said he would welcome that discussion if that were the direction the Plan Commission would want to pursue.

Mr. Popovich moved the discussion on to the Downtown Edge 1, which is the area immediately around the Core area, and would have a more urban setting around the downtown. The transition to the neighborhood areas and the Edge should be open green space and contain more open spaces. He referred to the area having a height of 70 feet with a proposal to increase it to 72 feet, or six stories in height. This would be a 12' floor. The minimum height is 32 feet with 24' proposed. He explained that there is no setback for existing properties at this time, with a proposed setback of 5' or 10% of the lot width. Properties on Main Street or immediately adjacent to the Core have no setback. The rear yard setback exists at 0' and is proposed to be 10' with additional setbacks for lots abutting a residential zoning district. He said that the Build-to-zone has no change, and no change in the parking. The Floor Area Ratio also shows no change. He noted that the minimum lot area per dwelling unit exists at 800 square feet with a proposed 3,000 square feet. Downtown Edge 1 also has a Build-to-zone of 10% at the front lot line for a 590' side lot.

Mr. Quirk asked about density again, and verified that it would be reduced. Mr. Popovich said that was correct. They are moving out from the Core providing a little more space. He thinks 800 is a good number. In further response to Mr. Quirk, Mr. Popovich said there is a comprehensive list of everything downtown that is multi-family and staff can provide that list to him. The Marquis on Maple has 54 units at 800 square feet with no request for increased density.

Ms. Gassen commented that the downtown Core isn't about density anymore. They are not encouraging more residential units in that area. Mr. Popovich replied that the Core discourages residential use on the first floor, and is limited in height. It can be residential on the above floors. Ms. Gassen said it would make sense to restrict the type of building in the Core.

Mr. Kulovany said he thought the essence was to let the downtown be more quaint and smaller with the largest density at Edge 1, and a reduction of the density as they got closer to residential neighborhoods. Mr. Popovich replied that was correct.

Chairman Rickard commented that he thought the minimum lot area for Downtown Edge 1 was something closer to 2,000 square feet while leaving the transition area alone. He doesn't know if that's the right number. It seems restrictive. The heightened area is where they're looking for the bulk of the density to go. Mr. Popovich said if you

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want to go to the amount of units based on the size of the lots, not a lot of the properties are as big as the Marquis site.

Mr. Kulovany asked whether they would consider the Marquis and Main and Maple higher density. He said it might be interesting to see what these developers have done in other communities.

Mr. Popovich then moved to the description for Downtown Edge 2, noting that the maximum height is 60-70 feet, with a proposed height of 60' or 5 stories. The minimum height for that area is 32 feet with no change proposed, since it is close to the transition area. The side yard existing setback varies from 0'-5' with 5'-10% of the lot width proposed. The rear yard setback is 0'-20' with 10' proposed. There is no change in parking, the FAR, or the minimum lot area per dwelling unit. There is also no build-to-zone in this area.

Regarding Downtown Transition, Mr. Popovich said that the existing maximum height is 35'-70', and is a mixture of multi-family, single-family residential, etc. The proposed height is for 36' or three stories. He showed the area map, saying the existing minimum height is 32', and there is no proposal to change the minimum height. The street yard setback proposed is to change to 10' from 0'-20'. The side yard is 5' with 5'/10% of the lot width proposed. There is no change in parking or the Floor Area Ratio.

Mr. Kulovany asked whether this is part of the Comprehensive Plan now. Mr. Popovich replied that it is the current map as it now stands. Mr. Kulovany then commented about bed and breakfasts and inns that would not compete with the downtown restaurants. He suggested adding those into the area. Mr. Popovich said the Commission could make that recommendation.

Regarding the Downtown Transition District, Mr. Popovich explained that the existing maximum height is 35'-70', with a proposed height of 36' or three stories. He showed the area map saying the minimum existing height is 32 feet. The proposed street yard setback is 10' from the existing 0'-20'. The side yard setback varies from 0'-5' with a proposal for a side yard of 5'/10% of the lot width. The rear yard setback proposal is for 10' from the 0'-20' existing. As for Core uses in the downtown, the ground floor would be an active space consisting of retail, entertainment, food service, while upper floors could be used as multi-family residential or office space. No residential uses would be permitted on the ground floor.

Downtown Edge 1 uses are commercial, retail, office, entertainment, service, restaurant and residential.

Downtown Edge 2 also includes commercial, retail, office use with residential single-family and multi-family permitted, as well as home occupations, institutional use, civic use, bed and breakfasts. He noted that staff would have to review the definitions of lodging, inn, hotel, etc., and how they could be worked into this area. Mr. Kulovany said that they would not be looking for something like a Hampton Inn to move into that area, which Mr. Popovich agreed was not the intention.

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Ms. Hogstrom noted that in the downtown transition, art galleries and studios are not permitted and she asked why they were excluded. Mr. Popovich replied that they were going to consider more residential uses there. If the Plan Commission feels it is not an intensive use, it could be put back in. Ms. Hogstrom said that she thought some group-type uses are perfect for a downtown transition.

Ms. Gassen asked why apartments/condos were transitioned, and Mr. Popovich responded that based on the direction of the Comprehensive Plan, if they were to meet all the bulk requirements, including parking, height and density, they would be permitted. The intent was to encourage redevelopment. Ms. Gassen said she understands the concern to meet all the requirements, however is a little bit unclear about second floor only apartments. Ms. Leitschuh asked what she was referring to. Mr. Kulovany replied she's referring to the second floor only as a mixed-use development. Mr. Popovich said the intention of the Core is not to have residential on the first floor. They want something commercial on the first floor that will attract traffic.

Mr. Kulovany suggested that the actual zoning wording would have to be a great deal more detailed.

Ms. Leitschuh commented that special uses should be assumed as allowed in that district. Staff would look at the evaluation that would go before the Plan Commission other than the actual result of that process. Chairman Rickard added it would be on a case-by-case basis depending upon what is surrounding it.

Discussing the Prospect and Rogers area, Mr. Popovich said there are some properties that are going to remain fairly close to the existing use, while others might be rezoned to a transition area.

Mr. Quirk asked about finite changes from the current zoning to the downtown transition. He said it looks as though the downtown transition will move into an R-4 district. He asked whether north of the tracks between Warren and Rogers, extending down to Prospect would be zoned as R-4. Mr. Popovich replied that the area between Prospect and Rogers would be removed from the transition area and changed to an R-6 zoning. Similarly adjacent to Immanuel Residences the idea was to stop the downtown transition and convert those homes back to a single-family home classification. There are some properties that will remain close to original zoning classification, while others may change to downtown transition. Once a framework is established they will have to do additional research on the individual lots.

Mr. Popovich replied further about Prospect and Rogers saying they meet the 800 square feet at the 54 units per acre, which is the allowable density. Single family is determined by lot area coverage, and not square footage of living space. He said that right now this framework appears to be appropriate, and the Village Council will have to make that decision. This is not becoming law in September, but is a framework of what the Zoning Ordinance could look like. At that point notifications would have to be made to property owners and surrounding properties.

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Chairman Rickard said that to date he's only heard two property owners who appeared with concerns, and those concerns had already been addressed in the Plan. Mr. Popovich said that Staff has reached out to Downtown Management, the Economic Development Corporation, and any groups interested in the downtown area to get the word out to all organizations. Chairman Rickard said he expects they will be hearing from many more people before this is over.

Mr. Popovich said the Acadia building has entrances on the first floor to the residential portions above commercial uses.

Chairman Rickard referred to the Land Use Chart under Business, Professional Offices and Core. He asked whether those are permitted on the second floor and above. Mr. Popovich said there was some discussion about allowing offices on the first floor, which could be done now. Some offices would have a lot of traffic going in and out. The intensity could be similar to some restaurants. Ms. Leitschuh noted that some people had strong opinions regarding office uses, while others didn't. Chairman Rickard said he expected to see a "special use" on the first floor and "permitted uses" on the second floor and above.

Mr. Popovich said Downtown Management was concerned about the ability of property owners to fill a vacant space. It became more difficult to determine the activity of one office versus another office in terms of foot traffic. Mr. Kulovany recalled the Comp Plan meeting where discussion included the idea of people moving down the street and looking at retail stores, then suddenly finding themselves in front of a real estate office. The concern was that could cause the public to stop at that office use. Someone mentioned that Anderson's felt they were losing business because people weren't moving beyond Coldwell Banker to the bookstore.

Ms. Rollins said she noticed on the use table that the drive-thru facility had been a special use and then was eliminated. She questioned whether banks wanted to have the drive-in option. Mr. Popovich said drive-ins tend to create open spaces and remove street walls. The desire was to create a pedestrian oriented downtown area. Ms. Rollins said that the drive-in seemed to fit as a special use in a transition area.

Mr. Quirk asked how many uses currently exist in the Core and Transition that would not be permitted there. He said he could think of the Toon Funeral Home, and the automobile repair facilities that would not be permitted. Mr. Popovich replied the funeral home is not a permitted use in that district. The funeral home was permitted at some point. Ms. Leitschuh said the question is whether to allow a new funeral home in that area. The existing business would be grandfathered in as a lawful non-conforming use.

Mr. Kulovany asked whether the essence of downtown transition is pedestrian rather than auto oriented. Mr. Popovich responded that the impetus is behind making it pedestrian friendly. He noted that at one time there were gas stations in the downtown area and now there aren't any. He explained that as they get closer to the Core they want less green space and more density. Mr. Kulovany asked if that would be what is at

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the south side of Maple where there are businesses that look like residences. Mr. Popovich said that area is proposed to be Edge 2. If you look at Forest, south of Franklin, there are some single-family homes converted to offices that have some greenspace as well. If there is a business in that area, they don't want it to be high intensity, with no storefronts. Mr. Kulovany asked if someone could come to the Plan Commission for a variance on uses. Mr. Popovich noted use variances are not permitted. Mr. Popovich said they would have to come to the Plan Commission for a text amendment, which would change the Ordinance and the District. He explained that there is always room for change.

Mr. Quirk asked what category yoga studios or gyms fall into. Mr. Popovich said it would be personal improvement services and they are permitted in the downtown area. The transition includes barbershops and beauty salons that must be on the ground floor and not exceed 2,500 square feet. Personal improvement services/physical therapy would be a gym. He doesn't think those fall under medical or health practitioners.

Mr. Leitschuh gave a definition of what is included in personal improvement services as: uses that provide a variety of services associated with personal grooming, instruction and maintenance of fitness, health and well being. Typical uses include barbers, hair and health salons, health studios, martial arts studios, and businesses purporting to offer fortune telling or psychic services.

Mr. Popovich said they tried to figure out the key concepts and how uses fit into the Comprehensive Plan.

Mr. Kulovany asked about medical/dental health practitioners and physical therapists and whether they would fall into personal improvement. As for massage therapists, Mr. Popovich said they would be as an ancillary use to a salon to avoid inappropriate activities. Chairman Rickard said there's a sign on the east side of Main Street for massages all the time. Mr. Popovich said that could be done if they are a chiropractor, where massage is not the principal use. Mr. Kulovany asked if they would ever consider a therapeutic massage therapy as a permitted use.

Mr. Maurer asked if a methadone dispensary falls under medical health practitioners. Mr. Popovich said that it does. Mr. Maurer responded that between Forest and Main, west of Rogers, according to the current Code he could more easily walk into a methadone clinic than he could to get his dog's heartworm treatment. Mr. Popovich said veterinary clinics are a special use.

Mr. Popovich said that there have been five traffic studies done as part of the Comprehensive Plan on a neighborhood basis, but not one done for downtown. He was not sure if one was planned at this time for the downtown. In response to an inquiry, Mr. Popovich said he thought Acadia on the Green was sold out, the 922 Warren site has one more unit to sell, and he doesn't know about any of the new ones under construction. The Village does not see changes in apartment rental occupancy. He said that there is some discussion about a pedestrian crossover at the railroad tracks;

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however, that is not something that has been discussed with BNSF and is not likely to be seen in the near future.

Mr. Popovich addressed the subject of façade improvements for the downtown, which was a program in about 2007-2008. One of the high priority action items that the Council will review is the future of the downtown because the TIF is expiring and the special service area is expiring as well. The question arises as to what will happen downtown with items such as flowers and improvements.

Chairman Rickard asked about design issues, verifying that they will discuss that at the July 24th meeting.

Ms. Hogstrom asked if All Creatures Great and Small and Yoga have been advised about their potential site as a parking deck. Mr. Popovich said that is only a concept at this point and is merely an idea of looking at a parking deck north of the downtown. He said that similarly there are ideas about other areas in the Village that need attention such as 75th and Lemont, 63rd and Belmont, etc., but no decision has been made for those areas at this time.

Chairman Rickard then called for anyone in the audience who wished to ask a question or make a comment.

1. Scott Richards of Oak Tree Towers at 1130 Warren Avenue asked whether they are locked into six stories as maximum in the Village. He said he hoped they were. Mr. Popovich replied that the downtown business district has a 70' height limit with a 60' height limit in the transition area. Mr. Richards said he has lived in the Village about fifteen years now and he's worried about what he is seeing as far as the well-being of the town. He thinks they are hell bent on choking it. He keeps hearing "increased density, increased density". He asked when they'll get to the point where enough is enough. His biggest concern is traffic and very seldom does he hear the Board discussing traffic. He thinks they are at a point where it will be detrimental to the whole town's well being. It's hard to find parking downtown right now, with so many trains going through every day and shutting the downtown down every year for festivals. This is the only town he knows of that actually does that. He doesn't want to see losing the character of Downers Grove, the town itself. He thinks the Village is going in the wrong direction. He referred to the "monstrosity" going up at Main and Maple that looks more like a hospital than high end housing, and said that he understands people are trying to get out of their contracts for that building because of the other building going up immediately adjacent to it. He doesn't understand why no one seems to worry about any of this. He is dismayed at what he sees being discussed, and would like to see more concern about traffic. People will just stop going into the downtown area because of the traffic.

2. Jim Wilkinson, 1125 Black Oak and a member of Transportation and Parking Commission, said there is a traffic study being proposed for downtown. They just completed the fifth study for the unincorporated area on the west side. There is no timeline right now but one study is proposed. He noted that the 2-hour parking signs do

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not agree with the Ordinance and that has to be addressed. In addition new equipment will be installed downtown to monitor the traffic flow in the area. Regarding the parking garage, the Commission struggled with that issue as well so as not to restrict the height in the area so that a parking garage could be built. He said the dilemma is how to put in a parking structure in an established area. They also have touched on surface lots. He asked that they consider where the parking garage could go, which obviously would be the north side of town. He said the parking garage would have to fit in with the imitations or descriptions they have such as in DE1 or DE2. As for drive-thrus, such as U.S. Bank, it really isn't a drive-thru but is more of a mini-parking lot and a cut-through for pedestrians. That has been grandfathered in at that location.

Someone on the Commission explained that if U.S. Bank was to close and another bank came in within a six-month period they could use the drive-thru. More than six-months out and the drive-thru would no longer be permitted without receiving approval.

Mr. Wilkinson said they have been talking about building upward. But he asked what about going down for parking in a basement level. Mr. Popovich said the building code restricts how deep you can go. It is not a zoning regulation. Some of the buildings under construction have below-grade parking.

Mr. Wilkinson also commented that at some point it would be good to have a grocery store downtown which would also require parking.

Mr. Wilkinson replied to a question that TAP often gets requests from residents regarding changes in parking in neighborhoods, such as high school students parking in residential neighborhoods. He said with regard to multi-family housing if there is only 1.4 parking spaces per unit, but two drivers/cars in the unit, that will affect street parking. The construction at Main and Maple has resulted in the loss of parking spaces on Main Street to accommodate some of the workers who are involved in the construction on Maple. He also referred to the 2-hour parking in the downtown with employees parking on the street all day on a Saturday. Mr. Popovich said one of the issues as well is enforcement of employees parking on the street.

Mr. Kulovany said that Yorktown shopping center is about 3-4 blocks long and people will park and walk in the mall. But they won't walk a block from the parking garage to the downtown stores. It is a behavior change. Mr. Kulovany asked whether TAP has looked at synchronized lights on Main Street. Mr. Wilkinson said that has not come before them for review. It might be part of Public Works, and they also would have to work it through with the railroad.

Mr. Kulovany then asked whether TAP is involved in Metra, and Mr. Wilkinson said they are not. He said that they were asked if they favored a pedestrian overpass at the railroad tracks.

Mr. Popovich said that from 1997 to present there were about 600 additional residential units added downtown, with 500 additional public parking spaces, and 400 private

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parking spaces. Other dynamics that could affect parking would be the change to driverless cars.

Chairman Rickard said he spends more time than he likes to admit in the parking deck. During normal business hours the deck is fairly well filled. There's a change after working hours and on the weekends. He thinks some of the issue with using the parking deck is laziness.

A commissioner asked about the 600 additional units, and asked how many are currently under construction. Mr. Popovich said that there are about 300 currently under construction. Mr. Maurer noted then that half of the residential units added over the last 20 years are under construction right now.

3. Mr. Richards who resides at Oak Tree Towers said that their residents have been allowed to use the AT&T south side parking. If something happens to that, there will be about 30 residents of Oak Tree without parking availability.

Mr. Popovich said Staff would obtain additional information about densities and uses in the downtown and allowable densities in other communities.

Mr. Kulovany asked what the reasoning is behind the Council wanting greater density downtown. Mr. Popovich said if there are more people downtown more businesses would remain open. They want to keep a quaint feel to the downtown, with density around the edge of residents who would frequent the downtown businesses.

Ms. Hogstrom said that they recently approved demolishing a building for St. Joseph's parish at Franklin and Main to allow for an additional surface parking area. There could be an opportunity for residents to use those parking areas during off hours.

Chairman Rickard called for a Motion to continue the meeting.

**Mr. Kulovany moved to continue the meeting for Case 16PLC-0019 to July 24, 2017, seconded by Ms. Gassen.
All in favor. Motion carried.**

Chairman Rickard said he might not be present for that meeting.

**Mr. Quirk moved, seconded by Mr. Kulovany to adjourn the meeting.
All in favor. The Motion carried.**

Chairman Rickard adjourned the meeting at 9:05 PM.

Respectfully submitted,

Tonie Harrington,
Recording Secretary.

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VILLAGE OF DOWNERS GROVE
COMPREHENSIVE PLANNING AD HOC COMMITTEE MEETING

PUBLIC WORKS BUILDING
5101 WALNUT AVENUE
FEBRUARY 20, 2017 - 7:00 P.M.

Chairman Gorman called the February 20, 2017 meeting of the Downers Grove Comprehensive Plan Ad Hoc Committee meeting to order at 7:06 p.m. and led the meeting with the recital of the Pledge of Allegiance.

ROLL CALL

PRESENT: Chairman Gorman, Ms. Acks, Ms. Hogstrom, Ms. Majauskas, Mr. Wilkinson

ABSENT: Members Kalina and Luka

STAFF: Community Development Director Stan Popovich

VISITORS: John Houseal with Houseal Levigne Associates, 188 W. Randolph, Chicago; Amy Gassen, 5320 Benton, Downers Grove; Linda Kunze, 933 Curtiss, Downers Grove; Rich Kulovany, 6825 Camden Rd., Downers Grove; Don Rickard, 4735 Main St., Downers Grove; Graham Mosey, 4925 Forest Ave., Downers Grove; Steve Kuhn, 4925 Forest Ave., Downers Grove

APPROVAL OF MINUTES – JANUARY 16, 2017

A typographical error was noted on page 5, second paragraph. On the same page, under Public Comment, fourth line, delete the word “fair” and insert “fare”. **MINUTES OF THE JANUARY 16, 2017 MEETING WERE APPROVED ON MOTION BY MR. WILKINSON, SECONDED BY MRS. ACKS. MOTION CARRIED BY VOICE VOTE OF 5-0.**

REVIEW DOWNTOWN DEVELOPMENT REGULATIONS

Community Development Dir. Stan Popovich referenced a memo prepared for members, a copy of the functional sub-areas and some emails. He reviewed the latest updates to the plan. However, he pointed out that tonight’s meeting would focus on the development regulations and how they would impact the development throughout the downtown area and how each of the sub-areas would function in terms of setbacks and bulk requirements. Those changes, he clarified, would be made at the village council level.

Mr. John Houseal, with Houseal Associates, reviewed what changes were made between the boundaries and what changes could not be made, i.e., those between the three primary districts. Ms. Acks, referring to the northwest corner of Maple and Main, voiced concern that a six-story

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building could be located there, creating a canyon. She further expressed her concern that the parking along the railroad tracks, north of Burlington, could be removed and replaced with six-story buildings and block out the downtown. She believed a cap in height should be required.

Mr. Houseal explained that the ad hoc committee could reduce the height at the northwest corner of Maple and Main (up to Grove Street) if desired. Ms. Acks, along with others, preferred to expand the northwest corner of Main and Maple, as well as the two parcels along the tracks (east and west of Main Street) to become part of the Downtown Core.

Mr. Popovich reviewed the other changes that were made from the last meeting. The group also discussed cell towers in the downtown area.

PUBLIC COMMENT

Mr. Graham Mosey, 4925 Forest Ave., Downers Grove, referenced his emails as well as another neighbor's email and shared his frustration that this committee was having its conversation and putting forth recommendations to council only to find out that the village council has made its decisions on district lines. Mr. Mosey stated his business has been in the village for forty years and the reason he purchased his business on Forest Avenue was because years ago the concept was to take off the load from the Downtown Central Business District and to bring it to secondary areas, such as Forest Avenue or Warren Avenue and to bring in service businesses, excluding retail, so that the downtown could have a vibrant retail area. He voiced frustration that council removed part of a footprint from Main Street at the corner of Maple and Main where parking existed and some very good opportunities existed for retail but instead residences were being constructed.

He said he was before this group because the footprint was being reduced, the real estate was being compromised with less services, less businesses and less opportunities for more multi-family high density to be developed in the downtown. Mr. Mosey summarized how Forest Avenue changed over the years and the variety of building types currently found on Forest Avenue. For the village to say that development could not increase in height was irresponsible and to make such changes on Forest Avenue would be economically impacted. He stated it was unfortunate that this committee did not have a say in such changes.

Mr. Mosey further explained he purchased his property with the knowledge that he could develop on Forest Avenue but that the proposed change hindered it and made it economically not feasible. He praised the downtown when a nearby business owner purchased three bankrupt properties with the idea that at some point all of the businesses could be doing a nice development on Forest Avenue. He was in the process of contacting other nearby property owners about this proposed change.

In response, Ms. Majauskas asked Mr. Mosey what he did want, wherein he stated he would not change anything on Forest Avenue. As an aside, he noted that prior to the changes being proposed, he and another property owner and tenant were discussing a proposed development with enclosed self parking for not only the building but "free range parking." His development would have been a mixed-use building.

Ms. Linda Kunze, Downtown Development Corporation, reiterated what the above speaker had said and also liked the comments that were made in Mr. Hinkel's letter to this committee – that some of the new Internet companies are hiring more employees because they want to be downtown and have

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access to Chicago as well as access to nearby amenities in the village. She supported having more offices on Forest Avenue.

Mr. Mosey returned and explained that people will begin to ask what was the reason for the village council to make such a proposal and he stated his job was to get in front of council to find out what was the reason and to make council responsible for that reason. Of the remaining businesses on Forest Avenue, Mr. Mosey explained they were tired because they could not afford to redevelop individually. He honestly thought that Forest Avenue would not have been discussed or compromised they way it had been and believed Forest Avenue should have been left alone.

Mr. Rich Kulovany, 6825 Camden, understood that Mr. Mosey's contention was tall buildings constructed on Forest Avenue as well as across the street and that the shorter buildings did not match the character of what already existed. Mr. Mosey returned and stated it did not match the character nor was it economical when considering the setbacks and the lower height because the ability for parking was being taken away – the taller the structure, the more ability there was to park internally. The lower buildings had no ability for parking. Mr. Mosey commented that the east side of Main Street could be compromised at some point as well, given that parking was an issue there.

Continuing, Mr. Mosey stated the downtown was slowly being “strangled” which made the transition area more important. However, now the transition area was being “strangled” especially on Forest Avenue. Asked if he needed 70 feet to make it feasible, Mr. Mosey indicated not necessarily so and proceeded to explain what his conversation was with staff from 20 years ago.

RECOMMENDATION OF DOWNTOWN DEVELOPMENT REGULATIONS

Mr. Popovich summarized for those in attendance all of the committees and commissions the plan was reviewed by, including village council. Next steps were explained as well as the motions the committee should consider forwarding to the Village Council: 1) motion to review the northwest corner of Maple and Main Streets, running up to Grove Street – to bring the properties into the Downtown Core; 2) motion to review two properties on Main Street, north of Burlington, located on the east and west sides of Main; and 3) motion to review Franklin Street, 7 blocks south on the east side of Forest Avenue.

MOTION BY MR. WILKINSON TO RECOMMEND THE DEVELOPMENT REGULATIONS REGARDLESS OF WHAT THE ZONING MAP LOOKS LIKE.

SECONDED BY MS. HOGSTROM.

MOTION CARRIED BY VOICE VOTE OF 5-0.

MOTION BY MS. ACKS THAT THE PLAN COMMISSION REVIEW THE NORTHWEST CORNER OF MAPLE AND MAIN STREETS, UP TO GROVE STREET AND TO CONSIDER BRINGING THE PROPERTIES INTO THE DOWNTOWN CORE.

SECONDED BY MS. MAJAUSKAS.

MOTION CARRIED BY VOICE VOTE OF 5-0.

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Discussion then followed regarding the two parking lots located on the east and west sides of Main Street along the railroad tracks. Comments followed that the parcels should be an expansion of the Downtown Core area from Main to Washington. Other comments followed that the train station should be included in the Downtown Core also.

Discussing the west side of Main Street, consensus was to leave it as Downtown Edge 1.

MOTION BY MS. ACKS THAT THE PLAN COMMISSION CONSIDER THE EXTENSION OF THE DOWNTOWN CORE TO THE AREA OF THE TRAIN STATION AND THE PARKING LOT NEXT TO IT (BETWEEN MAIN AND WASHINGTON, BURLINGTON AND THE TRAIN STATION).

SECONDED BY MS. MAJAUSKAS.

MOTION CARRIED BY VOICE VOTE OF 4-1 (NAY: MR. GORMAN)

Asked what members thought about Forest Avenue (east side) up to Franklin Street and it being currently depicted as Downtown Transition, Mr. Wilkinson felt it should remain in character with what currently existed. On the Main Street side he noted the various businesses that were currently there and said they could be incorporated into the type of development Mr. Mosey discussed above, i.e., service organizations that could be anchored. The Franklin to Forest to Main area should be kept as is. The taller buildings can be constructed with the caveat that parking be created underneath the building.

Ms. Majauskas, however, pointed out that with the new condo building in the area now, the village began to make the area “big buildings”, other than the houses that were businesses, and so, to cut it off mid-block did not make sense. Further dialog followed.

MOTION BY MR. WILKINSON THAT THE PLAN COMMISSION CONSIDER A RECOMMENDATION TO CONSIDER DOWNTOWN EDGE 2 ZONING FOR THE EAST SIDE OF FOREST AVENUE UP TO FRANKLIN.

SECONDED BY MS. ACKS.

MOTION CARRIED BY VOICE VOTE OF 5-0.

Mr. Popovich reviewed the next steps the comprehensive plan would follow. He thanked all of the members for volunteering their time and effort on the village’s comprehensive plan.

ADJOURNMENT

THE MEETING WAS ADJOURNED AT 8:14 P.M. ON MOTION BY MR. WILKINSON, SECONDED BY MS. MAJAUSKAS. MOTION CARRIED BY VOICE VOTE OF 5-0.

Respectfully submitted,
 /s/ Celeste K. Weilandt
 (As transcribed by MP-3 audio)

Approved 02/20/17

VILLAGE OF DOWNERS GROVE
COMPREHENSIVE PLANNING AD HOC COMMITTEE MEETING

PUBLIC WORKS BUILDING
5101 WALNUT AVENUE
JANUARY 16, 2017 - 7:00 P.M.

Chairman Gorman called the January 16, 2017 meeting of the Downers Grove Comprehensive Plan Ad Hoc Committee meeting to order at 7:03 p.m. and led the meeting with the recital of the Pledge of Allegiance.

ROLL CALL

PRESENT: Chairman Dave Gorman, Carine Acks, Ed Kalina, John Luka, Daiva Majauskas
(7:15 p.m.) Jim Wilkinson

ABSENT: Members Irene Hogstrom

STAFF: Community Development Director Stan Popovich, Senior Planner Rebecca Leitschuh

VISITORS: John Houseal with Houseal Lavigne Associates, 188 W. Randolph, Chicago; Amy Gassen, 5320 Benton, Downers Grove; Linda Kunze, 933 Curtiss, Downers Grove; Rich Kulovany, 6825 Camden Rd., Downers Grove; Don Rickard, 4735 Main St., Downers Grove

APPROVAL OF MINUTES – NOVEMBER 14, 2016

MINUTES OF NOVEMBER 14, 2016 WERE APPROVED ON MOTION BY MR. LUCA, SECONDED BY MR. WILKINSON. MOTION CARRIED BY VOICE VOTE OF 5-0.

INTRODUCTION OF DOWNTOWN DEVELOPMENT REGULATIONS

Mr. John Houseal, with Houseal Lavigne Associates, recalled for the commissioners how the Downtown Development Regulations were approached, i.e., built on the understanding of the previously approved plan for the downtown which created three previously defined districts: 1) the Downtown Core, 2) the Downtown Edge, and 3) the Downtown Transition. Characteristics of each district followed.

Mr. Houseal discussed that tonight's discussion would focus on the content/breakdown of guidelines with the next meeting to discuss the procedures by which regulations would be administered or relief granted. Community Development Director, Mr. Popovich, added that with the regulations, the goal was to have the this Ad hoc Committee and the Plan Commission review the regulations and if the Village Council accepted the findings then staff would draft regulations to

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incorporate into the village's zoning ordinance. Input and discussion from tonight's meeting was important.

REVIEW DOWNTOWN CORE

A review of the Down Core Area followed on the overhead, noting there were many two- and three-story buildings at the sidewalk line, a few one-story buildings, and no setbacks between buildings. Referring to the plan diagram, Mr. Houseal summarized that according to the current plan, building height could be two to three stories. Buildings along the sidewalks had no setbacks and mixed-use was recommended (retail or commercial on ground floor with residential or office on the upper floors).

Per Mr. Houseal, the new maximum building height proposed for the Core area would be 40 feet or three stories, based off of a 12- to 15-foot first floor and a 10- to 12-foot upper floor. Setbacks would remain the same. Proposed uses for the ground floor included commercial, retail, entertainment, and restaurant-type uses. Proposed uses for the upper floors included multi-family residential, office, or service uses. No residential uses would be permitted on the ground floor.

Staff clarified that current drive-thrus were grandfathered in and if the use changed they would have to be removed within a certain period of time. If the use remained with a new tenant, the drive-thru could be used. However, Mr. Houseal recommended that the village continue to not allow drive-thrus in the Core area. He pointed out that parking would probably be the largest driver of development intensity in this area and recommended no changes to the village's parking standards either, as they were very good. (Ms. Majauskas arrives.)

Reviewing the use table depicted on Page 6, Mr. Popovich asked for comments regarding a 24-foot high, two-story minimum building height. No negative comments followed. Further review of the table followed. As for a constructing a building that came to the lot line but had an opening/courtyard or an outdoor dining area, Mr. Houseal stated the space could be factored in but the key was to have some sort of built structure next to the sidewalk, such as a fence.

Discussing office use, Mr. Popovich stated that office was a permitted use on the ground floor. However, Ms. Majauskas preferred to change that, believing that office should be allowed on the second floor only. Mr. Wilkinson asked if there was an option to promote retail without prohibiting office use, should vacancies start occurring, wherein Mr. Houseal suggested making retail and restaurant use permitted uses as of right and make office use a special use with the appropriate standards.

PUBLIC COMMENT

Ms. Linda Kunze, 933 Curtiss Street, Downtown Management, appreciated the comments, stating the downtown businesses "were pleading" with her about this, commenting that Anderson's Bookstore was thinking of leaving because Berkshire Hathaway moved next to Caldwell Banker which has hurt Anderson's sales significantly. She stated that pedestrians stopped walking at Caldwell Banker and would not travel beyond that point. Ms. Kunze thought the idea of a special use may be fine and could also give the village an opportunity to think about the use if retail could not be obtained. At the same time though, Ms. Kunze asked whether the change would discourage the outdoor cafes currently. Mr. Popovich explained that as with any existing cafes, the building

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line would not have to be moved and outdoor seating can be placed in the right-of-way with the proper license agreement. Anything new, however, would have to meet the 0 to 5-foot setback requirement. The Cellar Door was used as an example by staff.

Mr. Rich Kulovany, 6825 Camden, Downers Grove, asked if there was consideration for roof-top parking, citing a grocery store using it in Glenview, to which Mr. Houseal indicated other cities were doing that same thing and that currently, in the village, it was not prohibited. Mr. Popovich indicated that the maximum height for such scenario would be to the “flat part of the roof” with the screening of mechanicals or cars being required. Asked if there was consideration to have a single-family use above a store/restaurant, Mr. Popovich explained that single-family use would need multiple units above to be considered a multi-family unit in the district and so it would be permitted in the village’s code.

Asked if outdoor rooftop seating could be permitted currently or in the future, Mr. Popovich explained it could with the proper building code compliance.

REVIEW DOWNTOWN EDGE -1

Mr. Houseal recalled this area was initially one sub-area under the Downtown Edge, but now delineated as Downtown Edge 1 (DE-1) and Downtown Edge 2 (DE-2). He recalled members had talked about having the greater intensity located closer to the Core area and the slightly less intensity located toward the Transitional area. Two districts were then created: DE-1 and DE-2, which were very similar districts except for their allowed height difference. An overview of the two districts were shown on the overhead. Current building height, setbacks and uses for the Downtown Edge were described.

Mr. Houseal then shared that the proposed building height maximum for the DE-1 District would be 72 feet to allow six stories, approximately 12 feet per floor. Proposed setbacks would be 0 feet. Proposed side yard setbacks would be a minimum of 5 feet or minimum of 10% of lot width, whichever was greater. Proposed side yard setback for properties adjacent to the Downtown Core District would be 0 feet. Proposed rear yard setbacks would be 10 feet throughout the entire district and special setback standards would apply if a building abuts the residential districts.

A variety of proposed uses followed. Mr. Popovich also added that since this district was so close to the Core district, staff wanted to incorporate some of the characteristics of the Core to this district and for the DE-2 district, characteristics from the Transition/residential area would be incorporated.

REVIEW DOWNTOWN EDGE – 2

Mr. Houseal noted that this district was a bit further away from the Downtown Core area and was less intense. He pointed out that this area was where setback requirements were beginning to emerge and height was coming down. Much of the bulk standards were the same as DE-1 except the height was being decreased to 1 to 5 floors with maximum of 60 feet. Setbacks were proposed at 10-feet throughout the entire district to allow for some landscaping and trees. Proposed rear setbacks were the same as DE-1. A wide range of uses still existed.

Ms. Majauskas expressed concern about what was not written in the comprehensive plan. While she appreciated all the new developments that would come, she voiced concern that another parking

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lot was being removed and questioned how businesses were suppose to remain and thrive when no one could park. Convenience and accessibility were important. Mr. Houseal explained the challenge was that buildings occupy the majority of the site and cannot provide the parking for all of their uses. In order for any downtown to be successful, he explained the municipality has to provide public parking whether surface or structure. He pointed out that it is typically the Village that needs to identify locations for future parking.

However, one member believed surface lots were moving toward development and an additional parking structure was needed for the downtown area, but the ideal uses had to draw people to the downtown. Agreeing, Mr. Houseal cited other cities that had full parking structures and that more were being constructed. He elaborated that if businesses could get their employees to park in a parking structure versus in front of their business, it would free up a significant amount of parking.

Mr. Kalina asked if the DE-1 on Forest Avenue could be pushed 100 to 200 feet north, or even to Franklin Street. Mr. Popovich indicated the Village Council delineated the subareas, but staff would follow up. Referring to zoning and its impact on a parking structure in the DE-1 area, someone asked if the structure would have to conform to the existing, as proposed, or could the village seek an exception. Mr. Houseal explained the village could seek a variation with the hardship being that it is not a typical standard DE-1 use because the village is providing parking for the entire downtown so the village cannot be bound by existing zoning regulations because it is a benefit for the entire three districts, not just the village's private use. So it becomes a hardship because no one else is in the same situation and it is entirely unique.

Dialog followed regarding some map discrepancies between the pages of the comprehensive plan relating to buildings located at Rogers and Washington which Mr. Houseal would fix. Additionally, there were concerns about building setbacks at the northwest corner of Maple and Washington and that if the nearby church removed its parking lot there could be a six-story building on the corner with three of the corners being "quaint." Staff pointed out that two newer developments on Maple Avenue were located nearby and were at the property line currently. Another member voiced concern that a six-story building could be constructed katty-corner from a residential home at the southeast corner Maple and Washington. Mr. Popovich pointed out that if the committee thought the church property and the property north of it should be in DE-2 district, they could discuss it. After discussing the matter, members believed there should be a setback due to the height and recommended the church parking lot designation should be changed from DE-1 to DE-2.

PUBLIC COMMENT

Ms. Linda Kunze, with Downtown Management, appreciated the comments about parking and agreed parking was still an issue in the downtown. She expressed the challenges of keeping business owners when they complain that they have no customers or the customers complain about parking. While the village was helpful, Ms. Kunze believed it was time for the village to consider taking some older homes and converting them to a parking structure. She also voiced concern that some of the mayors attending conferences were not even being encouraged to build parking decks due to the new variety of ride-sharing occurring with services like Uber, etc.

Mr. Rich Kulovany agreed that the above corner should be redrawn to be included in the DE-2 district because the Main and Maple development was 70 feet in height and the roof at Marquee on Maple was 59-1/2 feet. The proposal would allow for a slight step down.

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REVIEW DOWNTOWN TRANSITION

Mr. Houseal referenced this district and noted those areas where no front setbacks existed, those areas where setbacks were required, and those areas that had 25 ft. front setbacks. Current height requirements were referenced. Proposed for this district were buildings with a maximum height of 36 feet or three stories in height. Also proposed was a 20-foot front street setback throughout the district except for properties fronting on Main Street, which would have a 10-foot setback. Proposed side setbacks would be five feet or 10% of lot width. Special side setbacks would apply when abutting against a residential district. Proposed rear setbacks would be 20 feet throughout the entire district except for properties fronting on Main Street, which would have a 10 foot required setback. Proposed uses include single, multi-family, office, service, home occupation and institutional but on a residential scale and with residential compatibility. No retail or restaurant uses would be allowed.

Mr. Popovich then raised the topic of minimum lot area per dwelling unit and asked whether the village should be concerned with how many units are in a development if a developer can provide parking. Mr. Luka indicated that the village should care about it because the village did not want 100 square foot units being constructed. Then conversation followed regarding the rage with tiny houses among the Millennial market as well as the range of unit sizes being constructed by developers for the merging of the Millennials and the retirees, who both wanted walkability, rentability (not own homes) and to have nearby amenities. One member cautioned that the village should ensure that its rentable units were a saleable size in order to convert them to ownership units, if necessary, since it would give the village more options to people.

PUBLIC COMMENT

Mr. Kulovany recommended under “Usage” to add Inns and Bed & Breakfasts. Regarding the Millennial comments, Mr. Kulovany stated he was an Uber driver and shared some of the conversations he had while driving Millennials, who basically do not support owning cars in the city when a person can get to work for a relatively cheap fare. Millennials also rent cars and rent units because they do not want to fix anything. He stated that Millennials have a different mind set.

Further discussion followed how the Millennial demographics will change once children come into the picture and, as a demographic group, will eventually move out to the suburbs.

REVIEW DESIGN GUIDELINES

Mr. Popovich depicted various buildings in the village on the overhead, pointing out that their design was based off of the village’s Pattern Book. He described how the various buildings are broken up, the materials used on them, and the rhythm of the buildings, i.e., window patterns, corner treatments, and cornice/roof lines. He stated that EFIS, CMU brick, and vinyl siding are not encouraged in the village. Per a question, projecting signage or awnings can encroach into the right-of-ways but not balconies. Staff administers these guidelines.

Mr. Houseal explained that the intent of the guidelines is to use them for the different types of components or features the village anticipates addressing. However, once the district regulations

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are put into place, they are to be used as a reference. He recommended crafting design guidelines for each of the separate districts discussed above so developers know what to produce.

Asked if he has seen developers go green on their own or whether they need incentives, Mr. Popovich explained that he has seen developers go green when detention or stormwater practices are required. Details followed. Mr. Houseal also shared what he has seen in the field.

Of the three residential developments being constructed in the village, a question was asked whether the village had to change any of the designs wherein Mr. Popovich indicated that the designs were “tweaked” a bit, citing some of the design changes that were made at the Main and Maple project. Mr. Popovich inquired whether the guidelines should continue to be used as guidelines or whether they should be converted to regulations. He explained that currently, they are guidelines, whereby staff can enforce them. If they are regulations that require specific compliance, a review board would be needed. A review board could require developers spend more time to go through the process to receive approval from the board.

Members seemed to favor keeping the guidelines in place versus regulations. Mr. Houseal believed the guidelines needed to be drafted for each of the districts and the guidelines appeared to be successful currently. He reiterated that the guidelines were not meant to dictate architecture or dictate building design but to guide development to ensure the desired downtown character and sense of place is met.

PUBLIC COMMENT –

No additional public comment was provided.

Mr. Popovich closed the discussion by summarizing that he will revise the guidelines according to the changes discussed tonight and then at the next meeting he will discuss uses and procedures. The next meeting will be scheduled for February 20, 2017 at the Public Works Building.

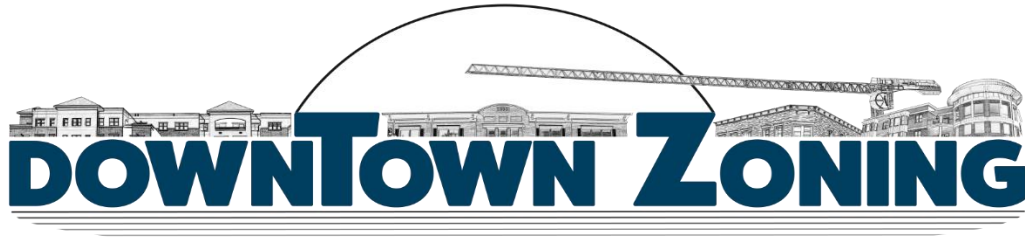
ADJOURNMENT

THE MEETING WAS ADJOURNED AT 9:10 P.M. ON MOTION BY MR. LUKA, SECONDED BY MRS. ACKS. MOTION CARRIED BY VOICE VOTE OF 6-0.

Respectfully submitted,

/s/ Celeste K. Weilandt

(As transcribed by MP-3 audio)



Downtown Business District  Downtown Core District

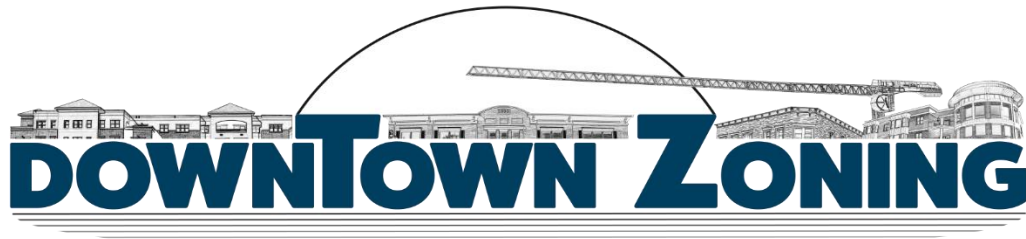
Regulation	Current	Proposed
	Downtown Business (DB)	Downtown Core (DC)
How tall can my building be?		
Maximum Height	70 feet	40 feet w/ a max of 3 stories
Minimum Height	32 feet	24 feet w/ a max of 2 stories
How close can I build to my property line?		
Front	0 feet	0 feet
Side	0 feet ¹	0 feet
Rear	0 feet ¹	0 feet
How many residential units (apartments or condos) can be on my property?	54 per acre ²	48 per acre ⁴
How many parking spaces would be needed for each apartment or condo?	1.4 per apartment ³	1.4 per apartment ³
How many parking spaces would I need for a retail tenant?	0 parking spaces	0 parking spaces
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Shop, restaurant, dry cleaner, art studio, hair salon, bank, day care , office, apartment, condo	Shop, restaurant, dry cleaner, art studio, hair salon, bank, office, apartment, condo

¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 800. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 900. Round the number down.



Downtown Business District → Downtown Edge-1 District

Regulation	Current	Proposed
	Downtown Business (DB)	Downtown Edge-1 (DE-1)
How tall can my building be?		
Maximum Height	70 feet	72 feet w/ a max of 6 stories
Minimum Height	32 feet	24 feet w/ a max of 2 stories
How close can I build to my property line?		
Front	0 feet	0 feet
Side	0 feet ¹	5 feet or 10% of lot width ⁴
Rear	0 feet ¹	10 feet ¹
How many residential units (apartments or condos) can be on my property?	54 per acre ²	62 per acre ⁵
How many parking spaces would be needed for each apartment or condo?	1.4 per apartment ³	1.4 per apartment ³
How many parking spaces would I need for a retail tenant?	0 parking spaces	0 parking spaces
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Shop, restaurant, dry cleaner, art studio, hair salon, bank, day care, office, apartment, condo	Shop, restaurant, dry cleaner, art studio, hair salon, bank, day care, office, B&B, townhome , apartment, condo,

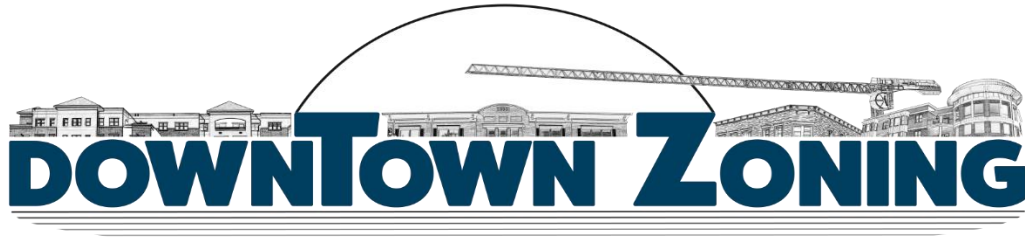
¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 800. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ Not applicable if adjacent to Downtown Core or along Main Street.

⁵ To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 700. Round the number down.



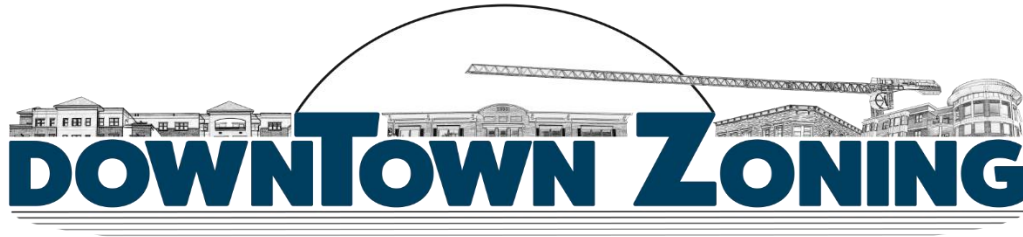
Downtown Business District  Downtown Edge-2 District

Regulation	Current	Proposed
	Downtown Business (DB)	Downtown Edge (DE-2)
How tall can my building be?		
Maximum Height	70 feet	60 feet w/ a max of 5 stories
Minimum Height	32 feet	No minimum
How close can I build to my property line?		
Front	0 feet	10 feet
Side	0 feet ¹	5 feet or 10% of lot width
Rear	0 feet ¹	10 feet ¹
How many residential units (apartments or condos) can be on my property?	54 per acre ²	54 per acre ²
How many parking spaces would be needed for each apartment or condo?	1.4 per apartment ³	1.4 per apartment ³
How many parking spaces would I need for a retail tenant?	0 parking spaces	0 parking spaces
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Shop, restaurant, dry cleaner, art studio, hair salon, bank, day care, office, apartment, condo	Shop, restaurant, dry cleaner, art studio, hair salon, bank, day care, office, B&B, townhome , apartment, condo

¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 800. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.



Downtown Business District → Downtown Transition District

Regulation	Current	Proposed
	Downtown Business (DB)	Downtown Transition (DT)
How tall can my building be?		
Maximum Height	70 feet	36 feet w/a max of 3 stories
Minimum Height	32 feet	No minimum
How close can I build to my property line?		
Front	0 feet	20 feet ⁴
Side	0 feet ¹	5 feet or 10% of lot width
Rear	0 feet ¹	20 feet
How many residential units (apartments or condos) can be on my property?	54 per acre ²	25 per acre ⁵
How many parking spaces would be needed for each apartment or condo?	1.4 per apartment ³	2 per apartment ³
How many parking spaces would I need for a retail tenant?	0 parking spaces	Varies based on tenant
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Shop, restaurant, dry cleaner, bank, art studio, beauty salon, day care, office, apartment, condo	Single-family, duplex, townhome, apartment, condo, office, day care, B&B, small art studio or beauty salon

¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 800. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ Along Main Street, the required setback is 10 feet.

⁵ To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 1,700. Round the number down.



Downtown Transition District  Downtown Edge-1 District

Regulation	Current	Proposed
	Downtown Transition (DT)	Downtown Edge-1 (DE-1)
How tall can my building be?		
Maximum Height	60 feet	72 feet w/ a max of 6 stories
Minimum Height	No minimum	24 feet w/ a max of 2 stories
How close can I build to my property line?		
Front	10 feet	0 feet
Side	5 feet	5 feet or 10% of lot width ⁴
Rear	20 feet	10 feet ¹
How many residential units (apartments or condos) can be on my property?	25 per acre ²	62 per acre ⁵
How many parking spaces would be needed for each apartment or condo?	2 per apartment ³	1.4 per apartment ³
How many parking spaces would I need for a retail tenant?	Varies based on tenant	0 parking spaces
What are some typical uses that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Single-family, duplex, townhome, apartment, condo, office, day care, small art studio or beauty salon	Townhome, apartment, condo, office, day care, art studio, beauty salon, shop, restaurant, dry cleaner, bank, B&B

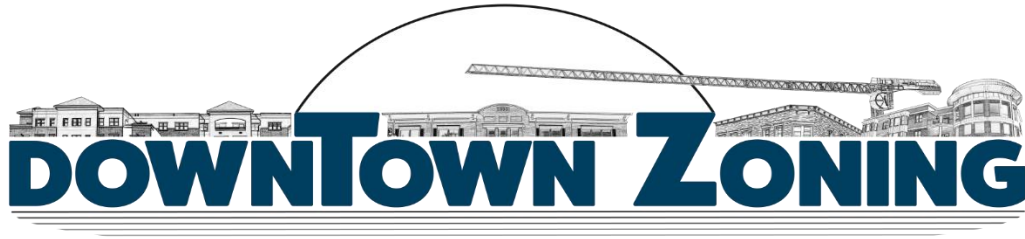
¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 1,700. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ Not applicable if adjacent to Downtown Core or along Main Street.

⁵ To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 700. Round the number down.



Downtown Transition District Downtown Edge-2 District

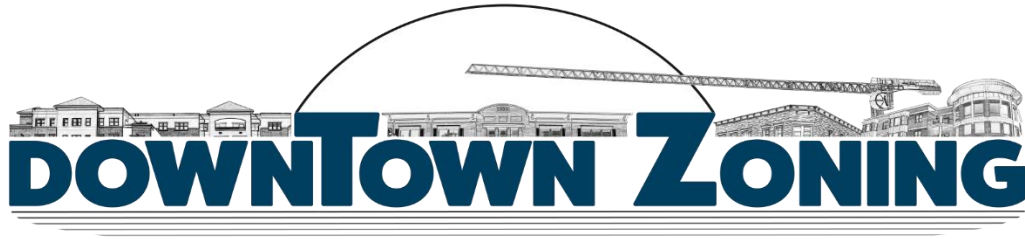
Regulation	Current	Proposed
	Downtown Transition (DT)	Downtown Edge-2 (DE-2)
How tall can my building be?		
Maximum Height	60 feet	60 feet w/ a max of 5 stories
Minimum Height	No minimum	No minimum
How close can I build to my property line?		
Front	10 feet	10 feet
Side	5 feet	5 feet or 10% of lot width
Rear	20 feet	10 feet ¹
How many residential units (apartments or condos) can be on my property?	25 per acre ²	54 per acre ⁴
How many parking spaces would be needed for each apartment or condo?	2 per apartment ³	1.4 per apartment ³
How many parking spaces would I need for a retail tenant?	Varies based on tenant	0 parking spaces
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Single-family, duplex, townhome, apartment, condo, office, day care, small art studio or beauty salon	Townhome, apartment, condo, office, day care, art studio, beauty salon, shop, restaurant, dry cleaner, bank, B&B

¹ If your property is adjacent to a residentially used building, there are additional setbacks.

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 1,700. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 800. Round the number down.



Downtown Transition District → Downtown Transition District

Regulation	Current	Proposed
	Downtown Transition (DT)	Downtown Transition (DT)
How tall can my building be?		
Maximum Height	60 feet	36 feet w/ a max of 3 stories
Minimum Height	No minimum	No minimum
How close can I build to my property line?		
Front	10 feet	20 feet ⁴
Side	5 feet	5 feet or 10% of lot width
Rear	20 feet	20 feet
How many residential units (apartments or condos) can be on my property?	25 per acre ²	25 per acre ²
How many parking spaces would be needed for each apartment or condo?	2 per apartment ³	2 per apartment ³
How many parking spaces would I need for a retail tenant?	Varies based on tenant	Varies based on tenant
What are <u>some typical uses</u> that can be located in my building? For a full list click here: https://tinyurl.com/yc388z2s	Single-family, duplex, townhome, apartment, condo, office, day care, small art studio or beauty salon	Single-family, duplex, townhome, apartment, condo, B&B , office, day care, small art studio or beauty salon

² To figure out the specific number of units allowed, take your property's total land area in square feet and divide by 1,700. Round the number down.

³ To figure out the specific number of parking spaces required, take the number of apartments allowed and multiply by 1.4. Round to the nearest whole number.

⁴ Along Main Street, the required setback is 10 feet.

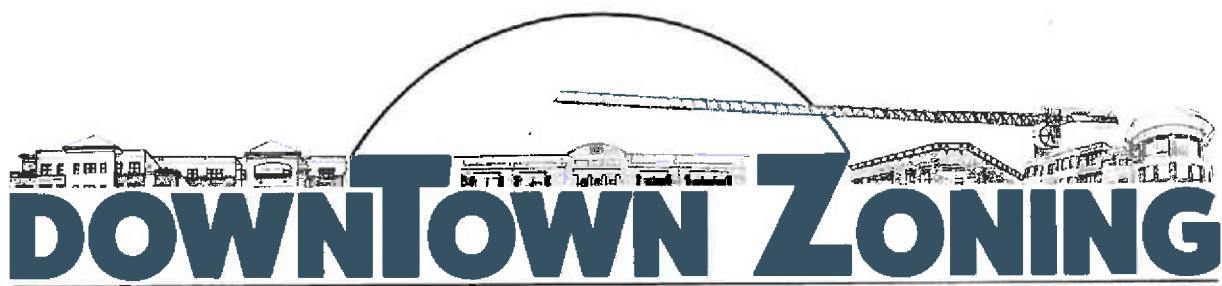
**COMMENTS:**

PLEASE COME BY PROPERTY ON
ROGERS - VIEW BLOG, 922 WASHINGTON
CURRENTLY AT 70 FT IN HEIGHT
PROPOSED WASHINGTON PROPERTY TO
72 FT. ISOLATES 5 PROPERTIES
WITH PROPOSED SET BACKS IMPOSSIBLE
TO DEVELOP OR BUILD.

Don WALTER

915 ROGERS

630-852-4410



DOWNTOWN ZONING

COMMENTS:

- ① Please do not impose setbacks (on the side and at the rear of the lots) on main st. where lots are not big or deep as that would make lots nonbuildable for any purpose. My property is at 4928-30 main st and the width is 51 ft x depth is 150 ft. If we remove 5 ft from either side, we end up with only 41 ft. Also if you take off 10% of space, it makes lot development commercially non-feasible.

Please do not impose setbacks on properties on main st.

- ~~Dean Lakhanani~~
- ② Non conforming legal use needs to be 3 years, 6 months is too short. In today's market, to get a tenant 6 months is too short.

Thank you for this opportunity.

Dean Lakhanani
630-880-9648
DMLAKHANI71@HOTMAIL.COM

10/9/2017

The Village of Downers Grove Mail - Downtown Core Comprehensive Plan

VILLAGE OF
DOWNERS GROVE

Stanley Popovich <spopovich@downers.us>

Downtown Core Comprehensive Plan

Kathy Viskocil <kvisk2000@yahoo.com>

Fri, Oct 6, 2017 at 12:19 PM

To: "Martin T. Tully" <mtully@downers.us>, Linda Kunze <lkunze@downtowndg.org>, David Fieldman <dfieldman@downers.us>, Stanley Popovich <spopovich@downers.us>, "mayorcouncil@downers.us" <mayorcouncil@downers.us>, Dave Depew <depewlaw@core.com>

Hello,

Unfortunately Rich & I were unable to attend the meetings regarding the Comprehensive plan. However, I just got off the phone with Linda Kunze and she informed me there was a flat No to my question "if a property has an adjacent building over 3 stories can that property meet the height of the adjacent property?" and no special allowances will be made. I'm not happy with a No, I think I would like (and deserve) to discuss this further with the people making the finally decisions regarding what I can do with my property, and to justify this huge devaluation of my property.

Linda also mentioned a clause regarding, selling my business and have some sort of 6 month cap, do so? Can you explain this clause and provide written documentation on this, I don't understand.

Attached is my original letter regarding my questions.

I hope we can work together on these issues, and I would like to think your vision of our downtown is of the best interest of your property owners, the business owners and the residents, which I am all three.

Kathy Viskocil-Heritage House Florist
<http://www.heritagehouseflorist.com>
www.facebook.com/hhflorist
www.twitter.com/hhflorist

September 17.docx
13K

September 19, 2017

Village Council, Village Management and Mayor Tully,

After attending the council meeting discussing the Downtown Core Strategic Plan, I have a few comments. I appreciate the work that is being done by your council but would like to state my concern on a few items that were discussed.

First, I have been running a retail business at 5109 Main Street for over 27 years and have been a property owner of 5111 and 5109 Main Street for over 17 years. We have survived ups & downs in the economy, as well as, major changes to infrastructure on Main Street and over time I have had the stress of an empty first floor space many times between tenants. Although I would prefer a retail business next door to my retail store, the future economy and vitality of our downtown is never a guaranteed. By limiting my options to retail only you would be forcing me to jeopardize my ability to find a tenant and can put a strong financial burden on us. The council can strongly suggest retail on first floor but unless you're going to pay the rents between retail tenants, it should always be my decision.

Second, Right now I am a one story building next to a four story building with your council approved. I eventually plan to sell my properties, probably to a developer that would knockdown both buildings. By limiting my addresses to a 3 story building you are lowering my property value tremendously. I have 4 stories next to me and think I too should have the ability to at least match this height. Might I suggest height limits to state "equal to building adjacent or 3 stories whichever is higher".

Lastly, my buildings are knockdowns, building and façade requirements are ok as long as I already have them in place (haha), an example discussed in the meeting was clear windows in front of building, which I agree with and do have. However, I personally will not be willing put extra money into my buildings for cosmetic improvements to my old buildings just for the sake of looking cohesive with other, newer buildings.

I look forward to more discussions with the council and the mayor regarding our wonderful downtown.

Thank you,

Kathy Viskocil

10/9/2017

The Village of Downers Grove Mail - Fwd: [downers.us website] Council & Director Request



VILLAGE OF
DOWNS GROVE

Stanley Popovich <spopovich@downers.us>

Fwd: [downers.us website] Council & Director Request

3 messages

David Fieldman <dfieldman@downers.us>
To: Stanley Popovich <spopovich@downers.us>

Mon, Oct 9, 2017 at 7:22 AM

----- Forwarded message -----

From: <noreply-webserver@downers.us>
Date: Thu, Oct 5, 2017 at 12:46 PM
Subject: [downers.us website] Council & Director Request
To: mayorcouncil@downers.us
Cc: michael_whittaker2003@yahoo.com

First Name : **Michael**Last Name : **Whittaker**

Email : michael_whittaker2003@yahoo.com

Send To : **8800**

Description: **Hello - I wanted to suggest that the city consider the rogers street corridor (west of Washington), be rezoned as transitional (to limit heights of any new developments to 35ft or less). If high density housing were to be put up in this area, that would pose a significant increase in the traffic and risk to the children and all residents in this part of the community. Please take this under consideration.**

--
David Fieldman
Village Manager
Village of Downers Grove
801 Burlington Avenue
630-434-5526

David Fieldman <dfieldman@downers.us>
To: Stanley Popovich <spopovich@downers.us>

Mon, Oct 9, 2017 at 7:23 AM

----- Forwarded message -----

From: <noreply-webserver@downers.us>
Date: Tue, Oct 3, 2017 at 11:30 AM
Subject: [downers.us website] Council & Director Request
To: mayorcouncil@downers.us
Cc: deo1@ntrs.com

First Name : **Daniel**Last Name : **ODonnell**

Email : deo1@ntrs.com

Send To : **8800**Description: **Re: Rezoning of Rogers St corridor**

I live at 835 Rogers St. I would like to express my strong desire to see the area on Rogers, west of Washington be rezoned as Transitional. My concerns, if it were to remain zoned as is, would be the additional traffic and congestion (not to mention the safety concerns for my children) that would accompany a large multi unit building in an area that already has such issues/problems. The proximity

Daniel McCormick, P.C.

5205 Washington St.
Downers Grove, IL 60515

PHONE: (630)769-1111

FAX: (630)769-1115

danmccormickl@downers.us

Via Email
MayorCouncil@Downers.us

October 9, 2017

To Mr. Mayor and Village Council:

My name is Dan McCormick and am one of the owners (along with my wife) of 5205 S. Washington Street. We have been owners of this property since 2005.

On Wednesday October 4th, my neighbors and I attended an informational meeting pertaining to proposed zoning changes to the Downtown business district. Currently, our property (and our neighbors') is zoned Downtown Business. There are no setback requirements nor parking restrictions. Under the proposed re-districting, our properties would be rezoned Downtown Transitional with 20-foot front and rear setbacks, 5 feet side yard setbacks and parking restrictions. For the following reasons, my neighbors and I believe that these impositions are unduly restrictive and are not in the best interests of the affected property owners or the Village. We believe that "transitional" would have a significant negative impact on the value of our properties and would be inconsistent with the businesses and buildings around us. If there were to be any changes in the zoning classifications, (a plan which we are not in favor of to begin with) it would make better sense to reclassify our properties on the east side of Washington as Downtown Edge-2.

If the logic of rezoning a portion of the 5200 block of Washington and 902 Maple to Downtown Transitional was the fact that these properties abut two residential properties (854 Maple and a small portion of 850 Maple), such logic is faulty when one notices that all of the residential properties along the north side of Maple east of Washington are contiguous with the proposed Downtown Edge-2 zoning district where the setbacks are half of what are proposed in the Downtown Transitional district without any parking restrictions. Furthermore, the properties on the west side of Washington would also be classified as Edge 2.

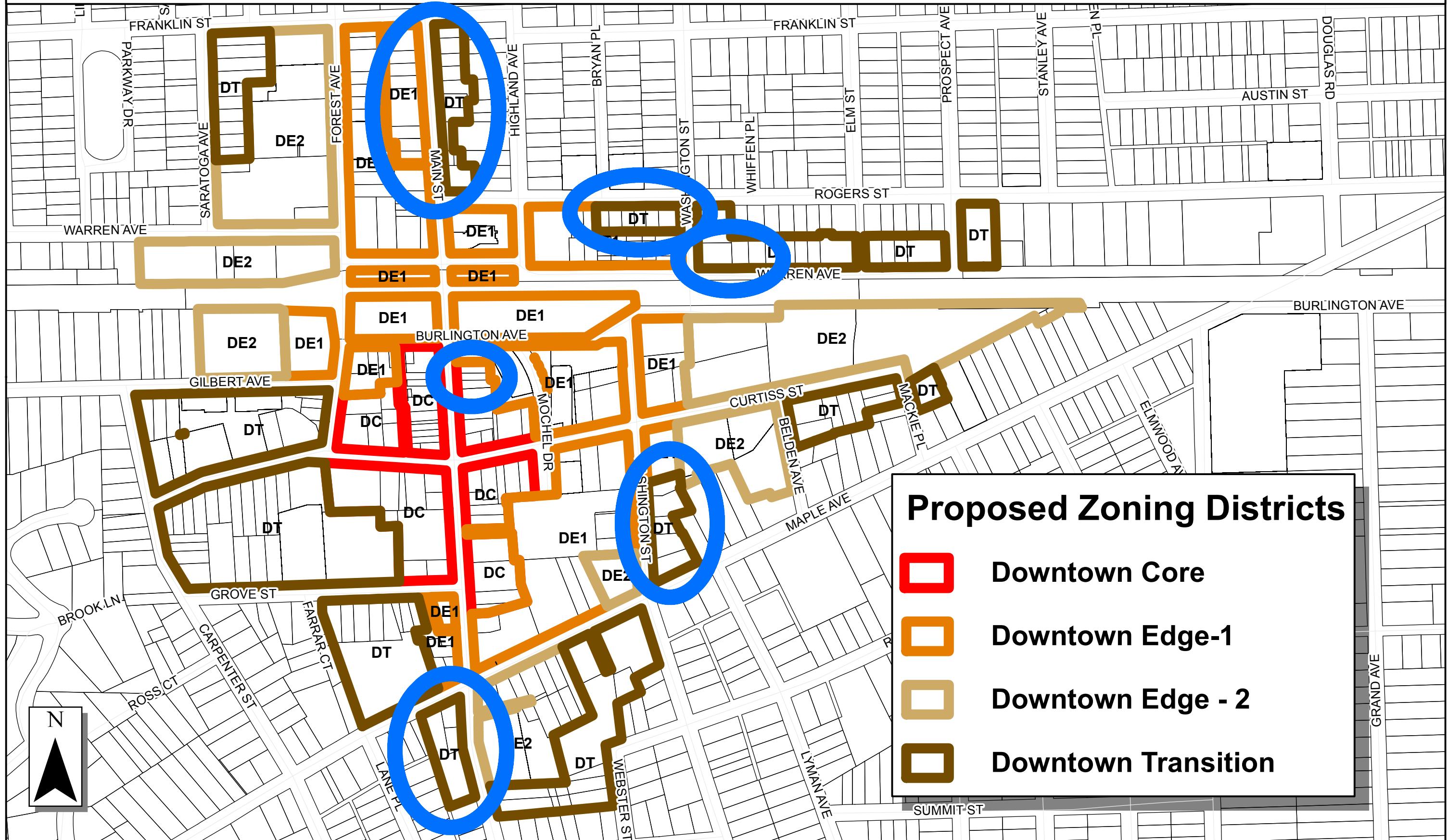
It is our understanding when talking with Staff that members of the Council struggled with this reclassification. We believe that the street scape view provided to the Council to review did not provide an adequate picture of each lot. By way of example, if the proposed setbacks were enacted, our building at 5205 would be grossly nonconforming in the rear and along the south side of the property, given that my rear lot line runs on an angle. If my building were to be destroyed, we could not replicate our building.

We all agree that a better solution would be to include our properties in the Downtown Edge-2 Zoning District which has less restrictive setback requirements and no parking requirements (given that we now have limited on-street parking along Washington which did not exist when we purchased the property).

We just want to retain the rights that we currently have without further governmental taking. Why "fix" what ain't broke?

Thank you for your consideration.

Property / Business Owner Areas of Concern



Proposed Zoning Districts

- Downtown Core
- Downtown Edge-1
- Downtown Edge - 2
- Downtown Transition