

VILLAGE OF DOWNERS GROVE
Report for the Village
3/8/2022

SUBJECT:	SUBMITTED BY:
Bid - 2022 Street Resurfacing (ST-004)	Andy Sikich Public Works Director

SYNOPSIS

A motion is requested to award a contract for the 2022 Street Resurfacing Contract B to K-Five Construction Company of Westmont, Illinois in the amount of \$2,689,352.56.

STRATEGIC PLAN ALIGNMENT

The goals for 2021-2023 identified *Top Quality Infrastructure*.

FISCAL IMPACT

The approved FY22 budget includes a total of \$3,182,100 including \$1,592,100 in the Motor Fuel Tax Fund and \$1,590,000 in Rebuild Illinois Funds for the Roadway Maintenance Program (ST-004). On February 1, 2022, Village Council appropriated Rebuild Illinois Funds in the amount of \$1,590,000 (Resolution 2022-11) and Motor Fuel Tax Funds in the amount of \$1,060,000 (Resolution 2022-12) for the 2022 Street Resurfacing Program. A supplemental resolution for Motor Fuel Tax Funds appropriating an additional \$40,000 for the 2022 Street Resurfacing Program has been separately presented by Staff, resulting in a total appropriation of \$2,690,000 for this project.

RECOMMENDATION

Approval on the March 15, 2022 active agenda.

BACKGROUND

The 2022 Street Resurfacing Program is a component of the 2022 Roadway Maintenance Program. The scope of this contract includes resurfacing the streets included on the attached list with a new layer of asphalt along with the repair of defective sections of pavement and concrete curb and gutter.

This contract represents a portion of the budgeted roadway maintenance work. For 2022, other projects include Crack Sealing and Roadway Patching.

A Call for Bids was published in accordance with the Village's Purchasing Policy. Five bids were received and publicly opened on February 16, 2022. A synopsis of the bids is as follows:

<u>Contractor</u>	<u>Base Bid</u>	
K-Five Construction Corp.	\$2,689,352.56	Low Bid
Geneva Construction Co.	\$2,783,937.40	
R.W. Dunteman Co.	\$2,798,189.92	
J.A. Johnson Paving Co.	\$2,818,930.42	
Lindahl Brothers	\$2,944,512.85	

K-Five has satisfactorily completed several recent projects for the Village, including the 2018 Road Resurfacing Program. They also satisfactorily completed the Finley Road Resurfacing in 2017 and the Dunham Road Resurfacing in 2017.

ATTACHMENTS

Contract Documents

List of Streets

Map

Resolution 2022-11

Resolution 2022-12



Illinois Department of Transportation

Local Public Agency Formal Contract



Contractor's Name

K-Five Construction Corporation

Contractor's Address

999 Oakmont Plaza Drive, Ste. 200

City

Westmont

State

IL

Zip Code

60559

STATE OF ILLINOIS

Local Public Agency

Village of Downers Grove

County

DuPage

Section Number

22-00117-00-RS

Street Name/Road Name

Various

Type of Funds

MFT & RBI

CONTRACT BOND (when required)

For a County and Road District Project

Submitted/Approved

Highway Commissioner Signature

Date

[Signature Box]

[Date Box]

Submitted/Approved

County Engineer/Superintendent of Highways

Date

[Signature Box]

[Date Box]

For a Municipal Project

Submitted/Approved/Passed

Signature

Date

[Signature Box]

[Date Box]

Official Title

Mayor

Department of Transportation

Concurrence in approval of award

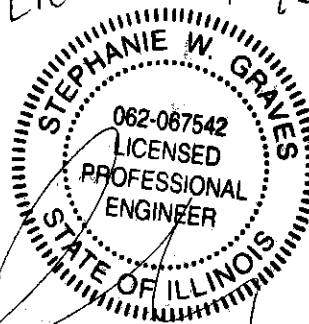
Regional Engineer Signature

Date

[Signature Box]

[Date Box]

LIC. EXP. 11/30/23



[Handwritten Signature]

Local Public Agency	Local Street/Road Name	County	Section Number
Village of Downers Grove	VARIOUS	DuPage	22-00117-00-RS

1. THIS AGREEMENT, made and concluded the _____ day of March 2022 between the Village of Downers Grove, known as the party of the first part, and K-Five Construction Corporation, its successor, and assigns, known as the party of the second part.
2. For and in consideration of the payments and agreements mentioned in the Proposal hereto attached, to be made and performed by the party of the first part, and according to the terms expressed in the Bond referring this contract, the party of the second part agrees with said party of the first part, at its own proper cost and expense, to do all the work, furnish all materials and all labor necessary to complete the work in accordance with the plans and specifications hereinafter described, and in full compliance with all of the terms of this contract.
3. It is also understood and agreed that the LPA Formal Contract Proposal, Special Provisions, Affidavit of Illinois Business Office, Apprenticeship or Training Program Certification, and Contract Bond hereto attached, and the Plans for Section 22-00117-00-RS in Village of Downers Grove, approved by the Illinois Department of Transportation on 02/02/22, are essential documents of this contract and are a part hereof.
4. IN WITNESS WHEREOF, the said parties have executed this contract on the date above mentioned.

Attest: The Village of Downers Grove
Local Public Agency Type Name of Local Public Agency

Clerk	Date

(SEAL)

Party of the First Part	Date
By: 	

(If a Corporation)

Corporate Name
K-Five Construction Corporation

President, Party of the Second Part	Date
By: 	2/23/22

(If a Limited Liability Corporation)

(SEAL)



LLC Name

Manager or Authorized Member, Party of the Second Part
By:

(If a Partnership)

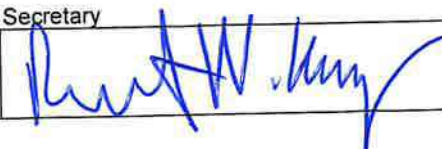
Partner	Date

Partner	Date

Partners doing Business under the firm name of Party of the Second Part

(If an individual)

Party of the Second Part	Date

Attest: Secretary


Date
2-23-22

(SEAL)

Bond No. 30146253



Contract Bond



Local Public Agency	County	Street Name/Road Name	Section Number
Village of Downers Grove	DuPage	VARIOUS	22-00117-00-RS

Bond information to be returned to Local Public Agency at 5101 Walnut Avenue, Downers Grove IL 60515
Complete Address

We, K-Five Construction Corporation, 999 Oakmont Plaza Drive, Suite 200, Westmont, IL 60559
Contractor's Name and Address

a/an corporation organized under the laws of the State of Illinois as PRINCIPAL, and
State

Continental Casualty Company, 151 N. Franklin St., Chicago, IL 60606
Surety Name and Address

as SURETY, are held and firmly bound unto the above Local Public Agency (hereafter referred to as "LPA") in the penal sum of
Two Million, Six Hundred and Eighty Nine Thousand, Three Hundred and Fifty Two and 56/100 Dollars

Dollars (\$2,689,352.56) lawful money of the United States, to be paid to said LPA, the payment of which we bind ourselves, successors and assigns jointly to pay to the LPA this sum under the conditions of this instrument.

WHEREAS, THE CONDITION OF THE FOREGOING OBLIGATION IS SUCH that the said Principal has entered into a written contract with the LPA acting through its awarding authority for the construction of work on the above sections, which contract is hereby referred to and made a part hereof, as if written herein at length, and whereby the said Principal has promised and agreed to perform said work in accordance with the terms of said contract, and has promised to pay all sums of money due for any labor, materials, apparatus, fixtures or machinery furnished to such Principal for the purpose of performing such work and has further agreed to pay all direct and indirect damages to any person, firm, company or corporation to whom any money may be due from the Principal, subcontractor or otherwise for any such labor, materials, apparatus, fixtures or machinery so furnished and that suit may be maintained on such bond by any such person, firm, company or corporation for the recovery of any such money.

NOW, THEREFORE, if the said Principal shall perform said work in accordance with the terms of said contract, and shall pay all sums of money due or to become due for any labor, materials, apparatus, fixtures or machinery furnished to it for the purpose of constructing such work, and shall commence and complete the work within the time prescribed in said contract, and shall pay and discharge all damages, direct and indirect, that may be suffered or sustained on account of such work during the time of the performance thereof and until the said work shall have been accepted, and shall hold the LPA and its awarding authority harmless on account of any such damages and shall in all respects fully and faithfully comply with all the provisions, conditions and requirements of said contract, then this obligation shall be void; otherwise it shall remain in full force and effect.

IN TESTIMONY WHEREOF, the said PRINCIPAL and the said SURETY have caused this instrument to be signed by their respective agents this 23rd day of February, 2022.
Day Month and Year

PRINCIPAL

Company Name	Company Name
K-Five Construction Corporation	

By	By
Signature & Title	Signature & Title
Date	Date
Robert G. Krug, President	
02/23/2022	

Attest	Attest
Signature & Title	Signature & Title
Date	Date
Robert W. Krug, Secretary	
02/23/2022	



(If PRINCIPAL is a joint venture of two or more contractors, the company names and authorized signature of each contractor must be affixed.)

STATE OF IL
COUNTY OF DuPage

I, Elizabeth Grogan, a Notary Public in and for said county, do hereby certify that

Robert G. Krug and Robert W. Krug

Insert name of Individuals signing on behalf of PRINCIPAL

who is/are each personally known to me to be the same person(s) whose name(s) is/are subscribed to the foregoing instrument on behalf of PRINCIPAL, appeared before me this day in person and acknowledged respectively, that he/she/they signed and delivered said instrument freely and voluntarily for the uses and purposes therein set forth.

Given under my hand and notarial seal this 22nd day of February, 2022.
Day Month, Year



Notary Public Signature
Elizabeth Grogan
Date commission expires 7/29/24

SURETY

Name of Surety
Continental Casualty Company

Title
By: Kimberly R. Holmes
Kimberly R. Holmes, Attorney-In-Fact

STATE OF IL
COUNTY OF DuPage

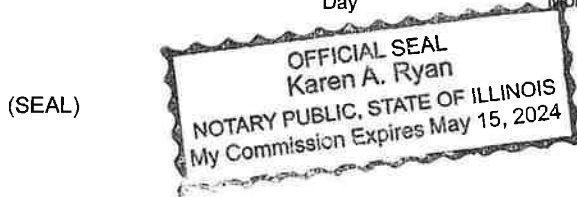
I, Karen A. Ryan, a Notary Public in and for said county, do hereby certify that

Kimberly R. Holmes

Insert name of Individuals signing on behalf of SURETY

who is/are each personally known to me to be the same person(s) whose name(s) is/are subscribed to the foregoing instrument on behalf of SURETY, appeared before me this day in person and acknowledged respectively, that he/she/they signed and delivered said instrument freely and voluntarily for the uses and purposes therein set forth.

Given under my hand and notarial seal this 23rd day of February, 2022.
Day Month, Year



Notary Public Signature
Karen A. Ryan
Date commission expires 05/15/2024

Approved this _____ day of _____ Month, Year

Attest:

Local Public Agency Clerk Signature Date

Local Public Agency Type Clerk

Awarding Authority
Village of Downers Grove

Awarding Authority Signature Date

POWER OF ATTORNEY APPOINTING INDIVIDUAL ATTORNEY-IN-FACT

Know All Men By These Presents, That Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company (herein called "the CNA Companies"), are duly organized and existing insurance companies having their principal offices in the City of Chicago, and State of Illinois, and that they do by virtue of the signatures and seals herein affixed hereby make, constitute and appoint

Karen A Ryan, William Cahill, Kimberly Sawicki, Ann Marie Waters, Brent R Wagner, Kimberly R Holmes, Rachel E Hernandez, Leigh Ann Francis, Richard A Freebourn Jr, Individually

of Naperville, IL, their true and lawful Attorney(s)-in-Fact with full power and authority hereby conferred to sign, seal and execute for and on their behalf bonds, undertakings and other obligatory instruments of similar nature

- In Unlimited Amounts -

and to bind them thereby as fully and to the same extent as if such instruments were signed by a duly authorized officer of their insurance companies and all the acts of said Attorney, pursuant to the authority hereby given is hereby ratified and confirmed.

This Power of Attorney is made and executed pursuant to and by authority of the By-Law and Resolutions, printed on the reverse hereof, duly adopted, as indicated, by the Boards of Directors of the insurance companies.

In Witness Whereof, the CNA Companies have caused these presents to be signed by their Vice President and their corporate seals to be hereto affixed on this 21st day of June, 2021.



Continental Casualty Company
National Fire Insurance Company of Hartford
American Casualty Company of Reading, Pennsylvania

Paul T. Bruflat
Vice President

State of South Dakota, County of Minnehaha, ss:

On this 21st day of June, 2021, before me personally came Paul T. Bruflat to me known, who, being by me duly sworn, did depose and say: that he resides in the City of Sioux Falls, State of South Dakota; that he is a Vice President of Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company described in and which executed the above instrument; that he knows the seals of said insurance companies; that the seals affixed to the said instrument are such corporate seals; that they were so affixed pursuant to authority given by the Boards of Directors of said insurance companies and that he signed his name thereto pursuant to like authority, and acknowledges same to be the act and deed of said insurance companies.



My Commission Expires March 2, 2026

M. Bent
Notary Public

CERTIFICATE

I, D. Johnson, Assistant Secretary of Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company do hereby certify that the Power of Attorney herein above set forth is still in force, and further certify that the By-Law and Resolution of the Board of Directors of the insurance companies printed on the reverse hereof is still in force. In testimony whereof I have hereunto subscribed my name and affixed the seal of the said insurance companies this 23rd day of February, 2022.



Continental Casualty Company
National Fire Insurance Company of Hartford
American Casualty Company of Reading, Pennsylvania

D. Johnson
Assistant Secretary

Authorizing By-Laws and Resolutions

ADOPTED BY THE BOARD OF DIRECTORS OF CONTINENTAL CASUALTY COMPANY:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company at a meeting held on May 12, 1995:

“RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective.”

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of Continental Casualty Company.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

“Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the “Authorized Officers”) to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, “Electronic Signatures”); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. “

ADOPTED BY THE BOARD OF DIRECTORS OF NATIONAL FIRE INSURANCE COMPANY OF HARTFORD:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company by unanimous written consent dated May 10, 1995:

“RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective.”

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of National Fire Insurance Company of Hartford.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

“Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the “Authorized Officers”) to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, “Electronic Signatures”); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. “

ADOPTED BY THE BOARD OF DIRECTORS OF AMERICAN CASUALTY COMPANY OF READING, PENNSYLVANIA:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company by unanimous written consent dated May 10, 1995:

“RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective.”

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of American Casualty Company of Reading, Pennsylvania.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

“Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the “Authorized Officers”) to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, “Electronic Signatures”); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. “

Local Public Agency	County	Section Number	Route(s) (Street/Road Name)
Village of Downers Grove	DuPage	22-00117-00-RS	Various

NOTICE TO BIDDERS

Sealed proposals for the project described below will be received at the office of Public Works Department
 5101 Walnut Avenue, Downers Grove IL 60515 until 10:00 AM on 02/16/22
Address Time Date

Sealed proposals will be opened and read publicly at the office of Public Works Department
 5101 Walnut Avenue, Downers Grove IL 60515 at 10:00 AM on 02/16/22
Address Time Date

DESCRIPTION OF WORK

Location	Project Length
Various Streets	5.04 mi (26,632 ft)

Proposed Improvement
 Pavement removal and replacement, level binder, hot mix asphalt surface course, curb and gutter removal and replacement, and all related work

1. Plans and proposal forms will be available in the office of
 DemandStar
 Stephanie Graves (630) 434-5487 Proposal Fee \$0

- Prequalification
 If checked, the 2 apparent as read low bidders must file within 24 hours after the letting an "Affidavit of Availability" (Form BC 57) in triplicate, showing all uncompleted contracts awarded to them and all low bids pending award for Federal, State, County, Municipal and private work. One original shall be filed with the Awarding Authority and two originals with the IDOT District Office.
- The Awarding Authority reserves the right to waive technicalities and to reject any or all proposals as provided in BLRS Special Provision for Bidding Requirements and Conditions for Contract Proposals.
- The following BLR Forms shall be returned by the bidder to the Awarding Authority:
 - Local Public Agency Formal Contract Proposal (BLR 12200)
 - Schedule of Prices (BLR 12201)
 - Proposal Bid Bond (BLR 12230) (if applicable)
 - Apprenticeship or Training Program Certification (BLR 12325) (do not use for project with Federal funds.)
 - Affidavit of Illinois Business Office (BLR 12326) (do not use for project with Federal funds)
- The quantities appearing in the bid schedule are approximate and are prepared for the comparison of bids. Payment to the Contractor will be made only for the actual quantities of work performed and accepted or materials furnished according to the contract. The scheduled quantities of work to be done and materials to be furnished may be increased, decreased or omitted as hereinafter provided.
- Submission of a bid shall be conclusive assurance and warranty the bidder has examined the plans and understands all requirements for the performance of work. The bidder will be responsible for all errors in the proposal resulting from failure or neglect to conduct an in depth examination. The Awarding Authority will, in no case, be responsible for any costs, expenses, losses or changes in anticipated profits resulting from such failure or neglect of the bidder.
- The bidder shall take no advantage of any error or omission in the proposal and advertised contract.
- If a special envelope is supplied by the Awarding Authority, each proposal should be submitted in that envelope furnished by the Awarding Agency and the blank spaces on the envelope shall be filled in correctly to clearly indicate its contents. When an envelope other than the special one furnished by the Awarding Authority is used, it shall be marked to clearly indicate its contents. When sent by mail, the sealed proposal shall be addressed to the Awarding Authority at the address and in care of the official in whose office the bids are to be received. All proposals shall be filed prior to the time and at the place specified in the Notice to Bidders. Proposals received after the time specified will be returned to the bidder unopened.
- Permission will be given to a bidder to withdraw a proposal if the bidder makes the request in writing or in person before the time for opening proposals.

Local Public Agency	County	Section Number	Route(s) (Street/Road Name)
Village of Downers Grove	DuPage	22-00117-00-RS	Various

PROPOSAL

1. Proposal of K-Five Construction Corporation
Contractor's Name
999 Oakmont Plaza Drive, Ste. 200, Westmont, IL 60559
Contractor's Address

2. The plans for the proposed work are those prepared by Village of Downers Grove
and approved by the Department of Transportation on _____

3. The specifications referred to herein are those prepared by the Department of Transportation and designated as "Standard Specifications for Road and Bridge Construction" and the "Supplemental Specifications and Recurring Special Provisions" thereto, adopted and in effect on the date of invitation for bids.

4. The undersigned agrees to accept, as part of the contract, the applicable Special Provisions indicated on the "Check Sheet for Recurring Special Provisions" contained in this proposal.

5. The undersigned agrees to complete the work within _____ working days or by 10/07/22 unless additional time is granted in accordance with the specifications.

6. The successful bidder at the time of execution of the contract will be required to deposit a contract bond for the full amount of the award. When a contract bond is not required, the proposal guaranty check will be held in lieu thereof. If this proposal is accepted and the undersigned fails to execute a contract and contract bond as required, it is hereby agreed that the Bid Bond of check shall be forfeited to the Awarding Authority.

7. Each pay item should have a unit price and a total price. If no total price is shown or if there is a discrepancy between the products of the unit price multiplied by the quantity, the unit price shall govern. If a unit price is omitted, the total price will be divided by the quantity in order to establish a unit price. A bid may be declared unacceptable if neither a unit price nor a total price is shown.

8. The undersigned submits herewith the schedule of prices on BLR 12201 covering the work to be performed under this contract.

9. The undersigned further agrees that if awarded the contract for the sections contained in the combinations on BLR 12201, the work shall be in accordance with the requirements of each individual proposal for the multiple bid specified in the Schedule for Multiple Bids below.

10. A proposal guaranty in the proper amount, as specified in BLRS Special Provision for Bidding Requirements and Conditions for Contract Proposals, will be required. Bid Bonds _____ be allowed as a proposal guaranty. Accompanying this proposal is either a bid bond, if allowed, on Department form BLR 12230 or a proposal guaranty check, complying with the specifications, made payable to: _____ Treasurer of Village of Downers Grove
The amount of the check is 5% of the bid amount (_____).

Attach Cashier's Check or Certified Check Here

In the event that one proposal guaranty check is intended to cover two or more bid proposals, the amount must be equal to the sum of the proposal guaranties which would be required for each individual bid proposal. If the proposal guaranty check is placed in another bid proposal, state below where it may be found.

The proposal guaranty check will be found in the bid proposal for: Section Number 22-00117-00-RS

Local Public Agency	County	Section Number	Route(s) (Street/Road Name)
Village of Downers Grove	DuPage	22-00117-00-RS	Various

CONTRACTOR CERTIFICATIONS

The certifications hereinafter made by the bidder are each a material representation of fact upon which reliance is placed should the Department enter into the contract with the bidder.

- Debt Delinquency.** The bidder or contractor or subcontractor, respectively, certifies that it is not delinquent in the payment of any tax administered by the Department of Revenue unless the individual or other entity is contesting, in accordance with the procedure established by the appropriate Revenue Act, its liability for the tax or the amount of the tax. Making a false statement voids the contract and allows the Department to recover all amounts paid to the individual or entity under the contract in a civil action.

- Bid-Rigging or Bid Rotating.** The bidder or contractor or subcontractor, respectively, certifies that it is not barred from contracting with the Department by reason of a violation of either 720 ILCS 5/33E-3 or 720 ILCS 5/33E-4.

A violation of section 33E-3 would be represented by a conviction of the crime of bid-rigging which, in addition to Class 3 felony sentencing, provides that any person convicted of this offense, or any similar offense of any state or the United States which contains the same elements as this offense shall be barred for 5 years from the date of conviction from contracting with any unit of State or local government. No corporation shall be barred from contracting with any unit of State or local government as a result of a conviction under this Section of any employee or agent of such corporation if the employee so convicted is no longer employed by the corporation: (1) it has been finally adjudicated not guilty or (2) if it demonstrates to the governmental entity with which it seeks to contract that entity finds that the commission of the offense was neither authorized, requested, commanded, nor performed by a director, officer or a high managerial agent on behalf of the corporation.

A violation of Section 33E-4 would be represented by a conviction of the crime of bid-rotating which, in addition to Class 2 felony sentencing, provides that any person convicted of this offense or any similar offense of any state or the United States which contains the same elements as this offense shall be permanently barred from contracting with any unit of State of Local government. No corporation shall be barred from contracting with any unit of State or Local government as a result of a conviction under this Section of any employee or agent of such corporation if the employee so convicted is no longer employed by the corporation and: (1) it has been finally adjudicated not guilty or (2) if it demonstrates to the governmental entity with which it seeks to contract and that entity finds that the commission of the offense was neither authorized, requested, commanded, nor performed by a director, officer or a high managerial agent on behalf of the corporation.

- Bribery.** The bidder or contractor or subcontractor, respectively, certifies that, it has not been convicted of bribery or attempting to bribe an officer or employee of the State of Illinois or any unit of local government, nor has the firm made an admission of guilt of such conduct which is a matter of record, nor has an official, agent, or employee of the firm committed bribery or attempted bribery on behalf of the firm and pursuant to the direction or authorization of a responsible official of the firm.
- Interim Suspension or Suspension.** The bidder or contractor or subcontractor, respectively, certifies that it is not currently under a suspension as defined in Subpart 1 of Title 44 Subtitle A Chapter III Part 6 of the Illinois Administrative code. Furthermore, if suspended prior to completion of this work, the contract or contracts executed for the completion of this work may be canceled.

Local Public Agency	County	Section Number	Route(s) (Street/Road Name)
Village of Downers Grove	DuPage	22-00117-00-RS	Various

SIGNATURES

(If an individual)

Signature of Bidder	Date	
Business Address		
City	State	Zip Code

(If a partnership)

Firm Name		
Signature	Date	
Title		
Business Address		
City	State	Zip Code

Insert the Names and Addresses of all Partners

(If a corporation)

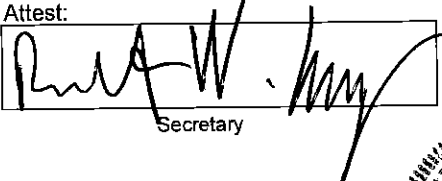


Corporate Name		
K-Five Construction Corporation		
Signature	Date	
	2/16/2022	
Title		
President		
Business Address		
999 Oakmont Plaza Drive, Ste. 200		
City	State	Zip Code
Westmont	IL	60559

Insert Names of Officers

President
Robert G. Krug

Attest:


Secretary

Secretary

Robert W. Krug

Treasurer

Josephine M. Krug





Illinois Department of Transportation

Schedule of Prices



Contractor's Name

[Empty box for Contractor's Name]

Contractor's Address

[Empty box for Contractor's Address]

City

[Empty box for City]

State

[Empty box for State]

Zip Code

[Empty box for Zip Code]

Local Public Agency

Village of Downers Grove

County

DuPage

Section Number

22-00117-00-RS

Route(s) (Street/Road Name)

Various

Schedule for Multiple Bids

Combination Letter	Section Included in Combinations	Total

Schedule for Single Bid

(For complete information covering these items, see plans and specifications.)

Item Number	Items	Unit	Quantity	Unit Price	Total
1	HMA Surf Course M D, N50 1.5	Ton	6574 6959 <i>R6K</i>	\$89.00	\$619,351.00 \$585,086.00
2	Leveling Binder (MM), N50 1.5"	Ton	6574 6959 <i>R6K</i>	\$87.00	\$571,938.00 \$571,938.00
3	Bit. Mats (Trackless Tack Coat)	LB	52825 55923 <i>R6K</i>	\$0.01	\$559.23 \$528.25
4	Longitudinal Joint Sealant	LF	24902 26582 <i>R6K</i>	\$2.30	\$61,136.60 \$57,274.60
5	HMA Removal & Replace, 4"	SY	4633 4805 <i>R6K</i>	\$10.00	\$48,050.00 \$46,330.00
6	HMA Removal & Replace, 6"	SY	1581 1679 <i>R6K</i>	\$12.50	\$20,912.50 \$19,762.50
7	Porous Granular Embank., Spl	CY	4 4 <i>R6K</i>	\$50.00	\$200.00 \$200.00
8	Rem & Disp of Unsuit Matl	CY	4 4 <i>R6K</i>	\$90.00	\$360.00 \$360.00
9	Geotech Fabric for Ground Stab	SY	150 350 <i>R6K</i>	\$10.00	\$3,500.00 \$1,500.00
10	Earth Excavation	CY	120	\$45.00	\$5,400.00
11	Agg. Base Course, Type B, 4"	CY	18	\$44.55	\$801.90
12	Aggregate for Temp Access	Ton	18	\$23.50	\$423.00
13	Comb Conc Curb & Gutter Rem	LF	13785 14232 <i>R6K</i>	\$6.25	\$86,950.00 \$86,156.25
14	Comb Conc C&G Type M-3.12	LF	8631 8822 <i>R6K</i>	\$24.85	\$219,226.70 \$214,480.3
15	Comb Conc C&G Type M-4.12	LF	108	\$25.20	\$2,721.60
16	Comb Conc C&G Type M-6.12	LF	4173 4427 <i>R6K</i>	\$25.35	\$112,224.45 \$105,785.5
17	Comb Conc C&G Type B-6.12	LF	875	\$25.50	\$22,312.50
18	Concrete Curb, Type B	LF	360	\$28.00	\$10,080.00
19	Manhole to be Adjusted	EA	11 12 <i>R6K</i>	\$450.00	\$5,100.00 \$4,950.00
20	Manhole to be Adjusted, Specia	EA	29	\$750.00	\$21,750.00
21	Frame & Lid, Type 1, Open Lid	EA	5	\$600.00	\$3,000.00
22	Frame & Lid, Ty 1, Closed Lid	EA	5	\$620.00	\$3,100.00
23	Frame and Grate, Type 3	EA	3	\$515.00	\$1,545.00
24	Manhole to be Reconstructed	EA	1	\$1,650.00	\$1,650.00

Local Public Agency		County		Section Number		Route(s) (Street/Road Name)	
Village of Downers Grove		DuPage		22-00117-00-RS		Various	
Item Number	Items	Unit	Quantity	Unit Price	Total		
25	Inlet to be Adjusted	EA	107444	\$350.00	ROK	\$38,856.00	\$37,450.00
26	Inlet to be Reconstructed	EA	6	\$805.00			\$4,830.00
27	Valve Boxes to be Adjusted	EA	5	\$490.00			\$2,450.00
28	Inlet Filters	EA	168	\$90.00			\$15,120.00
29	Inlet Filters Cleaning	EA	168	\$50.00			\$8,400.00
30	HMA Surface Removal, 3.0"	SY782898	32848	\$3.40	ROK	\$201,088.00	\$266,182.60
31	HMA Surface Removal- Butt Jt	SY 396	415	\$0.01	ROK	\$4.15	\$3.96
32	PCC Sidewalk Removal	SF18286	2012	\$2.15	ROK	\$43,260.00	\$39,314.90
33	PCC Sidewalk, 5"	SF17765	19550	\$6.10	ROK	\$119,255.00	\$108,366.50
34	PCC Sidewalk, 6"	SF	278	\$6.40			\$1,779.20
35	Detectable Warnings	SF 1140	1200	\$22.00	ROK	\$26,400.00	\$25,080.00
36	Decor Paver Driveway R&R	SY	25	\$250.00			\$6,250.00
37	Parkway Restoration	SY	10030	\$0.01			\$100.30
38	Growth-Inhibiting Erosion Cont	SY	5015	\$4.50			\$22,567.50
39	Supplemental Watering	Unit	10	\$40.00			\$400.00
40	Tree Root Pruning	EA	13	\$140.00			\$1,820.00
41	HMA Driveway Removal	SY 644	653	\$16.00	ROK	\$10,448.00	\$10,304.00
42	HMA Driveway Pavement, 3"	SY 644	653	\$30.00	ROK	\$19,590.00	\$19,320.00
43	PCC Driveway Removal	SY 3964	4009	\$12.90	ROK	\$51,716.10	\$51,135.60
44	PCC Driveway Pavement, 6"	SY 3964	4009	\$54.75	ROK	\$219,492.75	\$217,029.01
45	Short Term Pavt Marking, 4"	LF	11400	\$0.01			\$114.00
46	Short Term Pavt Mark Removal	SF	3800	\$0.01			\$38.00
47	Thermo Pavt Mark Line, 4"	LF	3690	\$0.95			\$3,505.50
48	Thermo Pavt Mark Line, 6"	LF	772	\$1.50			\$1,158.00
49	Thermo Pavt Mark Line, 12"	LF	50	\$3.00			\$150.00
50	Thermo Pavt Mark Line, 24"	LF 3315	460	\$6.00	ROK	\$2,070.00	\$1,986.00
51	Thermo Pavt Marking, L&S	SF	227	\$6.00			\$1,362.00
52	Street Sweeping	Hour	15	\$180.00			\$2,700.00
53	Erosion, Sediment, Dust Cont	LS	1	\$3,000.00			\$3,000.00
54	Construction Layout	LS	1	\$7,500.00			\$7,500.00
55	Traffic Cont & Prot Std 701501	LS	1	\$56,000.00			\$56,000.00
56	Traffic Cont & Prot Std 701502	LS	1	\$800.00			\$800.00
57	Traffic Cont & Prot Std 701801	LS	1	\$6,000.00			\$6,000.00
Bidder's Total Proposal						ROK	\$2,818,135.70
						ROK	\$2,689,352.56

- Each pay item should have a unit price and a total price.
- If no total price is shown or if there is a discrepancy between the product of the unit price multiplied by the quantity, the unit price shall govern.
- If a unit price is omitted, the total price will be divided by the quantity in order to establish a unit price.

Local Public Agency	County	Section Number	Route(s) (Street/Road Name)
Village of Downers Grove	DuPage	22-00117-00-RS	Various

4. A bid may be declared unacceptable if neither a unit price or total price is shown.



Special Provisions



Local Public Agency	County	Section Number
Village of Downers Grove	DuPage	22-00000-01-GM

The following Special Provision supplement the "Standard Specifications for Road and Bridge Construction", adopted April 1, 2016, the latest edition of the "Manual on Uniform Traffic Control Devices for Streets and Highways", and the "Manual of Test Procedures of Materials" in effect on the date of invitation of bids, and the Supplemental Specification and Recurring Special Provisions indicated on the Check Sheet included here in which apply to and govern the construction of the above named section, and in case of conflict with any parts, or parts of said Specifications, the said Special Provisions shall take precedence and shall govern.

Maintenance of Roadways

Effective: September 30, 1985

Revised: November 1, 1996

Beginning on the date that work begins on this project, the Contractor shall assume responsibility for normal maintenance of all existing roadways within the limits of the improvement. This normal maintenance shall include all repair work deemed necessary by the Engineer, but shall not include snow removal operations. Traffic control and protection for maintenance of roadways will be provided by the Contractor as required by the Engineer.

If items of work have not been provided in the contract, or otherwise specified for payment, such items, including the accompanying traffic control and protection required by the Engineer, will be paid for in accordance with Article 109.04 of the Standard Specifications.

TRAFFIC CONTROL PLAN

Effective: September 30, 1985

Revised: January 1, 2007

Traffic Control shall be according to the applicable sections of the Standard Specifications, the Supplemental Specifications, the "Illinois Manual on Uniform Traffic Control Devices for Streets and Highways", any special details and Highway Standards contained in the plans, and the Special Provisions contained herein.

Special attention is called to Article 107.09 of the Standard Specifications and the following Highway Standards, Details, Quality Standard for Work Zone Traffic Control Devices, Recurring Special Provisions and Special Provisions contained herein, relating to traffic control.

~~The Contractor shall contact the District One Bureau of Traffic at least 72 hours in advance of beginning work.~~ The Contractor shall contact Downers Grove Public Works at least 72 hours in advance of beginning work.

STANDARDS:

701301-04

701501-06

701502-08

701801-06

701901-07

DETAILS: TC-10, TC-13**SPECIAL PROVISIONS:**

Basis of Payment: This work shall be paid for at the contract Lump Sum price for TRAFFIC CONTROL AND PROTECTION STANDARD 701501 or TRAFFIC CONTROL AND PROTECTION STANDARD 701502 or TRAFFIC CONTROL AND PROTECTION STANDARD 701801

STATUS OF UTILITIES (D-1)

Effective: June 1, 2016

Revised: January 1, 2020

Utility companies and/or municipal owners located within the construction limits of this project have provided the following information in regard to their facilities and the proposed improvements. The tables below contain a description of specific conflicts to be resolved and/or facilities which will require some action on the part of the Department's contractor to proceed with work. Each table entry includes an identification of the action necessary and, if applicable, the estimated duration required for the resolution.

UTILITIES TO BE ADJUSTED

Conflicts noted below have been identified by following the suggested staging plan included in the contract. The company has been notified of all conflicts and will be required to obtain the necessary permits to complete their work; in some instances resolution will be a function of the construction staging. The responsible agency must relocate, or complete new installations as noted below; this work has been deemed necessary to be complete for the Department's contractor to then work in the stage under which the item has been listed.

Pre-Stage

STAGE / LOCATION	TYPE	DESCRIPTION	RESPONSIBLE AGENCY	DURATION OF TIME
				None Anticipated

Stage 1

STAGE / LOCATION	TYPE	DESCRIPTION	RESPONSIBLE AGENCY	DURATION OF TIME
				None Anticipated

Stage 2

STAGE / LOCATION	TYPE	DESCRIPTION	RESPONSIBLE AGENCY	DURATION OF TIME
				None Anticipated

No conflicts to be resolved (or if there are conflicts they are to be listed as noted above)

Pre-Stage: _____ Days Total Installation
 Stage 1: _____ Days Total Installation
 Stage 2: _____ Days Total Installation

The following contact information is what was used during the preparation of the plans as provided by the Agency/Company responsible for resolution of the conflict.

Agency/Company Responsible to Resolve Conflict	Name of contact	Address	Phone	e-mail address

UTILITIES TO BE WATCHED AND PROTECTED

The areas of concern noted below have been identified by following the suggested staging plan included for the contract. The information provided is not a comprehensive list of all remaining utilities, but those which during coordination were identified as ones which might require the Department's contractor to take into consideration when making the determination of the means and methods that would be required to construct the proposed improvement. In some instances, the contractor will be responsible to notify the owner in advance of the work to take place so necessary staffing on the owner's part can be secured.

Pre-Stage

STAGE / LOCATION	TYPE	DESCRIPTION	OWNER
N/A			

Stage 1

STAGE / LOCATION	TYPE	DESCRIPTION	OWNER
N/A			

Stage 2

STAGE / LOCATION	TYPE	DESCRIPTION	OWNER
N/A			

No facilities requiring extra consideration (or listed as noted above)

The following contact information is what was used during the preparation of the plans as provided by the owner of the facility.

Agency/Company Responsible to Resolve Conflict	Name of contact	Address	Phone	E-mail address

The above represents the best information available to the Department and is included for the convenience of the bidder. The days required for conflict resolution should be considered in the bid as this information has also been factored into the timeline identified for the project when setting the completion date. The applicable portions of the Standard Specifications for Road and Bridge Construction shall apply.

Estimated duration of time provided above for the first conflicts identified will begin on the date of the executed contract regardless of the status of the utility relocations. The responsible agencies will be working toward resolving subsequent conflicts in conjunction with contractor activities in the number of days noted.

The estimated relocation duration must be part of the progress schedule submitted by the contractor. A utility kickoff meeting will be scheduled between the Department, the Department's contractor and the utility companies when necessary. The Department's contractor is responsible for contacting J.U.L.I.E. prior to any and all excavation work.

ADJUSTMENTS AND RECONSTRUCTIONS

Effective: March 15, 2011

Revise the first paragraph of Article 602.04 to read:

“602.04 Concrete. Cast-in-place concrete for structures shall be constructed of Class SI concrete according to the applicable portions of Section 503. Cast-in-place concrete for pavement patching around adjustments and reconstructions shall be constructed of Class PP-1 concrete, unless otherwise noted in the plans, according to the applicable portions of Section 1020.”

Revise the third, fourth and fifth sentences of the second paragraph of Article 602.11(c) to read:

“Castings shall be set to the finished pavement elevation so that no subsequent adjustment will be necessary, and the space around the casting shall be filled with Class PP-1 concrete, unless otherwise noted in the plans, to the elevation of the surface of the base course or binder course. HMA surface or binder course material shall not be allowed. The pavement may be opened to traffic according to Article 701.17(e)(3)b.”

Revise Article 603.05 to read:

“603.05 Replacement of Existing Flexible Pavement. After the castings have been adjusted, the surrounding space shall be filled with Class PP-1 concrete, unless otherwise noted in the plans, to the elevation of the surface of the base course or binder course. HMA surface or binder course material shall not be allowed. The pavement may be opened to traffic according to Article 701.17(e)(3)b.”

Revise Article 603.06 to read:

“603.06 Replacement of Existing Rigid Pavement. After the castings have been adjusted, the pavement and HMA that was removed, shall be replaced with Class PP-1 concrete, unless otherwise noted in the plans, not less than 9 in. (225 mm) thick. The pavement may be opened to traffic according to Article 701.17(e)(3)b.

The surface of the Class PP concrete shall be constructed flush with the adjacent surface.”

Revise the first sentence of Article 603.07 to read:

“603.07 Protection Under Traffic. After the casting has been adjusted and the Class PP concrete has been placed, the work shall be protected by a barricade and two lights according to Article 701.17(e)(3)b.”

PUBLIC CONVENIENCE AND SAFETY (DIST 1)

Effective: May 1, 2012

Revised: July 15, 2012

Add the following to the end of the fourth paragraph of Article 107.09:

"If the holiday is on a Saturday or Sunday, and is legally observed on a Friday or Monday, the length of Holiday Period for Monday or Friday shall apply."

Add the following sentence after the Holiday Period table in the fourth paragraph of Article 107.09:

"The Length of Holiday Period for Thanksgiving shall be from 5:00 AM the Wednesday prior to 11:59 PM the Sunday After"

Delete the fifth paragraph of Article 107.09 of the Standard Specifications:

"On weekends, excluding holidays, roadways with Average Daily Traffic of 25,000 or greater, all lanes shall be open to traffic from 3:00 P.M. Friday to midnight Sunday except where structure construction or major rehabilitation makes it impractical."

GROUND TIRE RUBBER (GTR) MODIFIED ASPHALT BINDER (D1)

Effective: June 26, 2006

Revised: December 1, 2021

Add the following to the end of article 1032.05 of the Standard Specifications:

“(c) Ground Tire Rubber (GTR) Modified Asphalt Binder. A quantity of 10.0 to 14.0 percent GTR (Note 1) shall be blended by dry unit weight with a PG 64-28 to make a GTR 70-28 or a PG 58-28 to make a GTR 64-28. The base PG 64-28 and PG 58-28 asphalt binders shall meet the requirements of Article 1032.05(a). Compatible polymers may be added during production. The GTR modified asphalt binder shall meet the requirements of the following table.

Test	Asphalt Grade GTR 70-28	Asphalt Grade GTR 64-28
Flash Point (C.O.C.), AASHTO T 48, °F (°C), min.	450 (232)	450 (232)
Rotational Viscosity, AASHTO T 316 @ 275 °F (135 °C), Poises, Pa·s, max.	30 (3)	30 (3)
Softening Point, AASHTO T 53, °F (°C), min.	135 (57)	130 (54)
Elastic Recovery, ASTM D 6084, Procedure A (sieve waived) @ 77 °F, (25 °C), aged, ss, 100 mm elongation, 5 cm/min., cut immediately, %, min.	65	65

Note 1. GTR shall be produced from processing automobile and/or light truck tires by the ambient grinding method. GTR shall not exceed 1/16 in. (2 mm) in any dimension and shall contain no free metal particles or other materials. A mineral powder (such as talc) meeting the requirements of AASHTO M 17 may be added, up to a maximum of four percent by weight of GTR to reduce sticking and caking of the GTR particles. When tested in accordance with Illinois modified AASHTO T 27, a 50 g sample of the GTR shall conform to the following gradation requirements:

Sieve Size	Percent Passing
No. 16 (1.18 mm)	100
No. 30 (600 µm)	95 ± 5
No. 50 (300 µm)	> 20

Add the following to the end of Note 1. of article 1030.03 of the Standard Specifications:

“A dedicated storage tank for the Ground Tire Rubber (GTR) modified asphalt binder shall be provided. This tank must be capable of providing continuous mechanical mixing throughout by continuous agitation and recirculation of the asphalt binder to provide a

uniform mixture. The tank shall be heated and capable of maintaining the temperature of the asphalt binder at 300 °F to 350 °F (149 °C to 177 °C). The asphalt binder metering systems of dryer drum plants shall be calibrated with the actual GTR modified asphalt binder material with an accuracy of ± 0.40 percent.”

FRICITION AGGREGATE (D1)

Effective: January 1, 2011
 Revised: December 1, 2021

Revise Article 1004.03(a) of the Standard Specifications to read:

"1004.03 Coarse Aggregate for Hot-Mix Asphalt (HMA). The aggregate shall be according to Article 1004.01 and the following.

(a) Description. The coarse aggregate for HMA shall be according to the following table.

Use	Mixture	Aggregates Allowed
Class A	Seal or Cover	<u>Allowed Alone or in Combination</u> ^{5/} : Gravel Crushed Gravel Carbonate Crushed Stone Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag Crushed Concrete
HMA Low ESAL	Stabilized Subbase or Shoulders	<u>Allowed Alone or in Combination</u> ^{5/} : Gravel Crushed Gravel Carbonate Crushed Stone Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag ^{1/} Crushed Concrete
HMA High ESAL Low ESAL	Binder IL-19.0 or IL-19.0L SMA Binder	<u>Allowed Alone or in Combination</u> ^{5/ 6/} : Crushed Gravel Carbonate Crushed Stone ^{2/} Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Concrete ^{3/}

Use	Mixture	Aggregates Allowed								
HMA High ESAL Low ESAL	C Surface and Binder IL-9.5 IL-9.5FG or IL-9.5L	<u>Allowed Alone or in Combination</u> ^{5/} : Crushed Gravel Carbonate Crushed Stone ^{2/} Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag ^{4/} Crushed Concrete ^{3/}								
HMA High ESAL	D Surface and Binder IL-9.5 or IL-9.5FG	<u>Allowed Alone or in Combination</u> ^{5/} : Crushed Gravel Carbonate Crushed Stone (other than Limestone) ^{2/} Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag ^{4/} <u>Other Combinations Allowed:</u> <table border="1" data-bbox="771 1092 1266 1417"> <thead> <tr> <th data-bbox="771 1092 1006 1144">Up to...</th> <th data-bbox="1006 1092 1266 1144">With...</th> </tr> </thead> <tbody> <tr> <td data-bbox="771 1144 1006 1197">25% Limestone</td> <td data-bbox="1006 1144 1266 1197">Dolomite</td> </tr> <tr> <td data-bbox="771 1197 1006 1302">50% Limestone</td> <td data-bbox="1006 1197 1266 1302">Any Mixture D aggregate other than Dolomite</td> </tr> <tr> <td data-bbox="771 1302 1006 1417">75% Limestone</td> <td data-bbox="1006 1302 1266 1417">Crushed Slag (ACBF) or Crushed Sandstone</td> </tr> </tbody> </table>	Up to...	With...	25% Limestone	Dolomite	50% Limestone	Any Mixture D aggregate other than Dolomite	75% Limestone	Crushed Slag (ACBF) or Crushed Sandstone
Up to...	With...									
25% Limestone	Dolomite									
50% Limestone	Any Mixture D aggregate other than Dolomite									
75% Limestone	Crushed Slag (ACBF) or Crushed Sandstone									
HMA High ESAL	E Surface IL-9.5 SMA Ndesign 80 Surface	<u>Allowed Alone or in Combination</u> ^{5/ 6/} : Crushed Gravel Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag No Limestone. <u>Other Combinations Allowed:</u> <table border="1" data-bbox="771 1785 1266 1841"> <thead> <tr> <th data-bbox="771 1785 1006 1841">Up to...</th> <th data-bbox="1006 1785 1266 1841">With...</th> </tr> </thead> <tbody> <tr> <td data-bbox="771 1785 1006 1841"></td> <td data-bbox="1006 1785 1266 1841"></td> </tr> </tbody> </table>	Up to...	With...						
Up to...	With...									

Use	Mixture	Aggregates Allowed	
		50% Dolomite ^{2/}	Any Mixture E aggregate
		75% Dolomite ^{2/}	Crushed Sandstone, Crushed Slag (ACBF), Crushed Steel Slag, or Crystalline Crushed Stone
		75% Crushed Gravel ^{2/}	Crushed Sandstone, Crystalline Crushed Stone, Crushed Slag (ACBF), or Crushed Steel Slag
HMA High ESAL	F Surface IL-9.5 SMA Ndesign 80 Surface	<u>Allowed Alone or in Combination</u> ^{5/ 6/} :	
		Crystalline Crushed Stone Crushed Sandstone Crushed Slag (ACBF) Crushed Steel Slag No Limestone.	
		<u>Other Combinations Allowed:</u>	
		<i>Up to...</i>	<i>With...</i>
		50% Crushed Gravel ^{2/} or Dolomite ^{2/}	Crushed Sandstone, Crushed Slag (ACBF), Crushed Steel Slag, or Crystalline Crushed Stone

- 1/ Crushed steel slag allowed in shoulder surface only.
- 2/ Carbonate crushed stone (limestone) and/or crushed gravel shall not be used in SMA Ndesign 80.
- 3/ Crushed concrete will not be permitted in SMA mixes.
- 4/ Crushed steel slag shall not be used as binder.
- 5/ When combinations of aggregates are used, the blend percent measurements shall be by volume."
- 6/ Combining different types of aggregate will not be permitted in SMA Ndesign 80."

INDEX
FOR
SUPPLEMENTAL SPECIFICATIONS
AND RECURRING SPECIAL PROVISIONS

Adopted January 1, 2021

This index contains a listing of SUPPLEMENTAL SPECIFICATIONS, frequently used RECURRING SPECIAL PROVISIONS, and LOCAL ROADS AND STREETS RECURRING SPECIAL PROVISIONS.

ERRATA Standard Specifications for Road and Bridge Construction
(Adopted 4-1-16) (Revised 1-1-21)

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**Illinois Department
of Transportation**

Check Sheet for Recurring Special Provisions



Local Public Agency	County	Section Number
Village of Downers Grove	DuPage	22-00000-01-GM

Check this box for lettings prior to 01/01/2022.

The Following Recurring Special Provisions Indicated By An "X" Are Applicable To This Contract And Are Included By Reference:

<u>Recurring Special Provisions</u>		<u>Reference Page No.</u>
<u>Check Sheet #</u>		
1	<input type="checkbox"/> Additional State Requirements for Federal-Aid Construction Contracts	1
2	<input type="checkbox"/> Subletting of Contracts (Federal-Aid Contracts)	4
3	<input type="checkbox"/> EEO	5
4	<input type="checkbox"/> Specific EEO Responsibilities Non Federal-Aid Contracts	15
5	<input type="checkbox"/> Required Provisions - State Contracts	20
6	<input type="checkbox"/> Asbestos Bearing Pad Removal	26
7	<input type="checkbox"/> Asbestos Waterproofing Membrane and Asbestos HMA Surface Removal	27
8	<input type="checkbox"/> Temporary Stream Crossings and In-Stream Work Pads	28
9	<input checked="" type="checkbox"/> Construction Layout Stakes	29
10	<input type="checkbox"/> Use of Geotextile Fabric for Railroad Crossing	32
11	<input type="checkbox"/> Subsealing of Concrete Pavements	34
12	<input type="checkbox"/> Hot-Mix Asphalt Surface Correction	38
13	<input type="checkbox"/> Pavement and Shoulder Resurfacing	40
14	<input type="checkbox"/> Patching with Hot-Mix Asphalt Overlay Removal	41
15	<input type="checkbox"/> Polymer Concrete	43
16	<input type="checkbox"/> PVC Pipeliner	45
17	<input type="checkbox"/> Bicycle Racks	46
18	<input type="checkbox"/> Temporary Portable Bridge Traffic Signals	48
19	<input type="checkbox"/> Nighttime Inspection of Roadway Lighting	50
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21	<input type="checkbox"/> Calcium Chloride Accelerator for Portland Cement Concrete	52
22	<input type="checkbox"/> Quality Control of Concrete Mixtures at the Plant	53
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Local Public Agency	County	Section Number
Village of Downers Grove	DuPage	22-00000-01-GM

The Following Local Roads And Streets Recurring Special Provisions Indicated By An "X" Are Applicable To This Contract And Are Included By Reference:

Local Roads And Streets Recurring Special Provisions

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LRS 2	<input type="checkbox"/> Furnished Excavation	102
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LRS 5	<input checked="" type="checkbox"/> Contract Claims	105
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IV. SPECIAL PROVISIONS

The following Special Provisions shall modify, supercede, or supplement the Standard Specifications.

Where any section, subsection, paragraph, or subparagraph of the Standard Specifications is *supplemented* by any of the following paragraphs, the provisions of such section, subsection, paragraph, or subparagraph shall remain in effect. The Special Provisions shall govern in addition to the particular Standard Specification so supplemented, and not in lieu thereof.

Where any section, subsection, paragraph, or subparagraph of the Standard Specifications is *amended, voided, or superceded* by any of the following paragraphs, any provision of such section, subsection, paragraph, or subparagraph standing unaffected, shall remain in effect. The Special Provisions shall govern in lieu of any particular provision of the Standard Specification so amended, voided, or superceded, and not in addition to the portion changed.

1 GENERAL CONSTRUCTION REQUIREMENTS

The following general requirements are intended to govern the overall priority for the performance of the work described in this contract. As general requirements, they are not intended to dictate to the Contractor the precise method by which these tasks shall be performed.

(A) The contractor shall also make special note that any contract work on the following streets prior to June 7, 2022 must be within the hours of 9:00AM-2:00PM Monday through Friday, until after the school year ends. Majority of contract work in this area should then be complete prior to start of the new school year on August 12, 2022, including striping and restoration. Any work after August 12, 2022 must be within the hours of 9:00AM-2:00PM.

a. Fairmount Avenue (North end to 63rd St)

Special consideration to hours and location of work near schools shall be made to allow for full and safe access during normal student arrival and departure schedules.

The Contractor shall maintain traffic flow on All Streets during the day in accordance with the applicable special provision. Adequate signing and flagging is of particular importance for safe travel of all residents.

The Contractor shall conduct his operations to interfere as little as possible with Village employees or the public on or near the Work. All construction work specified under this contract shall be so engaged as to not impede normal traffic and pedestrian ways. Any barricading to detour traffic must receive prior written approval from the Engineer.

Unless otherwise allowed by the Village, non-poured and/or non-finished concrete shall not be allowed to extend over a Saturday and Sunday period. All construction work shall be done such that continuous access to schools or businesses is maintained, although it may be restricted to one lane with proper barricading.

All voids and open excavation remaining adjacent to newly constructed curb and gutter, sidewalks, driveways, etc., must be addressed in a timely manner. For that period prior to full parkway restoration or turf placement, the Contractor shall backfill and grade all disturbed areas in the parkway so as to insure the

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safety of the general public. Parkways shall be left in a safe, clean and usable condition conducive to foot traffic and to the satisfaction of the Village. The Contractor shall also work to keep disturbed areas in the parkway weed free.

All street openings made prior to November 15th shall be fully restored according to the applicable special provisions, and the street reopened to regular traffic upon the availability of hot-mix bituminous concrete. The Contractor shall assume the risk of restoration over those reaches of pipe installed but not yet pressure-tested for pipe integrity.

If the project requires the phasing of construction, the contractor is to follow the phasing shown in the plan set. Any variations in the phasing plan shown on the plan set must be approved in writing by the Engineer before construction begins. The contractor will not be allowed to proceed to another phase without the approval of the Engineer. **The contractor will receive no additional compensation for constructing the project in phases.**

No more than three hundred linear feet (300 LF) of pavement may be open-cut and closed to use by the motoring public, and access to all individual drives within the current work zone must be restored at the end of each workday, unless a Village-approved phasing plan shows otherwise.

2 PREQUALIFICATION

All bidders must supply Certificate of Eligibility from IDOT, Prequalified 003 HMA Plant Mix.

3 COMPLETION TIME

In addition to the completion date of October 7, 2022 listed on Proposal, BLR 12200 Pg. 3 of 6, the Contractor shall note the following. This project incorporates multiple phases of construction with various types of street rehabilitation treatments. Besides the overall time limit of the project, there are also interim deadlines on specific parts of the work in order to reduce the time residents are inconvenienced as a result of the project. Work shall be completed by October 7, 2022 or liquidated damages shall apply, with the exception of Fairmount Avenue, which shall be completed by August 12, 2022 or liquidated damages shall apply. Should the Contractor fail to complete the work within the stipulated time frames and/or prior to the completion date, the Contractor shall be liable for liquidated damages.

Phases and time frames are as follows:

- **Final surface course placement of all streets throughout project shall be completed prior to August 12, 2022.**
- **Streets are organized into geographic groups and order of work will be agreed upon at preconstruction meeting. Controlling item of each group must be completed prior to starting work on next group.**
- **Curb and PCC driveway replacement and permanent driveway restoration shall be completed within 10 calendar days of curb removal. This includes any adjacent sidewalk work and / or replacement of HMA or PCC driveway as designated.**
- **All open excavations remaining adjacent to newly constructed curb and gutter, sidewalks, driveways, etc., shall be properly backfilled, compacted and graded per the specifications within 5 calendar days of their completion.**

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- **The Contractor shall complete final surface course placement within 10 calendar days of pavement milling / surface removal.**
- **Unless otherwise dictated by the specifications, final parkway restoration / sod placement shall be completed within 7 calendar days of a street completing concrete work.**

4 LIQUIDATED DAMAGES

The Contractor must complete the work in accordance with the completion time requirements. If he fails to do so within the times stipulated, the Contractor shall be liable for liquidated damages for each additional calendar day in strict adherence to article 108.09 of the SSRBC, except that liquidated damages shall be fixed at \$1,275.00 per day. The Contractor shall notify the Village in writing when all contract work is completed. Contractor will be allowed 10 working days after all contract work is completed to address punch list items and/or items as deemed by the Village. The contractor is allowed 5-7 calendar days after issuance of punch list to re-mobilize to perform punch list items before the Village begins to charge working days.

Monetary damages will be assessed against the Contractor if he fails to complete each phase of construction as described in this contract, and the overall completion of this project within the stipulated time frames, not as a penalty but liquidated damages for delay in completion of work.

The Contractor must read carefully the special provisions pertaining to each portion of work. Certain parts or phases of the proposed work will have intermittent time frames stipulated to lessen the disruption to affected and adjacent residents and businesses.

Phases and time frames are as follows:

- **Final surface course placement of all streets throughout project shall be completed prior to August 12, 2022.**
- **Streets are organized into geographic groups and order of work will be agreed upon at preconstruction meeting. Controlling item of each group must be completed prior to starting work on next group.**
- **Curb and PCC driveway replacement and permanent driveway restoration shall be completed within 10 calendar days of curb removal. This includes any adjacent sidewalk work and / or replacement of HMA or PCC driveway as designated.**
- **All open excavations remaining adjacent to newly constructed curb and gutter, sidewalks, driveways, etc., shall be properly backfilled, compacted and graded per the specifications within 5 calendar days of their completion.**
- **The Contractor shall complete final surface course placement within 10 calendar days of pavement milling / surface removal.**
- **Unless otherwise dictated by the specifications, final parkway restoration / sod placement shall be completed within 7 calendar days of a street completing concrete work.**

5 ACCESS AND WATER SHUT OFF NOTIFICATION

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If access to a driveway will be blocked, or water will be turned off, the Contractor shall give that resident or business proper written notification at least 24 hours in advance. The Contractor must provide them the opportunity to remove their cars from the drive or make other arrangements, and prepare for any shutdown of the water system. Samples of written notices shall be submitted to the Engineer for approval.

In addition, the Contractor shall be responsible for notifying the resident or business verbally on the morning of any driveway closure, to ensure awareness of the lack of access.

Basis of Payment: This work shall be considered **INCIDENTAL** to the project.

6 EXISTING UTILITIES

Existing Public Utilities, such as watermains, sewers, gas lines, streetlights, telephone lines, electric power lines, etc., shall be protected against damage during the construction of this project. The Contractor shall contact the Owners of all public utilities and obtain locations of all utilities within the limits of the proposed construction and make arrangements, if necessary, to adjust or move any existing utility at the utility company's expense. Any expense incurred by the contractor in connection with making arrangements shall be borne by the Contractor and considered incidental to the contract. It shall be this Contractor's responsibility to determine the actual location of all such facilities in the field.

The adjustment of all facilities of Nicor, AT&T, the Commonwealth Edison Co., etc. shall be done by the respective utility company, and if known, are indicated on the plans as to be done "By Others". All other utility adjustments to sewer, water, and local facilities shall be performed under this contract, under the supervision of the Owner of the utility, and will be paid for under the respective items in the contract unless otherwise indicated on the plans or directed by the Engineer.

Any existing facilities, residential or commercial sprinkler systems, etc. disturbed are the responsibility of the property/utility owner. The contractor shall treat as regular utility if marked. If not marked, contractor shall treat as a utility in an unanticipated location per Sec. 107 of the Standard Specifications. The contractor shall notify the Village when a utility has been damaged. The cost of repairs of any damaged utility shall be repaired at no cost to the Village.

Whenever the locations of existing utilities are known, the approximate location of said utility is indicated on the plans. This information is given only for the convenience of the Bidder and the Village assumes no responsibility as to accuracy of the information provided. The Contractor shall consider in his bid the location of all permanent and temporary utility appurtenances to their present or relocated positions, whether shown on the plans or not, and no additional compensation will be allowed for delays, inconvenience, or special construction methods required due to the existence of said appurtenances.

Whenever obstructions are encountered during the progress of the work and interfere to such an extent that an alteration in the plan is required, the Engineer shall order a deviation in the plan as required, the Engineer shall order a deviation in the line and/or grade to resolve the conflict, or relocation of the obstruction. The Contractor will be compensated for any additional pipe material, fittings, granular backfill, or structures required at the respective contract prices, and measured as specified in the Contract. No additional compensation will be allowed for delays or inconveniences, additional excavation, or any special construction methods required in prosecuting the work due to the existence of said obstruction.

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7 COMBINATION CONCRETE CURB AND GUTTER REMOVAL

Description: This work shall consist of the removal of existing P.C.C. Curb and Gutter of the type and size at the locations noted in Schedule of Quantities. This work shall be performed in accordance with Section 440 of the Standard Specifications, except as amended herein.

Unless otherwise allowed by the engineer, curb and gutter removal and replacement shall be done on one side of a street at a time to allow for on street parking. No curb shall be removed from the opposite side of the street until completion of curb replacement and full access to driveways is restored on the first side.

This work shall include a full depth, perpendicular, straight joint sawn at the ends and all edges, including along the edge of pavement, of portions to be removed, unless otherwise directed by the engineer.

At those locations where curb removal operations fall within the Critical Root Zone (CRZ) the Contractor will be required to trench with a "chain" driven trencher immediately back of curb prior to curb removal. This procedure will proceed uninterrupted through the CRZ and insure general tree root pruning. The width of the CRZ shall be determined as noted in the general provision for TREE PROTECTION elsewhere in these documents. If it is determined that proposed removal methods do not cause undo harm to adjacent roots, the Village Forester may waive the need to perform trenching.

During removal operations Contractor shall take special care not to damage or extend sawed joint into adjacent appurtenances such as driveways and sidewalks which are to remain in place. During machine sawing operations Contractor shall also take special care to remove, clean, or otherwise account for any residue / slurry produced by the sawing so material will not be tracked by either vehicular or foot traffic onto adjacent appurtenances which are to remain in place.

Basis of Payment: This work shall be paid for at the contract unit price per **LINEAR FOOT** for:

COMBINATION CONCRETE CURB AND GUTTER REMOVAL

which price shall be payment in full for all work specified herein.

8 COMBINATION CONCRETE CURB AND GUTTER OF TYPE SPECIFIED

Description: This work shall consist of the replacement of existing PCC Curb and Gutter in accordance with the applicable parts of Sec. 606 of the Standard Specifications, except as amended herein.

Replacement of curb and gutter shall include the placement of three-quarter inch (3/4") premolded expansion joint filler along the back of curb, for the full depth of the curb and gutter, where abutting existing concrete.

Transverse expansion joints with 3/4" joint filler shall be constructed at five feet (5') either side of utility structures, and at no more than ninety foot (90') intervals. All expansion joints shall include the placement of two (2) three-quarter inch (3/4") dowel bars with pinched stop caps as specified on detail sheet. Two (2)

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three quarter inch (3/4") dowel bars shall also be placed at all construction joints as specified on detail sheet and shall be drilled into existing curb and gutter a minimum of six inches (6").

New curb and gutter shall be backfilled with existing excavated earth.

Transverse contraction joints shall be constructed at no more than fifteen foot (15') intervals. When new curb and gutter is placed adjacent to concrete pavement or base, it shall be tied along the longitudinal construction joint with No. 6 (3/4") bars at 24" centers in accordance with the applicable portions of Article 420.05 of the Standard Specifications.

Placement of curb or curb and gutter as noted on Schedule of Quantities to be reinforced shall also include the placement of two (2) No. 4 (1/2") epoxy coated deformed reinforcement bars meeting the applicable portions of Section 508 of the Standard Specifications. Bars shall be placed at one-half depth of the body of the gutter running the entire length of newly placed sections. Curb or curb and gutter placed as described in this paragraph shall be paid for as CONCRETE CURB (TYPE SPECIFIED), REINFORCED or COMBINATION CONCRETE CURB AND GUTTER (TYPE SPECIFIED), REINFORCED.

All voids existing between newly placed curb and gutter and the adjacent roadway pavement shall be filled with Class SI concrete, prior to bituminous surface placement, to a point 1-1/2 inches below finish grade. This work shall be considered incidental.

All curb and gutter that is in front of sidewalk crossings shall have a cross-slope of a minimum of one percent (1.0%) and a maximum of two percent (2.0%) to facilitate drainage and shall have a defined flow line of not greater than one half inch (1/2") from back of curb.

Placement of curb and gutter shall include the application of membrane curing compound, Type III, in accordance with Articles 1020.13 and 1022.01 of the Standard Specifications unless otherwise directed by the Engineer.

If placement of curb and gutter takes place prior to April 15, or after November 1, the curb and gutter shall be properly cured and that followed by the application of protective coat in accordance with Article 420.18 of the Standard Specifications.

Basis of Payment: This work shall be paid for at the contract unit price per **LINEAR FOOT** for:

COMBINATION CONCRETE CURB AND GUTTER (TYPE SPECIFIED) or COMBINATION CONCRETE CURB AND GUTTER (TYPE SPECIFIED), REINFORCED

which price shall be payment in full for the work as specified herein.

9 POROUS GRANULAR EMBANKMENT, SPECIAL

Description: This work shall consist of removing and disposing of unsuitable sub-grade, furnishing, placing and compacting porous granular material to the lines and grades shown on the plans or as directed by the Engineer in accordance with the applicable portions of Sections 202 and 207 of the Standard Specifications.

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The material shall be used as a bridging layer over soft, pumpy, loose soil areas and for placement under water. The material shall conform with Article 1003.04 and 1004.05 of the Standard Specifications except the gradation shall be as follows:

1. Crushed Stone, Crushed Blast Furnace Slag and Crushed Concrete

<u>Sieve Size</u>	<u>Percent Passing</u>
*6"	97 \pm 3
*4"	90 \pm 10
2"	45 \pm 25
#200	5 \pm 5

2. Gravel, Crushed Gravel and Pit Run Gravel

<u>Sieve Size</u>	<u>Percent Passing</u>
*6"	97 \pm 3
*4"	90 \pm 10
2"	55 \pm 25
#4	30 \pm 20
#200	5 \pm 5

*For undercuts less than 18" the percent passing the 6" sieve may be 90 \pm 10 and the 4" sieve requirement eliminated.

The porous granular material shall be placed in one lift when the total thickness to be placed is two (2) feet thick or less or as directed by the Engineer. Rolling each lift of the porous granular material with a vibratory roller meeting the requirements of Article 1101.01 of the Standard Specifications should be sufficient to obtain the desired keying or interlock and necessary compaction. The Engineer shall verify that adequate keying has been obtained.

A three- (3) inch nominal thickness top lift of capping aggregate having a gradation of CA-6 will be required. The use of on-site bituminous grindings resulting from bituminous surface removal, substantially meeting the gradation of CA-6, shall also be permitted. The granular cap shall be compacted to the satisfaction of the Engineer. It shall be the Contractor's responsibility that all proposed bituminous replacement regarding patching and paving operations in these areas will meet the specified performance criteria of their respective pay items.

Construction equipment not necessary for the completion of the replacement material will not be allowed on the undercut areas until completion of the recommended thickness of the porous granular embankment, special.

This work will be measured for payment in accordance with Article 207.04 of the Standard Specifications. When specified on the contract, the theoretical elevation of the bottom of the aggregate subgrade shall be used to determine the upper limit of Porous Granular Embankment, Special. The volume will be computed by the method of average end areas.

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Basis of Payment: This work shall be paid for at the contract unit price per **CUBIC YARD** for:

POROUS GRANULAR EMBANKMENT, SPECIAL,

which price shall include the capping aggregate, as required.

The Porous Granular Embankment, Special shall be used as field conditions warrant at the time of construction. No adjustment in unit price will be allowed for an increase or decrease in quantities from the estimated quantities shown on the plans.

10 EARTH EXCAVATION, SPECIAL

Description: This work shall consist of the excavation, removal, and disposal of existing materials located on site required for installation of sidewalk ramp in locations where existing material is in excess of removal and replacement/installation (greater than 9" removal for New Sidewalk Installation, greater than 3" removal for HMA Driveway Replacement, etc). This work shall be as specified and in accordance with Sections 202, 204, 205 and 440 of the SSRBC and as specified herein.

Method of Measurement: This work will be measured for payment in their original positions, and volumes in cubic yards will be computed by the method of average end areas.

Basis of Payment: This work shall be measured and paid for at the contract unit price per **CUBIC YARD** for:

EARTH EXCAVATION, SPECIAL,

which shall include all labor, materials and equipment necessary to do the work.

11 MANHOLES OR INLETS, TO BE ADJUSTED OR RECONSTRUCTED

Description: This item shall be done in accordance with Sec. 602 of the Standard Specifications for Road and Bridge Construction and the following provisions.

All excavation for structure adjustment shall be replaced with Class SI concrete and in accordance with the attached details. For excavation required for reconstructed items, backfill materials shall be mechanically compacted **SELECTED GRANULAR BACKFILL** placed per the special provision elsewhere in these documents.

Castings shall be set in full mortar or bituminous mastic beds. The adjustment of the casting to the required final grade shall be made with precast concrete adjusting rings. Brick, concrete block, or wooden shims will not be permitted.

When adjustments include new frame and grate or new frame and lid, all replacement frames, grates and lids shall be heavy duty. Depending on the type of frame, care shall be taken to properly align the new frame with the curb and gutter, and maintain the proper size opening into the structure.

Although the cost of adjusting structures per this specification will be paid for under this contract, the Contractor shall be aware that many of the structures are not the property of the Village of Downers Grove,

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and that such work may require inspections and/or permits from other governmental agencies.

For those structures noted on the Schedule of Quantities or as designated by the Engineer as MANHOLE TO BE ADJUSTED, SPECIAL, for that period after Hot-Mix Asphalt Surface Removal operations and prior to adjustment to finished pavement elevation, frames and lids or grates shall be removed from the structure and stored in a safe manner until reused. The resulting void over the structure shall be covered with a steel plate and temporary pavement, or other approved method, capable of carrying the anticipated daily traffic in a safe manner. The Contractor shall also make note of structure location so it may be reestablished after initial bituminous paving operations have been completed.

For those structures designated as INLET TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE, SPECIAL, the new frame and grate shall be a standard Type 3, or approved equal, except the barred curb box shall be replaced with an open face curb box.

Basis of Payment: This item shall be paid for at the contract unit price Each for MANHOLE TO BE ADJUSTED or MANHOLE TO BE ADJUSTED, SPECIAL or MANHOLE TO BE ADJUSTED WITH NEW FRAME AND LID (TYPE SPECIFIED) or MANHOLE TO BE RECONSTRUCTED.

This item shall also be paid for at the contract unit price **EACH** for:

INLET TO BE ADJUSTED or INLET TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE, SPECIAL or INLET TO BE RECONSTRUCTED

which price shall be payment in full for all labor and materials specified herein including backfill with Selected Granular Backfill.

12 TREE ROOT PRUNING

Description: All trees, public or private, affected by new sidewalk installation within its root protection zone, shall be root pruned prior to any excavation taking place. Root pruning shall be performed in accordance with the applicable portions of Section 201 of the Standard Specifications as well as the Tree Protection Zone detail of the Plans. Root pruning shall be done only to the depth of the excavation necessary for installing the new walk. Root pruning shall start and proceed uninterrupted for the length of travel through the root protection zone. Root pruning shall be made no more than 10 inches from the tree-side edge of the proposed walk.

Approval by the Village Forester of the equipment to be used for root pruning, as well as the proposed path of the root pruning work, is required prior to the work being performed. The Engineer or his representative shall permit no excavation until written approval is obtained by the Contractor from the Village Forester. Additionally, no materials or equipment may be stored or kept in the Tree Protection Zone. Tree damage, as determined by the Village Forester, shall be assessed to the Contractor using the most recent edition of the Guide for Plant Appraisal, published by the International Society of Arboriculture.

Basis of Payment: This work shall be paid for at the contract unit price per **EACH** for:

TREE ROOT PRUNING.

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13 PORTLAND CEMENT CONCRETE SIDEWALK

Description: This work shall consist of the removal and replacement of P.C.C. Sidewalk in accordance with the SSRBC, except as amended herein.

Sidewalk removal and replacement shall be done on one side of a street at a time to allow for pedestrian mobility. No sidewalk shall be removed from the opposite side of the street until sidewalks on the first side are safely open to pedestrian traffic.

Removal of sidewalk shall include the saw cutting of existing concrete as directed by the Engineer. Except for those locations specifically marked for Tree Root Pruning, removal of sidewalks shall also include any necessary additional pruning and removal of tree roots, bituminous paved sidewalks and/or bituminous overlayment of existing sidewalks, or excavation necessary to place the proposed sidewalk, curb ramp or side curb.

Replacement of sidewalk shall be of the width and thickness as noted on the Schedule of Quantities and as directed by the Engineer. Thickness of the proposed sidewalk shall generally be (5") five inch for standard or courtesy walks, (6") six inch for full width across residential drives, and (8") eight inch for full width across commercial drives.

Placement of P.C.C. sidewalk shall include the excavation for and placement of four inches (4") of Type B, CA-6, compacted aggregate base, the (3/4") three-quarter inch scoring of contraction joints (5') five feet on center, the placing of (1/2") one-half inch premolded expansion joints where new concrete abuts existing concrete and/or at (50') fifty feet on center and/or at the end of a pour. This work shall also include the adjustment to proper grade of all water valve or utility boxes encountered.

Replacement of sidewalk shall include the application of membrane curing compound, Type III, in accordance with Articles 1020.13 and 1022.01 of the Standard Specifications unless otherwise directed by the Engineer.

At those locations where existing street configuration does not contain curb and gutter, it is necessary to end construction of new sidewalk with a minimum of two (2) feet separation from the existing or proposed edge of pavement. At these locations, a HMA transition sidewalk shall be constructed between the concrete sidewalk and the edge of pavement.

Construction of the transition sidewalk shall include excavation as necessary for the full width of the concrete sidewalk, placement and compaction of the four inches (4") of Type B, CA-6 aggregate base, and the placement and compaction of 5 inches (5") of Hot-Mix Asphalt Surface, Mixture D, N50 (IL 9.5) per the applicable portions of Sec. 442 of the Standard Specifications. Asphalt to be placed in compacted layers not to exceed four inches (4").

Hot-Mix Asphalt Binder Course, IL-19.0, N50 may be utilized for the bottom courses, but in all cases the top course shall be a minimum 1 ½ inch lift of the HMA Surface noted above.

For those locations as noted on the Schedule of Quantities or as designated by the Engineer for Detectable Warnings, work shall be completed in accordance with Section 424 of the SSRBC and the Standards

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included in the details regarding curb ramps with detectable warnings and as amended herein.

Detectable Warnings will NOT include any placement of full depth red dyed concrete or other on-site fabrication such as stamping or molding the fresh concrete with coloring added to the surface of the concrete.

Detectable Warnings shall be limited to inserts meeting the requirements of the ADAAG and subject to approval by the Village.

Color of detectable warnings shall be brick red. The area of red detectable warning shall be protected from overspray during the application of Type III membrane curing compound.

If replacement of sidewalk takes place prior to April 15, or after November 1, all sidewalk shall be properly cured and that followed by the application of protective coat in accordance with Article 420.18 of the Standard Specifications.

Basis of Payment: This work shall be paid for at the contract unit price per **SQUARE FOOT** for:

PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL and for **PORTLAND CEMENT CONCRETE SIDEWALK, 5"** or **PORTLAND CEMENT CONCRETE SIDEWALK, 6"**

which price shall be payment in full for the work as specified herein except for Detectable Warnings which shall be paid for separately.

Detectable warnings shall be paid for at the contract unit price per **SQUARE FOOT** for:

DETECTABLE WARNINGS

which price shall be in addition to the cost for placement of the 5" sidewalk at the curb ramp.

14 PARKWAY RESTORATION

Description: This item shall be done in accordance with the applicable portions of Sec. 252 of the Standard Specifications and the following provisions.

As contract work progresses through the Village, parkway restoration work shall commence in a timely manner in areas where permanent placement of new curb and gutter, driveways, sidewalks, etc., has been completed. **Parkway restoration including sod placement or topsoil and growth-inhibiting erosion control blanket shall be completed on a street within 7 calendar days of completion of concrete work.** Under no circumstances shall the Contractor prolong final grading, shaping and sod placement so that the entire project can be permanently restored at the same time.

This work shall consist of the excavation, topsoiling and sodding from a minimum of one and one-half (1-1/2) feet to a maximum of three (3) feet behind or adjacent to all curbs, sidewalks and driveways removed and replaced during the course of construction or as directed by the Engineer. Restoration will also be performed on areas disturbed by storm sewer or culvert construction.

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A number of locations may require extensive excavation or regrading of the parkway due to alignment change necessary to bring corner sidewalk ramps within ADA compliance.

All topsoil to be used for parkway restoration shall be obtained from outside the limits of this improvement, transported to the site and placed at required locations to a minimum depth of 4". All materials shall meet the requirements of Art. 1081.05 of the Standard Specifications. All placement of topsoil shall meet the requirements of Sec. 211 of the Standard Specifications.

All sod shall be an approved grass that is native to the locality of work meeting the requirements of Art. 1081.03 of the Standard Specifications. All placement of sod shall meet the requirements of Sec. 252 of the Standard Specifications.

If timing of restoration work falls outside of specifications for sod placement, topsoil and growth-inhibiting erosion control blanket shall be installed within the required restoration timeframe and sod shall be installed when specifications allow. The material specifications shall be submitted to the Village for approval prior to use. The blanket shall consist of a material which inhibits the growth of weeds, such that the area does not require additional tilling of topsoil prior to sod placement. The installation of the blanket shall follow manufacturer's specifications such that no soil or debris shall run off from the disturbed areas. Following the use of any blanket, the Contractor shall remove the product from the site.

For that period prior to full parkway restoration, the Contractor shall backfill and grade all disturbed areas so as to insure the safety of the general public. **All open excavations remaining adjacent to newly constructed curb and gutter, sidewalks, driveways, etc., shall be properly backfilled, compacted and graded within 5 calendar days of their completion.**

Backfill shall be compacted by mechanical and/or hand methods so future consolidation / settlement does not occur. Parkway shall be left in a safe, clean and usable condition conducive to foot traffic and to the satisfaction of the Village. The Contractor shall protect these unfinished areas against erosion and work to keep them weed free. Erosion control work such as placement of temporary seed or erosion control blanket, including their removal and redressing of the disturbed areas, shall not be paid for separately but shall be considered incidental to the cost of PARKWAY RESTORATION.

Basis of Payment: This work shall be paid for at the contract unit price per **SQUARE YARD** for:

PARKWAY RESTORATION

which price shall be payment in full for any excavation and grading necessary, the furnishing, transporting and placement of all topsoil and sod, and the full watering of sod. Unless otherwise directed by the Engineer, restoration of disturbed parkways more than three (3) feet behind the back of curb or more than three (3) feet adjacent to newly constructed driveway or sidewalk or more than six (6) feet either side of the newly placed storm sewer or pipe culvert will not be paid for separately but shall be considered incidental to the contract. The installation of temporary growth-inhibiting erosion control blanket shall be paid for at the contract unit price per **SQUARE YARD** for:

GROWTH-INHIBITING EROSION CONTROL BLANKET.

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15 HOT-MIX ASPHALT DRIVEWAY

Description: This work shall consist of the removal and replacement of asphalt driveways at locations indicated on the plans and/or as required by the Engineer.

The replacement of the driveways shall consist of preparing a subgrade at all required locations, shaping of slopes adjacent to the driveways, the placement and compacting of six inches of CA-6 Aggregate Base, and the placement and compacting of three inches (3") of Hot-Mix Asphalt Surface, Mixture D, N50 (IL 9.5).

At locations noted on Schedule of Quantities, asphalt driveways shall be replaced with the six inches of CA-6 aggregate base along with eight inches (8") of Hot-Mix Asphalt comprised of six inches (6") of Hot-Mix Asphalt Binder, IL-19.0, N50 and finished with a minimum of two inches (2") of Hot-Mix Asphalt Surface, Mixture D, N50 (IL 9.5). Asphalt to be placed in compacted layers not to exceed four inches (4").

This work shall also include the adjustment to proper grade of all water valve or utility boxes encountered.

Where the edges of the new driveway pavement are exposed adjacent to the parkway, the edges shall have a neat forty-five (45) degree angle bevel shaped, compacted and tamped tight by mechanical and/or hand methods.

The locations at which this work will be measured for payment will consist of only those areas bounded by combination concrete curb and gutter. Those areas where the surface course of the pavement flares into existing driveways beyond the limits of the fully improved areas will not be included for payment.

Basis of Payment: This work will be paid for at the contract unit price per **SQUARE YARD** for:

**HOT-MIX ASPHALT DRIVEWAY REMOVAL and for HOT-MIX ASPHALT DRIVEWAY
PAVEMENT, 3"**

which price shall be payment in full for all work as specified herein.

16 PORTLAND CEMENT CONCRETE DRIVEWAY

Description: This work shall consist of the removal and replacement of concrete driveways in accordance with the applicable parts of Sec. 423 of the SSRBC except as amended herein.

This work shall include the placement of three-quarter inch (3/4") premolded expansion joint filler, for the full depth of the driveway pavement, where new concrete abuts existing concrete or as directed by the Engineer.

This work shall also include the adjustment to proper grade of all water valve or private utility boxes encountered.

Replacement of the driveways shall include the application of membrane curing compound, Type III, in accordance with Articles 1020.13 and 1022.01 of the SSRBC, unless otherwise directed by the Engineer. If replacement of the driveways takes place prior to April 15, or after November 1, the driveway shall be properly cured and that followed by the application of protective coat in accordance with Article 420.18 of

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the Standard Specifications.

Basis of Payment: This work will be paid for at the contract unit price per **SQUARE YARD** for:

PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL and for PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6"

which price will be payment in full for all work as specified herein.

17 DECORATIVE PAVER DRIVEWAY OR SIDEWALK REMOVAL & REPLACEMENT

Description: This work shall consist of removal and replacement of existing decorative concrete or brick paver driveways or sidewalks per the applicable portions of Check Sheet LRS 14 of the SSRBC except as amended herein.

At those locations noted on the plans or as directed by the Engineer, the Contractor shall remove existing decorative pavers in such a manner so that no damage occurs to the pavers and with full intent to reuse said paver blocks. Any decorative paver block damaged to an extent that it may not be reused as part of the final pavement, sidewalk or driveway shall be replaced in kind by the Contractor at no additional cost to the Village.

Extent of existing paver removal shall be at the direction of the Engineer. This removal will only be that amount necessary to construct the new curb and gutter or other appurtenance, and replace the decorative pavers to an acceptable grade and appearance.

At those locations where it is determined that an existing bituminous base warrants removal and replacement or repair, this portion of the work would be performed and measured for payment per the special provision for CLASS D PATCHING, of the necessary thickness.

Basis of Payment: This work shall be paid for at the contract unit price per **SQUARE YARD** for:

DECORATIVE PAVER DRIVEWAY REMOVAL AND REPLACEMENT or DECORATIVE PAVER SIDEWALK REMOVAL AND REPLACEMENT,

which price shall be payment in full for all materials and work as specified herein.

18 MANHOLE AND INLET CONSTRUCTION

Description: This work shall consist of the construction of precast concrete drainage structures of the size and type shown on the plans or specified by the Engineer. Included in the contract unit price shall be all excavation, bedding, backfilling and reconnection of all existing inlet and outlet pipe. For all new structures backfill materials shall be mechanically compacted **SELECTED GRANULAR BACKFILL** placed per the special provision elsewhere in these documents.

All structures in excess of four feet in depth shall be equipped with cast iron steps meeting the standards of ASTM A48. Precast sections shall conform to ASTM C 478 and shall be substantially free from fractures, large or deep cracks and surface roughness. Joints between precast sections shall be designed for rubber gaskets or bituminous mastic material.

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Adequate foundation for all structures shall be obtained by removal and replacement of unsuitable materials with well graded granular material; or by tightening with coarse ballast rock, or by such other means as provided for foundation preparation of the connected sewers.

Precast base sections, risers and bottoms, shall be one piece and shall be placed on a well graded granular bedding of not less than two (2) inches in thickness. The bedding course shall be firmly tamped and made smooth and level to assure uniform contact and support of the precast element.

All lift holes shall be completely filled with mortar to ensure water tightness.

Castings shall be set in full mortar or bituminous mastic beds. The adjustment of the casting to the required final grade shall be made with precast concrete adjusting rings set in full mortar or bituminous mastic beds. Maximum adjustment with rings shall be twelve (12) inches. Brick, concrete block, or wooden shims will not be permitted.

In pavements, frames and grates or lids shall be heavy duty.

Basis of Payment: This work shall be paid for at the contract unit price **EACH** for:

INLET, TYPE A, 24" WITH NEW FRAME AND GRATE (TYPE SPECIFIED) or INLET, TYPE B, 36" WITH SALVAGED FRAME AND GRATE,

which price shall be payment in full for all labor and materials specified herein including **SELECTED GRANULAR BACKFILL**.

19 **SELECTED GRANULAR BACKFILL**

Description: All trenches and excavations beneath pavements and driveways, as shown on the plans or as directed by the Engineer in the field, will require **SELECTED GRANULAR BACKFILL**.

Such material shall meet the applicable requirements of Section 1004 of the SSRBC, except as amended herein. Except for the capping aggregate, the material will meet the gradation for CA-7, CA-11 or the gradation commonly known as ¾" chip.

Backfill shall be placed in maximum 12" lifts and compacted by vibrating plate or other mechanical compacting device in a manner consistent with the Standard Specifications, to ensure that no future settlement occurs.

All backfilling shall be done in accordance with Section 20-2.21 of the Standard Specifications for Water and Sewer Main Construction in Illinois. Specifically, all trenches and excavations other than those shown on the plans or designated by the Engineer to receive **SELECTED GRANULAR BACKFILL** shall be backfilled by any acceptable method which will not dislodge or damage the pipe, or cause bridging action in the trench. After **SELECTED GRANULAR BACKFILL** is placed as haunching to one-half pipe outside diameter, spoil material may be used as backfill in turf areas.

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All backfilling, including granular bedding and backfill of approved excavated material, and placement and compaction of SELECTED GRANULAR BACKFILL around new or reconstructed storm sewer or structures shall be considered incidental to the contract.

When Select Backfill is placed to the existing surface elevation and used as a temporary driving or walking surface, this item shall also include the maintenance of trench surface in a safe and usable condition, satisfactory to the engineer, until the permanent proposed pavement or walkway is completed.

This item also includes the disposal of the surplus excavated material that is replaced by selected granular backfill. Any material meeting the aforementioned gradation that has been excavated from the trenches may be used for backfilling the trenches. However, no compensation will be allowed as selected granular backfill for the portion of the trench backfilled with excavated material.

Basis of Payment: All work to backfill around new and reconstructed storm sewer or structures with SELECTED GRANULAR BACKFILL shall be considered **INCIDENTAL** to each respective pay item and will not be paid for separately.

20 EROSION, SEDIMENTATION AND DUST CONTROL

Description: Throughout each and every phase of the project, all downstream ditches and storm sewers shall be protected from the run-off of roadway surfaces, excavations, and other construction activities generating the movement of dirt, mud, dust and debris. This work shall consist of constructing temporary erosion and sedimentation control systems as shown on the plans or as directed by the Engineer. The work shall be placed by methods and with materials in accordance with Sections 280, 1080 and 1081 of the SSRBC, except as amended herein.

All roadway surfaces shall be kept free of dirt, mud, dust and debris of any kind at all times through all phases of the project. All downstream ditches shall be protected from erosion and sedimentation by the installation of silt fence ditch checks; straw bales shall not be used. Piles of excavated material and/or trench backfill material, allowed to be in place in excess of three days, shall be protected against erosion and sedimentation runoff by use of silt fence or sediment filter logs. Storm sewer inlet structures or manholes shall be protected by temporary placement of geotextile fabric, filter baskets, or solid lids, as authorized in the field by the Engineer.

Dirt, mud, dust and debris of any kind shall be removed from the roadway surface to the satisfaction of the Engineer by any one or combination of the following: approved mechanical sweeping equipment, manual labor, or other approved techniques.

Erosion and sedimentation control measures as indicated in the Erosion Control Plan, or as directed by the Engineer shall be installed on the project site prior to beginning any construction activities which will potentially create conditions subject to erosion. Erosion control devices shall be in place and approved by the Engineer as to proper placement and installation prior to beginning other work. Erosion control protection for Contractor equipment storage sites, plant sites, and other sites shall be installed by the Contractor and approved by the Engineer prior to beginning construction activities at each site.

On those streets designated for Aggregate Base Repair and Preparation of Aggregate Base, dust control shall include the application of water to the existing aggregate base, as conditions warrant, by water truck.

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or other approved method. Unless otherwise directed by the Engineer, during dry periods between rains, a minimum of two applications per day will be necessary.

Temporary or permanent storage in the flood plain of the following are prohibited unless elevated or flood proofed to one foot above the base flood elevation:

- Items susceptible to flood damage; or
- Unsecured buoyant materials or materials that may cause off-site damage including bulky materials, flammable liquids, chemicals, explosives, pollutants, or other hazardous materials; or
- Landscape waste.

Silt Fence Placement, maintenance, and removal of silt fence at areas designated by the Engineer. The work shall be placed by methods and materials in accordance with Sections 280 and 1080 of the SSRBC, except as amended herein.

Erosion Barrier, Special Placement, maintenance, and removal of EROSION BARRIER, SPECIAL shall be by methods and materials in accordance with applicable portions of Sections 280, 1080 and 1081 of the SSRBC, except as amended herein.

Barrier shall be placed approximately two (2 ft) +/- off edge of existing pavement or sidewalks being repaired at those locations noted on the schedule of quantities or as designated by the Engineer.

Barrier shall consist of a combination of two (2) excelsior logs or sediment filter logs staked immediately adjacent and parallel to each other. Barrier is intended to protect more sensitive wetland vegetation and turf areas from runoff and any and all workers and equipment during the duration of the improvements. All contract work near these designated sections shall take place outside the EROSION BARRIER, SPECIAL.

Basis of Payment: This work shall be paid for at the contract unit price **LUMP SUM** for:

EROSION, SEDIMENTATION, AND DUST CONTROL,

which price will be payment in full for all work as specified herein.

21 HOT-MIX ASPHALT BINDER AND SURFACE COURSE

This item shall be done in accordance with all applicable parts of Sections 406 and 1030 of the SSRBC, the included D-1 and BDE Specifications, and included mix table.

All preparation of the existing base shall be considered incidental to its respective pay item. This shall include but not be limited to cleaning cracks with an air compressor or other approved method prior to placement of mixture for cracks, joints and flangeways.

The target value for the air voids of the Hot-Mix Asphalt Surface Course, Mix D, N50 shall be 3.5% at the design number of gyrations.

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Basis of Payment: The HMA surfacing shall be paid for at the contract unit price per TON for:

LEVELING BINDER (MACHINE METHOD), N50, and HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50.

22 BITUMINOUS MATERIALS, TACK COAT (TRACKLESS)

Description: This work shall consist of the application of tack coat in accordance with the applicable parts of Sec. 406 of the Standard Specifications except as amended herein.

It is the responsibility of the contractor to notify the Village twenty four (24) hours in advance of any tack coat applications so it can be verified that signs and traffic control plans are in place. The contractor shall only apply tack coat in areas that can be paved with new asphalt in the same working day. Trackless tack shall be used to minimize tracking.

If lane cannot be closed to traffic until the material has been allowed to break, sand must be applied to prevent tracking. The cleaning of any tracking or stains on driveways will be the responsibility of the contractor, to the satisfaction of the Village.

Basis of Payment: This work shall be paid for at the contract unit price per POUND of Residual Asphalt for:

BITUMINOUS MATERIALS (TACK COAT),

which price will be payment in full for all work as specified herein. Any sand used shall be considered **INCIDENTAL** to the project.

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State of Illinois
Department of Transportation
Bureau of Local Roads and Streets

SPECIAL PROVISION
FOR
EQUIPMENT RENTAL RATES

Effective: January 1, 2012

All references to Sections or Articles in this specification shall be construed to mean a specific Section or Article of the Standard Specifications for Road and Bridge Construction, adopted by the Department of Transportation.

Replace Article 109.04(b)(4) with the following:

- "(4) Equipment. For any machinery or special equipment (other than small tools) the use of which has been authorized by the Engineer, the Contractor will be paid according to the latest revision of "SCHEDULE OF AVERAGE ANNUAL EQUIPMENT OWNERSHIP EXPENSE" and latest index factor as issued by the Illinois Department of Transportation. The equipment should be of a type and size reasonably required to complete the extra work."

BDE SPECIAL PROVISIONS
For the January 21, 2022 and March 11, 2022 Lettings

The following special provisions indicated by a "check mark" are applicable to this contract and will be included by the Project Coordination and Implementation Section of the BD&E. An * indicates a new or revised special provision for the letting.

File Name #	Special Provision Title	Effective	Revised
* 80099 1	<input type="checkbox"/> Accessible Pedestrian Signals (APS)	April 1, 2003	Jan. 1, 2022
80274 2	<input type="checkbox"/> Aggregate Subgrade Improvement	April 1, 2012	April 1, 2016
80192 3	<input type="checkbox"/> Automated Flagger Assistance Device	Jan. 1, 2008	
80173 4	<input type="checkbox"/> Bituminous Materials Cost Adjustments	Nov. 2, 2006	Aug. 1, 2017
* 80426 5	<input type="checkbox"/> Bituminous Surface Treatment with Fog Seal	Jan. 1, 2020	Jan. 1, 2022
80436 6	<input type="checkbox"/> Blended Finely Divided Minerals	April 1, 2021	
80241 7	<input type="checkbox"/> Bridge Demolition Debris	July 1, 2009	
50261 8	<input type="checkbox"/> Building Removal-Case I (Non-Friable and Friable Asbestos)	Sept. 1, 1990	April 1, 2010
50481 9	<input type="checkbox"/> Building Removal-Case II (Non-Friable Asbestos)	Sept. 1, 1990	April 1, 2010
50491 10	<input type="checkbox"/> Building Removal-Case III (Friable Asbestos)	Sept. 1, 1990	April 1, 2010
50531 11	<input type="checkbox"/> Building Removal-Case IV (No Asbestos)	Sept. 1, 1990	April 1, 2010
80384 12	<input checked="" type="checkbox"/> Compensable Delay Costs	June 2, 2017	April 1, 2019
80198 13	<input type="checkbox"/> Completion Date (via calendar days)	April 1, 2008	
80199 14	<input type="checkbox"/> Completion Date (via calendar days) Plus Working Days	April 1, 2008	
80293 15	<input type="checkbox"/> Concrete Box Culverts with Skews > 30 Degrees and Design Fills ≤ 5 Feet	April 1, 2012	July 1, 2016
80311 16	<input type="checkbox"/> Concrete End Sections for Pipe Culverts	Jan. 1, 2013	April 1, 2016
80261 17	<input checked="" type="checkbox"/> Construction Air Quality – Diesel Retrofit	June 1, 2010	Nov. 1, 2014
80434 18	<input type="checkbox"/> Corrugated Plastic Pipe (Culvert and Storm Sewer)	Jan. 1, 2021	
80029 19	<input type="checkbox"/> Disadvantaged Business Enterprise Participation	Sept. 1, 2000	March 2, 2019
80229 20	<input type="checkbox"/> Fuel Cost Adjustment	April 1, 2009	Aug. 1, 2017
* 80433 21	<input type="checkbox"/> Green Preformed Thermoplastic Pavement Markings	Jan. 1, 2021	Jan. 1, 2022
* 80422 22	<input type="checkbox"/> High Tension Cable Median Barrier	Jan. 1, 2020	Jan. 1, 2022
* 80442 23	<input type="checkbox"/> Hot-Mix Asphalt – Start of Production	Jan. 1, 2022	
* 80438 24	<input type="checkbox"/> Illinois Works Apprenticeship Initiative – State Funded Contracts	June 2, 2021	Sept. 2, 2021
* 80411 25	<input type="checkbox"/> Luminaires, LED	April 1, 2019	Jan. 1, 2022
* 80045 26	<input type="checkbox"/> Material Transfer Device	June 15, 1999	Jan. 1, 2022
80418 27	<input type="checkbox"/> Mechanically Stabilized Earth Retaining Walls	Nov. 1, 2019	Nov. 1, 2020
* 80441 28	<input type="checkbox"/> Performance Graded Asphalt Binder	Jan. 1, 2022	
80430 29	<input type="checkbox"/> Portland Cement Concrete – Haul Time	July 1, 2020	
* 34261 30	<input type="checkbox"/> Railroad Protective Liability Insurance	Dec. 1, 1986	Jan. 1, 2022
80395 31	<input type="checkbox"/> Sloped Metal End Section for Pipe Culverts	Jan. 1, 2018	
* 80340 32	<input type="checkbox"/> Speed Display Trailer	April 2, 2014	Jan. 1, 2022
* 80127 33	<input type="checkbox"/> Steel Cost Adjustment	April 2, 2004	Jan. 1, 2022
80397 34	<input type="checkbox"/> Subcontractor and DBE Payment Reporting	April 2, 2018	
80391 35	<input checked="" type="checkbox"/> Subcontractor Mobilization Payments	Nov. 2, 2017	April 1, 2019
80437 36	<input type="checkbox"/> Submission of Payroll Records	April 1, 2021	
* 80435 37	<input type="checkbox"/> Surface Testing of Pavements – IRI	Jan. 1, 2021	Jan. 1, 2022
80410 38	<input type="checkbox"/> Traffic Spotters	Jan. 1, 2019	
* 20338 39	<input type="checkbox"/> Training Special Provisions	Oct. 15, 1975	Sept. 2, 2021
80318 40	<input type="checkbox"/> Traversable Pipe Grate for Concrete End Sections	Jan. 1, 2013	Jan. 1, 2018
* 80429 41	<input type="checkbox"/> Ultra-Thin Bonded Wearing Course	April 1, 2020	Jan. 1, 2022
80439 42	<input checked="" type="checkbox"/> Vehicle and Equipment Warning Lights	Nov. 1, 2021	
80440 43	<input type="checkbox"/> Waterproofing Membrane System	Nov. 1, 2021	
80302 44	<input type="checkbox"/> Weekly DBE Trucking Reports	June 2, 2012	Nov. 1, 2021
80427 45	<input checked="" type="checkbox"/> Work Zone Traffic Control Devices	Mar. 2, 2020	
80071 46	<input type="checkbox"/> Working Days	Jan. 1, 2002	

The following special provisions are in the 2022 Standard Specifications and Recurring Special Provisions.

<u>File Name</u>	<u>Special Provision Title</u>	<u>New Location(s)</u>	<u>Effective</u>	<u>Revised</u>
80425	Cape Seal	Sections 405, 1003	Jan. 1, 2020	Jan. 1, 2021
80387	Contrast Preformed Plastic Pavement Marking	Articles 780.08, 1095.03	Nov. 1, 2017	
80402	Disposal Fees	Article 109.04(b)	Nov. 1, 2018	
80378	Dowel Bar Inserter	Articles 420.03, 420.05, 1103.20	Jan. 1, 2017	Jan. 1, 2018
80421	Electric Service Installation	Articles 804.04, 804.05	Jan. 1, 2020	
80415	Emulsified Asphalts	Article 1032.06	Aug. 1, 2019	
80423	Engineer's Field Office and Laboratory	Section 670	Jan. 1, 2020	
80417	Geotechnical Fabric for Pipe Underdrains and French Drains	Articles 1080.01(a), 1080.05	Nov. 1, 2019	
80420	Geotextile Retaining Walls	Article 1080.06(d)	Nov. 1, 2019	
80304	Grooving for Recessed Pavement Markings	Articles 780.05, 780.14, 780.15	Nov. 1, 2012	Nov. 1, 2020
80416	Hot-Mix Asphalt – Binder and Surface Course	Sections 406, 1003, 1004, 1030, 1101	July 2, 2019	Nov. 1, 2019
80398	Hot-Mix Asphalt – Longitudinal Joint Sealant	Sections 406, 1032	Aug. 1, 2018	Nov. 1, 2019
80406	Hot-Mix Asphalt – Mixture Design Verification and Production (Modified for I-FIT)	Sections 406, 1030	Jan. 1, 2019	Jan. 2, 2021
80347	Hot-Mix Asphalt – Pay for Performance Using Percent Within Limits – Jobsite Sampling	Sections 406, 1030	Nov. 1, 2014	July 2, 2019
80383	Hot-Mix Asphalt – Quality Control for Performance	Sections 406, 1030	April 1, 2017	July 2, 2019
80393	Manholes, Valve Vaults, and Flat Slab Tops	Articles 602.02, 1042.10	Jan. 1, 2018	Mar. 1, 2019
80424	Micro-Surfacing and Slurry Sealing	Sections 404, 1003	Jan. 1, 2020	Jan. 1, 2021
80428	Mobilization	Article 671.02	April 1, 2020	
80412	Obstruction Warning Luminaires, LED	Sections 801, 822, 1067	Aug. 1, 2019	
80359	Portland Cement Concrete Bridge Deck Curing	Articles 1020.13, 1022.03	April 1, 2015	Nov. 1, 2019
80431	Portland Cement Concrete Pavement Patching	Articles 701.17(e)(3)b, 1001.01(d), 1020.05(b)(5)	July 1, 2020	
80432	Portland Cement Concrete Pavement Placement	Article 420.07	July 1, 2020	
80300	Preformed Plastic Pavement Marking Type D - Inlaid	Articles 780.08, 1095.03	April 1, 2012	April 1, 2016
80157	Railroad Protective Liability Insurance (5 and 10)	Article 107.11	Jan. 1, 2006	
80306	Reclaimed Asphalt Pavement (RAP) and Reclaimed Asphalt Shingles (RAS)	Section 1031	Nov. 1, 2012	Jan. 2, 2021
80407	Removal and Disposal of Regulated Substances	Section 669	Jan. 1 2019	Jan. 1, 2020
80419	Silt Fence, Inlet Filters, Ground Stabilization and Riprap Filter Fabric	Articles 280.02, 280.04, 1080.02, 1080.03, 1081.15	Nov. 1, 2019	July 1, 2021
80408	Steel Plate Beam Guardrail Manufacturing	Article 1006.25	Jan. 1, 2019	
80413	Structural Timber	Article 1007.03	Aug. 1, 2019	
80298	Temporary Pavement Marking	Section 703, Article 1095.06	April 1, 2012	April 1, 2017
80409	Traffic Control Devices – Cones	Article 701.15(a), 1106.02(b)	Jan. 1, 2019	
80288	Warm Mix Asphalt	Sections 406, 1030, 1102	Jan. 1, 2012	April 1, 2016
80414	Wood Fence Sight Screen	Article 641.02	Aug. 1, 2019	April 1, 2020

The following special provisions require additional information from the designer. The additional information needs to be submitted as a separate document. The Project Coordination and Implementation section will then include the information in the applicable special provision.

- Bridge Demolition Debris
- Building Removal - Case I
- Building Removal – Case II
- Building Removal - Case III
- Building Removal-Case IV
- Completion Date
- Completion Date Plus Working Days
- DBE Participation
- Railroad Protective Liability Insurance
- Training Special Provisions
- Working Days

CONSTRUCTION AIR QUALITY – DIESEL RETROFIT (BDE)

Effective: June 1, 2010

Revised: November 1, 2014

The reduction of emissions of particulate matter (PM) for off-road equipment shall be accomplished by installing retrofit emission control devices. The term "equipment" refers to diesel fuel powered devices rated at 50 hp and above, to be used on the jobsite in excess of seven calendar days over the course of the construction period on the jobsite (including rental equipment).

Contractor and subcontractor diesel powered off-road equipment assigned to the contract shall be retrofitted using the phased in approach shown below. Equipment that is of a model year older than the year given for that equipment's respective horsepower range shall be retrofitted:

Effective Dates	Horsepower Range	Model Year
June 1, 2010 ^{1/}	600-749	2002
	750 and up	2006
June 1, 2011 ^{2/}	100-299	2003
	300-599	2001
	600-749	2002
	750 and up	2006
June 1, 2012 ^{2/}	50-99	2004
	100-299	2003
	300-599	2001
	600-749	2002
	750 and up	2006

1/ Effective dates apply to Contractor diesel powered off-road equipment assigned to the contract.

2/ Effective dates apply to Contractor and subcontractor diesel powered off-road equipment assigned to the contract.

The retrofit emission control devices shall achieve a minimum PM emission reduction of 50 percent and shall be:

- a) Included on the U.S. Environmental Protection Agency (USEPA) *Verified Retrofit Technology List* (<http://www.epa.gov/cleandiesel/verification/verif-list.htm>), or verified by the California Air Resources Board (CARB) (<http://www.arb.ca.gov/diesel/verdev/vt/cvt.htm>); or
- b) Retrofitted with a non-verified diesel retrofit emission control device if verified retrofit emission control devices are not available for equipment proposed to be used on the project, and if the Contractor has obtained a performance certification from the retrofit

device manufacturer that the emission control device provides a minimum PM emission reduction of 50 percent.

Note: Large cranes (Crawler mounted cranes) which are responsible for critical lift operations are exempt from installing retrofit emission control devices if such devices adversely affect equipment operation.

Diesel powered off-road equipment with engine ratings of 50 hp and above, which are unable to be retrofitted with verified emission control devices or if performance certifications are not available which will achieve a minimum 50 percent PM reduction, may be granted a waiver by the Department if documentation is provided showing good faith efforts were made by the Contractor to retrofit the equipment.

Construction shall not proceed until the Contractor submits a certified list of the diesel powered off-road equipment that will be used, and as necessary, retrofitted with emission control devices. The list(s) shall include (1) the equipment number, type, make, Contractor/rental company name; and (2) the emission control devices make, model, USEPA or CARB verification number, or performance certification from the retrofit device manufacturer. Equipment reported as fitted with emissions control devices shall be made available to the Engineer for visual inspection of the device installation, prior to being used on the jobsite.

The Contractor shall submit an updated list of retrofitted off-road construction equipment as retrofitted equipment changes or comes on to the jobsite. The addition or deletion of any diesel powered equipment shall be included on the updated list.

If any diesel powered off-road equipment is found to be in non-compliance with any portion of this special provision, the Engineer will issue the Contractor a diesel retrofit deficiency deduction.

Any costs associated with retrofitting any diesel powered off-road equipment with emission control devices shall be considered as included in the contract unit prices bid for the various items of work involved and no additional compensation will be allowed. The Contractor's compliance with this notice and any associated regulations shall not be grounds for a claim.

Diesel Retrofit Deficiency Deduction

When the Engineer determines that a diesel retrofit deficiency exists, a daily monetary deduction will be imposed for each calendar day or fraction thereof the deficiency continues to exist. The calendar day(s) will begin when the time period for correction is exceeded and end with the Engineer's written acceptance of the correction. The daily monetary deduction will be \$1,000.00 for each deficiency identified.

The deficiency will be based on lack of diesel retrofit emissions control.

If a Contractor accumulates three diesel retrofit deficiency deductions for the same piece of equipment in a contract period; the Contractor will be shutdown until the deficiency is corrected.

Such a shutdown will not be grounds for any extension of the contract time, waiver of penalties, or be grounds for any claim.

80261

AGGREGATE SUBGRADE IMPROVEMENT (D1)

Effective: February 22, 2012

Revised: December 1, 2021

Add the following Section to the Standard Specifications:

"SECTION 303. AGGREGATE SUBGRADE IMPROVEMENT

303.01 Description. This work shall consist of constructing an aggregate subgrade improvement.

303.02 Materials. Materials shall be according to the following.

Item	Article/Section
(a) Coarse Aggregate	1004.07
(b) Reclaimed Asphalt Pavement (RAP) (Notes 1, 2 and 3)	1031

Note 1. Crushed RAP, from either full depth or single lift removal, may be mechanically blended with aggregate gradation CS 01 but shall not exceed 40 percent by weight of the total product. The top size of the Coarse RAP shall be less than 4 in. (100 mm) and well graded.

Note 2. RAP having 100 percent passing the 1 1/2 in (37.5 mm) sieve and being well graded, may be used as capping aggregate in the top 3 in. (75 mm) when aggregate gradation CS 01 is used in lower lifts. When RAP is blended with any of the coarse aggregates, the blending shall be done with mechanically calibrated feeders. The final product shall not contain more than 40 percent by weight of RAP.

Note 3. The RAP used for aggregate subgrade improvement shall be according to the current Bureau of Materials Policy Memorandum, "Reclaimed Asphalt Pavement (RAP) for Aggregate Applications".

303.03 Equipment. The vibratory machine shall be according to Article 1101.01, or as approved by the Engineer. The calibration for the mechanical feeders shall have an accuracy of ± 2.0 percent of the actual quantity of material delivered.

303.04 Soil Preparation. The stability of the soil shall be according to the Department's Subgrade Stability Manual for the aggregate thickness specified.

303.05 Placing Aggregate. The maximum nominal lift thickness of aggregate gradation CS 01 shall be 24 in. (600 mm).

303.06 Capping Aggregate. The top surface of the aggregate subgrade shall consist of a minimum 3 in. (75 mm) of aggregate gradations CA 06 or CA 10. When Reclaimed Asphalt Pavement (RAP) is used, it shall be crushed and screened where 100 percent is passing the 1 1/2 in. (37.5 mm) sieve and being well graded. RAP that has been fractionated to size will not be permitted for use in capping. Capping aggregate will not be required when the aggregate subgrade improvement is used as a cubic yard pay item for undercut applications. When RAP is

blended with any of the coarse aggregates, the blending shall be done with mechanically calibrated feeders.

303.07 Compaction. All aggregate lifts shall be compacted to the satisfaction of the Engineer. If the moisture content of the material is such that compaction cannot be obtained, sufficient water shall be added so that satisfactory compaction can be obtained.

303.08 Finishing and Maintenance of Aggregate Subgrade Improvement. The aggregate subgrade improvement shall be finished to the lines, grades, and cross sections shown on the plans, or as directed by the Engineer. The aggregate subgrade improvement shall be maintained in a smooth and compacted condition.

303.09 Method of Measurement. This work will be measured for payment according to Article 311.08.

303.10 Basis of Payment. This work will be paid for at the contract unit price per cubic yard (cubic meter) for AGGREGATE SUBGRADE IMPROVEMENT or at the contract unit price per square yard (square meter) for AGGREGATE SUBGRADE IMPROVEMENT, of the thickness specified.

Add the following to Section 1004 of the Standard Specifications:

"1004.07 Coarse Aggregate for Aggregate Subgrade Improvement. The aggregate shall be according to Article 1004.01 and the following.

- (a) Description. The coarse aggregate shall be crushed gravel, crushed stone, or crushed concrete. The top 12 inches of the aggregate subgrade improvement shall be 3 inches of capping material and 9 inches of crushed gravel, crushed stone or crushed concrete. In applications where greater than 36 inches of subgrade material is required, rounded gravel, meeting the CS01 gradation, may be used beginning at a depth of 12 inches below the bottom of pavement.
- (b) Quality. The coarse aggregate shall consist of sound durable particles reasonably free of deleterious materials. Non-mechanically blended RAP may be allowed up to a maximum of 5.0 percent.
- (c) Gradation.
- (1) The coarse aggregate gradation for total subgrade thicknesses of 12 in. (300 mm) or greater shall be CS 01.

Grad No.	COARSE AGGREGATE SUBGRADE GRADATIONS				
	Sieve Size and Percent Passing				
	8"	6"	4"	2"	#4
CS 01	100	97 ± 3	90 ± 10	45 ± 25	20 ± 20

COARSE AGGREGATE SUBGRADE GRADATIONS (Metric)					
Grad No.	Sieve Size and Percent Passing				
	200 mm	150 mm	100 mm	50 mm	4.75 mm
CS 01	100	97 ± 3	90 ± 10	45 ± 25	20 ± 20

(2) The 3 in. (75 mm) capping aggregate shall be gradation CA 6 or CA 10.

COMPENSABLE DELAY COSTS (BDE)

Effective: June 2, 2017

Revised: April 1, 2019

Revise Article 107.40(b) of the Standard Specifications to read:

"(b) Compensation. Compensation will not be allowed for delays, inconveniences, or damages sustained by the Contractor from conflicts with facilities not meeting the above definition; or if a conflict with a utility in an unanticipated location does not cause a shutdown of the work or a documentable reduction in the rate of progress exceeding the limits set herein. The provisions of Article 104.03 notwithstanding, compensation for delays caused by a utility in an unanticipated location will be paid according to the provisions of this Article governing minor and major delays or reduced rate of production which are defined as follows.

- (1) Minor Delay. A minor delay occurs when the work in conflict with the utility in an unanticipated location is completely stopped for more than two hours, but not to exceed two weeks.
- (2) Major Delay. A major delay occurs when the work in conflict with the utility in an unanticipated location is completely stopped for more than two weeks.
- (3) Reduced Rate of Production Delay. A reduced rate of production delay occurs when the rate of production on the work in conflict with the utility in an unanticipated location decreases by more than 25 percent and lasts longer than seven calendar days."

Revise Article 107.40(c) of the Standard Specifications to read:

"(c) Payment. Payment for Minor, Major, and Reduced Rate of Production Delays will be made as follows.

- (1) Minor Delay. Labor idled which cannot be used on other work will be paid for according to Article 109.04(b)(1) and (2) for the time between start of the delay and the minimum remaining hours in the work shift required by the prevailing practice in the area.

Equipment idled which cannot be used on other work, and which is authorized to standby on the project site by the Engineer, will be paid for according to Article 109.04(b)(4).

- (2) Major Delay. Labor will be the same as for a minor delay.

Equipment will be the same as for a minor delay, except Contractor-owned equipment will be limited to two weeks plus the cost of move-out to either the

Contractor's yard or another job and the cost to re-mobilize, whichever is less. Rental equipment may be paid for longer than two weeks provided the Contractor presents adequate support to the Department (including lease agreement) to show retaining equipment on the job is the most economical course to follow and in the public interest.

- (3) Reduced Rate of Production Delay. The Contractor will be compensated for the reduced productivity for labor and equipment time in excess of the 25 percent threshold for that portion of the delay in excess of seven calendar days. Determination of compensation will be in accordance with Article 104.02, except labor and material additives will not be permitted.

Payment for escalated material costs, escalated labor costs, extended project overhead, and extended traffic control will be determined according to Article 109.13."

Revise Article 108.04(b) of the Standard Specifications to read:

"(b) No working day will be charged under the following conditions.

- (1) When adverse weather prevents work on the controlling item.
- (2) When job conditions due to recent weather prevent work on the controlling item.
- (3) When conduct or lack of conduct by the Department or its consultants, representatives, officers, agents, or employees; delay by the Department in making the site available; or delay in furnishing any items required to be furnished to the Contractor by the Department prevents work on the controlling item.
- (4) When delays caused by utility or railroad adjustments prevent work on the controlling item.
- (5) When strikes, lock-outs, extraordinary delays in transportation, or inability to procure critical materials prevent work on the controlling item, as long as these delays are not due to any fault of the Contractor.
- (6) When any condition over which the Contractor has no control prevents work on the controlling item."

Revise Article 109.09(f) of the Standard Specifications to read:

- "(f) Basis of Payment. After resolution of a claim in favor of the Contractor, any adjustment in time required for the work will be made according to Section 108. Any adjustment in the costs to be paid will be made for direct labor, direct materials, direct equipment, direct jobsite overhead, direct offsite overhead, and other direct costs allowed by the resolution. Adjustments in costs will not be made for interest charges, loss of anticipated profit, undocumented loss of efficiency, home office overhead and unabsorbed overhead

other than as allowed by Article 109.13, lost opportunity, preparation of claim expenses and other consequential indirect costs regardless of method of calculation.

The above Basis of Payment is an essential element of the contract and the claim cost recovery of the Contractor shall be so limited."

Add the following to Section 109 of the Standard Specifications.

"109.13 Payment for Contract Delay. Compensation for escalated material costs, escalated labor costs, extended project overhead, and extended traffic control will be allowed when such costs result from a delay meeting the criteria in the following table.

Contract Type	Cause of Delay	Length of Delay
Working Days	Article 108.04(b)(3) or Article 108.04(b)(4)	No working days have been charged for two consecutive weeks.
Completion Date	Article 108.08(b)(1) or Article 108.08(b)(7)	The Contractor has been granted a minimum two week extension of contract time, according to Article 108.08.

Payment for each of the various costs will be according to the following.

- (a) Escalated Material and/or Labor Costs. When the delay causes work, which would have otherwise been completed, to be done after material and/or labor costs have increased, such increases will be paid. Payment for escalated material costs will be limited to the increased costs substantiated by documentation furnished by the Contractor. Payment for escalated labor costs will be limited to those items in Article 109.04(b)(1) and (2), except the 35 percent and 10 percent additives will not be permitted.
- (b) Extended Project Overhead. For the duration of the delay, payment for extended project overhead will be paid as follows.
- (1) Direct Jobsite and Offsite Overhead. Payment for documented direct jobsite overhead and documented direct offsite overhead, including onsite supervisory and administrative personnel, will be allowed according to the following table.

Original Contract Amount	Supervisory and Administrative Personnel
Up to \$5,000,000	One Project Superintendent
Over \$ 5,000,000 - up to \$25,000,000	One Project Manager, One Project Superintendent or Engineer, and One Clerk
Over \$25,000,000 - up to \$50,000,000	One Project Manager, One Project Superintendent, One Engineer, and

	One Clerk
Over \$50,000,000	One Project Manager, Two Project Superintendents, One Engineer, and One Clerk

- (2) Home Office and Unabsorbed Overhead. Payment for home office and unabsorbed overhead will be calculated as 8 percent of the total delay cost.
- (c) Extended Traffic Control. Traffic control required for an extended period of time due to the delay will be paid for according to Article 109.04.

When an extended traffic control adjustment is paid under this provision, an adjusted unit price as provided for in Article 701.20(a) for increase or decrease in the value of work by more than ten percent will not be paid.

Upon payment for a contract delay under this provision, the Contractor shall assign subrogation rights to the Department for the Department's efforts of recovery from any other party for monies paid by the Department as a result of any claim under this provision. The Contractor shall fully cooperate with the Department in its efforts to recover from another party any money paid to the Contractor for delay damages under this provision."

80384

SUBCONTRACTOR MOBILIZATION PAYMENTS (BDE)

Effective: November 2, 2017

Revised: April 1, 2019

Replace the second paragraph of Article 109.12 of the Standard Specifications with the following:

"This mobilization payment shall be made at least seven days prior to the subcontractor starting work. The amount paid shall be at the following percentage of the amount of the subcontract reported on form BC 260A submitted for the approval of the subcontractor's work.

Value of Subcontract Reported on Form BC 260A	Mobilization Percentage
Less than \$10,000	25%
\$10,000 to less than \$20,000	20%
\$20,000 to less than \$40,000	18%
\$40,000 to less than \$60,000	16%
\$60,000 to less than \$80,000	14%
\$80,000 to less than \$100,000	12%
\$100,000 to less than \$250,000	10%
\$250,000 to less than \$500,000	9%
\$500,000 to \$750,000	8%
Over \$750,000	7%"

80391

TRAFFIC CONTROL DEVICES - CONES (BDE)

Effective: January 1, 2019

Revise Article 701.15(a) of the Standard Specifications to read:

“(a) Cones. Cones are used to channelize traffic. Cones used to channelize traffic at night shall be reflectorized; however, cones shall not be used in nighttime lane closure tapers or nighttime lane shifts.”

Revise Article 1106.02(b) of the Standard Specifications to read:

“(b) Cones. Cones shall be predominantly orange. Cones used at night that are 28 to 36 in. (700 to 900 mm) in height shall have two white circumferential stripes. If non-reflective spaces are left between the stripes, the spaces shall be no more than 2 in. (50mm) in width. Cones used at night that are taller than 36 in. (900 mm) shall have a minimum of two white and two fluorescent orange alternating, circumferential stripes with the top stripe being fluorescent orange. If non-reflective spaces are left between the stripes, the spaces shall be no more than 3 in. (75 mm) in width.

The minimum weights for the various cone heights shall be 4 lb for 18 in. (2 kg for 450 mm), 7 lb for 28 in. (3 kg for 700 mm), and 10 lb for 36 in. (5 kg for 900 mm) with a minimum of 60 percent of the total weight in the base. Cones taller than 36 in. shall be weighted per the manufacturer's specifications such that they are not moved by wind or passing traffic.”

80409

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
PAVEMENT RESURFACING	
Leveling Binder (Machine Method), N50 (IL-9.5 mm)	4% @ 50 Gyr.
Hot-Mix Asphalt Surface Course, Mix "D", N50 (IL-9.5 mm)	3.5% @ 50 Gyr.
PATCHING	
Class D Patches (HMA Binder IL-19 mm)	4% @ 70 Gyr.
Pavement Removal & HMA Replacement (HMA Binder IL-19 mm)	4% @ 70 Gyr.
DRIVEWAYS	
Hot-Mix Asphalt Surface Course, Mix "D", N50 (IL-9.5 mm), 3"	3.5% @ 50 Gyr.
Hot-Mix Asphalt Base Course (HMA Binder IL-19 mm), 6"	4% @ 50 Gyr.

The unit weight used to calculate all Hot-Mix Asphalt Surface Mixture Quantities is 112 Lbs/SqYd/In.

The "AC Type" for polymerized HMA mixes shall be SBS/SBR PG 76-22 and for non-polymerized HMA the "AC Type" shall be "PG 64-22" unless modified by district one special provisions. For use of recycled materials see special provisions.

DuPage County Prevailing Wage Rates posted on 1/6/2022

Trade Title	Rg	Type	C	Base	Foreman	Overtime				H/W	Pension	Vac	Trng	Other Ins
						M-F	Sa	Su	Hol					
ASBESTOS ABT-GEN	All	ALL		45.90	46.90	1.5	1.5	2.0	2.0	16.55	14.71	0.00	0.90	
ASBESTOS ABT-MEC	All	BLD		38.85	41.96	1.5	1.5	2.0	2.0	14.42	12.61	0.00	0.82	
BOILERMAKER	All	BLD		52.61	57.34	2.0	2.0	2.0	2.0	6.97	22.34	0.00	1.40	
BRICK MASON	All	BLD		48.56	53.42	1.5	1.5	2.0	2.0	11.70	21.06	0.00	1.03	
CARPENTER	All	ALL		50.86	52.86	1.5	1.5	2.0	2.0	11.79	24.76	0.00	0.79	
CEMENT MASON	All	ALL		47.50	49.50	2.0	1.5	2.0	2.0	16.75	20.74	0.00	1.00	
CERAMIC TILE FINISHER	All	BLD		42.80	42.80	1.5	1.5	2.0	2.0	11.45	14.27	0.00	0.94	
COMMUNICATION TECHNICIAN	All	BLD		34.71	37.51	1.5	1.5	2.0	2.0	12.85	23.75	3.20	0.68	0.10
ELECTRIC PWR EQMT OP	All	ALL		46.06	62.84	1.5	1.5	2.0	2.0	6.75	12.90	0.00	1.15	1.38
ELECTRIC PWR GRNDMAN	All	ALL		35.38	62.84	1.5	1.5	2.0	2.0	6.75	9.91	0.00	0.88	1.06
ELECTRIC PWR LINEMAN	All	ALL		55.37	62.84	1.5	1.5	2.0	2.0	6.75	15.50	0.00	1.38	1.66
ELECTRIC PWR TRK DRV	All	ALL		36.67	62.84	1.5	1.5	2.0	2.0	6.75	10.27	0.00	0.92	1.10
ELECTRICIAN	All	BLD		41.83	46.08	1.5	1.5	2.0	2.0	12.85	27.00	6.85	0.85	0.10
ELEVATOR CONSTRUCTOR	All	BLD		60.42	67.97	2.0	2.0	2.0	2.0	15.87	19.31	4.83	0.64	
FENCE ERECTOR	NE	ALL		45.67	47.67	1.5	1.5	2.0	2.0	13.68	16.39	0.00	0.65	
FENCE ERECTOR	W	ALL		48.83	52.74	2.0	2.0	2.0	2.0	13.31	25.25	0.00	1.28	
GLAZIER	All	BLD		47.73	49.23	1.5	2.0	2.0	2.0	14.99	23.42	0.00	1.43	
HEAT/FROST INSULATOR	All	BLD		51.80	54.91	1.5	1.5	2.0	2.0	14.42	15.36	0.00	0.82	
IRON WORKER	E	ALL		54.51	56.51	2.0	2.0	2.0	2.0	15.40	25.06	0.00	0.44	
IRON WORKER	W	ALL		48.83	52.74	2.0	2.0	2.0	2.0	13.31	25.25	0.00	1.28	
LABORER	All	ALL		45.90	46.65	1.5	1.5	2.0	2.0	16.55	14.71	0.00	0.90	
LATHER	All	ALL		50.86	52.86	1.5	1.5	2.0	2.0	11.79	24.76	0.00	0.79	
MACHINIST	All	BLD		50.68	53.18	1.5	1.5	2.0	2.0	8.93	8.95	1.85	1.47	
MARBLE FINISHER	All	ALL		37.00	50.10	1.5	1.5	2.0	2.0	11.70	19.10	0.00	0.93	
MARBLE MASON	All	BLD		47.71	52.48	1.5	1.5	2.0	2.0	11.70	20.53	0.00	1.02	
MATERIAL TESTER I	All	ALL		35.90		1.5	1.5	2.0	2.0	16.55	14.71	0.00	0.90	
MATERIALS TESTER II	All	ALL		40.90		1.5	1.5	2.0	2.0	16.55	14.71	0.00	0.90	
MILLWRIGHT	All	ALL		50.86	52.86	1.5	1.5	2.0	2.0	11.79	24.76	0.00	0.79	
OPERATING ENGINEER	All	BLD	1	53.60	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	BLD	2	52.30	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	BLD	3	49.75	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	

OPERATING ENGINEER	All	BLD	4	48.00	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	BLD	5	57.35	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	BLD	6	54.60	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	BLD	7	56.60	57.60	2.0	2.0	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	FLT		41.00	41.00	1.5	1.5	2.0	2.0	20.90	17.85	2.00	2.15	
OPERATING ENGINEER	All	HWY	1	51.80	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	2	51.25	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	3	49.20	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	4	47.80	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	5	46.60	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	6	54.80	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
OPERATING ENGINEER	All	HWY	7	52.80	55.80	1.5	1.5	2.0	2.0	21.40	18.60	2.00	2.40	
ORNAMENTAL IRON WORKER	E	ALL		52.13	54.63	2.0	2.0	2.0	2.0	14.23	23.99	0.00	1.25	
ORNAMENTAL IRON WORKER	W	ALL		48.83	52.74	2.0	2.0	2.0	2.0	13.31	25.25	0.00	1.28	
PAINTER	All	ALL		49.30	51.30	1.5	1.5	1.5	2.0	19.08	4.15	0.00	1.10	
PAINTER - SIGNS	All	BLD		40.74	45.75	1.5	1.5	2.0	2.0	3.04	3.90	0.00	0.00	
PILEDRIIVER	All	ALL		50.86	52.86	1.5	1.5	2.0	2.0	11.79	24.76	0.00	0.79	
PIPEFITTER	All	BLD		52.00	55.00	1.5	1.5	2.0	2.0	11.60	21.85	0.00	2.92	
PLASTERER	All	BLD		48.60	51.52	1.5	1.5	2.0	2.0	11.70	20.98	0.00	1.02	
PLUMBER	All	BLD		52.80	55.95	1.5	1.5	2.0	2.0	16.45	16.75	0.00	1.47	
ROOFER	All	BLD		46.70	50.70	1.5	1.5	2.0	2.0	11.58	14.56	0.00	0.96	
SHEETMETAL WORKER	All	BLD		51.83	54.42	1.5	1.5	2.0	2.0	11.22	19.08	0.00	1.45	2.46
SPRINKLER FITTER	All	BLD		52.25	55.00	1.5	1.5	2.0	2.0	14.20	18.60	0.00	0.75	
STEEL ERECTOR	E	ALL		54.51	56.51	2.0	2.0	2.0	2.0	15.40	25.06	0.00	0.44	
STEEL ERECTOR	W	ALL		48.83	52.74	2.0	2.0	2.0	2.0	13.31	25.25	0.00	1.28	
STONE MASON	All	BLD		48.56	53.42	1.5	1.5	2.0	2.0	11.70	21.06	0.00	1.03	
TERRAZZO FINISHER	All	BLD		44.54	44.54	1.5	1.5	2.0	2.0	11.45	16.64	0.00	0.97	
TERRAZZO MASON	All	BLD		48.38	51.88	1.5	1.5	2.0	2.0	11.45	18.10	0.00	1.00	
TILE MASON	All	BLD		49.75	53.75	1.5	1.5	2.0	2.0	11.45	17.98	0.00	1.02	
TRAFFIC SAFETY WORKER	All	HWY		38.50	40.10	1.5	1.5	2.0	2.0	8.90	8.90	0.00	0.90	
TRUCK DRIVER	All	ALL	1	40.06	40.61	1.5	1.5	2.0	2.0	10.15	13.57	0.00	0.15	
TRUCK DRIVER	All	ALL	2	40.21	40.61	1.5	1.5	2.0	2.0	10.15	13.57	0.00	0.15	
TRUCK DRIVER	All	ALL	3	40.41	40.61	1.5	1.5	2.0	2.0	10.15	13.57	0.00	0.15	
TRUCK DRIVER	All	ALL	4	40.61	40.61	1.5	1.5	2.0	2.0	10.15	13.57	0.00	0.15	
TUCK POINTER	All	BLD		48.25	49.25	1.5	1.5	2.0	2.0	8.79	20.47	0.00	1.01	

Legend**Rg** Region**Type** Trade Type - All, Highway, Building, Floating, Oil & Chip, Rivers**C** Class**Base** Base Wage Rate**OT M-F** Unless otherwise noted, OT pay is required for any hour greater than 8 worked each day, Mon through Fri. The number listed is the multiple of the base wage.**OT Sa** Overtime pay required for every hour worked on Saturdays**OT Su** Overtime pay required for every hour worked on Sundays**OT Hol** Overtime pay required for every hour worked on Holidays**H/W** Health/Welfare benefit**Vac** Vacation**Trng** Training**Other Ins** Employer hourly cost for any other type(s) of insurance provided for benefit of worker.

Explanations DUPAGE COUNTY

IRON WORKERS AND FENCE ERECTOR (WEST) - West of Route 53.

The following list is considered as those days for which holiday rates of wages for work performed apply: New Years Day, Memorial Day, Fourth of July, Labor Day, Thanksgiving Day, Christmas Day and Veterans Day in some classifications/counties. Generally, any of these holidays which fall on a Sunday is celebrated on the following Monday. This then makes work performed on that Monday payable at the appropriate overtime rate for holiday pay. Common practice in a given local may alter certain days of celebration. If in doubt, please check with IDOL.

EXPLANATION OF CLASSES

ASBESTOS - GENERAL - removal of asbestos material/mold and hazardous materials from any place in a building, including mechanical systems where those mechanical systems are to be removed. This includes the removal of asbestos materials/mold and hazardous materials from ductwork or pipes in a building when the building is to be demolished at the time or at some close future date.

ASBESTOS - MECHANICAL - removal of asbestos material from mechanical systems, such as pipes, ducts, and boilers, where the mechanical systems are to remain.

TRAFFIC SAFETY

Effective November 30, 2018, the description of the traffic safety worker trade in this County is as follows: Work associated with barricades, horses and drums used to reduce lane usage on highway work, the installation and removal of temporary, non-temporary or permanent lane, pavement or roadway markings, and the installation and removal of temporary road signs.

CERAMIC TILE FINISHER

The grouting, cleaning, and polishing of all classes of tile, whether for interior or exterior purposes, all burned, glazed or unglazed products; all composition materials, granite tiles, warning detectable tiles, cement tiles, epoxy composite materials, pavers, glass, mosaics, fiberglass, and all substitute materials, for tile made in tile-like units; all mixtures in tile like form of cement, metals, and other materials that are for and intended for use as a finished floor surface, stair treads, promenade roofs, walks, walls, ceilings, swimming pools, and all other places where tile is to form a finished interior or exterior. The mixing of all setting mortars including but not limited to thin-set mortars, epoxies, wall mud, and any other sand and cement mixtures or adhesives when

used in the preparation, installation, repair, or maintenance of tile and/or similar materials. The handling and unloading of all sand, cement, lime, tile, fixtures, equipment, adhesives, or any other materials to be used in the preparation, installation, repair, or maintenance of tile and/or similar materials. Ceramic Tile Finishers shall fill all joints and voids regardless of method on all tile work, particularly and especially after installation of said tile work. Application of any and all protective coverings to all types of tile installations including, but not be limited to, all soap compounds, paper products, tapes, and all polyethylene coverings, plywood, masonite, cardboard, and any new type of products that may be used to protect tile installations, Blastrac equipment, and all floor scarifying equipment used in preparing floors to receive tile. The clean up and removal of all waste and materials. All demolition of existing tile floors and walls to be re-tiled.

COMMUNICATIONS TECHNICIAN

Low voltage installation, maintenance and removal of telecommunication facilities (voice, sound, data and video) including telephone and data inside wire, interconnect, terminal equipment, central offices, PABX, fiber optic cable and equipment, micro waves, V-SAT, bypass, CATV, WAN (wide area networks), LAN (local area networks), and ISDN (integrated system digital network), pulling of wire in raceways, but not the installation of raceways.

MARBLE FINISHER

Loading and unloading trucks, distribution of all materials (all stone, sand, etc.), stocking of floors with material, performing all rigging for heavy work, the handling of all material that may be needed for the installation of such materials, building of scaffolding, polishing if needed, patching, waxing of material if damaged, pointing up, caulking, grouting and cleaning of marble, holding water on diamond or Carborundum blade or saw for setters cutting, use of tub saw or any other saw needed for preparation of material, drilling of holes for wires that anchor material set by setters, mixing up of molding plaster for installation of material, mixing up thin set for the installation of material, mixing up of sand to cement for the installation of material and such other work as may be required in helping a Marble Setter in the handling of all material in the erection or installation of interior marble, slate, travertine, art marble, serpentine, alberene stone, blue stone, granite and other stones (meaning as to stone any foreign or domestic materials as are specified and used in building interiors and exteriors and customarily known as stone in the trade), carrara, sanionyx, vitrolite and similar opaque glass and the laying of all marble tile, terrazzo tile, slate tile and precast tile, steps, risers treads, base, or any other materials that may be used as substitutes for any of the aforementioned materials and which are used on interior and exterior which are installed in a similar manner.

MATERIAL TESTER I: Hand coring and drilling for testing of materials; field inspection of uncured concrete and asphalt.

MATERIAL TESTER II: Field inspection of welds, structural steel, fireproofing, masonry, soil, facade, reinforcing steel, formwork, cured concrete, and concrete and asphalt batch plants; adjusting proportions of bituminous mixtures.

OPERATING ENGINEER - BUILDING

Class 1. Asphalt Plant; Asphalt Spreader; Autograde; Backhoes with Caisson Attachment; Batch Plant; Benoto (requires Two Engineers); Boiler and Throttle Valve; Caisson Rigs; Central Redi-Mix Plant; Combination Back Hoe Front End-loader Machine; Compressor and Throttle Valve; Concrete Breaker (Truck Mounted); Concrete Conveyor; Concrete Conveyor (Truck Mounted); Concrete Paver Over 27E cu. ft; Concrete Paver 27E cu. ft. and Under; Concrete Placer; Concrete Placing Boom; Concrete Pump (Truck Mounted); Concrete Tower; Cranes, All; Cranes, Hammerhead; Cranes, (GCI and similar Type); Creter Crane; Spider Crane; Crusher, Stone, etc.; Derricks, All; Derricks, Traveling; Formless Curb and Gutter Machine; Grader, Elevating; Grouting Machines; Heavy Duty Self-Propelled Transporter or Prime Mover; Highlift Shovels or Front Endloader 2-1/4 yd. and over; Hoists, Elevators, outside type rack and pinion and similar machines; Hoists, One, Two and Three Drum; Hoists, Two Tugger One Floor; Hydraulic Backhoes; Hydraulic Boom Trucks; Hydro Vac (and similar equipment); Locomotives, All; Motor Patrol; Lubrication Technician; Manipulators; Pile Drivers and Skid Rig; Post Hole Digger; Pre-Stress Machine; Pump Cretes Dual Ram; Pump Cretes: Squeeze Cretes-Screw Type Pumps; Gypsum Bulker and Pump; Raised and Blind Hole Drill; Roto Mill Grinder; Scoops - Tractor Drawn; Slip-Form Paver; Straddle Buggies; Operation of Tie Back Machine; Tournapull; Tractor with Boom and Side Boom; Trenching

Machines.

Class 2. Boilers; Broom, All Power Propelled; Bulldozers; Concrete Mixer (Two Bag and Over); Conveyor, Portable; Forklift Trucks; Highlift Shovels or Front Endloaders under 2-1/4 yd.; Hoists, Automatic; Hoists, Inside Elevators; Hoists, Sewer Dragging Machine; Hoists, Tugger Single Drum; Laser Screed; Rock Drill (Self-Propelled); Rock Drill (Truck Mounted); Rollers, All; Steam Generators; Tractors, All; Tractor Drawn Vibratory Roller; Winch Trucks with "A" Frame.

Class 3. Air Compressor; Combination Small Equipment Operator; Generators; Heaters, Mechanical; Hoists, Inside Elevators (remodeling or renovation work); Hydraulic Power Units (Pile Driving, Extracting, and Drilling); Pumps, over 3" (1 to 3 not to exceed a total of 300 ft.); Low Boys; Pumps, Well Points; Welding Machines (2 through 5); Winches, 4 Small Electric Drill Winches.

Class 4. Bobcats and/or other Skid Steer Loaders; Oilers; and Brick Forklift.

Class 5. Assistant Craft Foreman.

Class 6. Gradall.

Class 7. Mechanics; Welders.

OPERATING ENGINEERS - HIGHWAY CONSTRUCTION

Class 1. Asphalt Plant; Asphalt Heater and Planer Combination; Asphalt Heater Scarfire; Asphalt Spreader; Autograder/GOMACO or other similar type machines; ABG Paver; Backhoes with Caisson Attachment; Ballast Regulator; Belt Loader; Caisson Rigs; Car Dumper; Central Redi-Mix Plant; Combination Backhoe Front Endloader Machine, (1 cu. yd. Backhoe Bucket or over or with attachments); Concrete Breaker (Truck Mounted); Concrete Conveyor; Concrete Paver over 27E cu. ft.; Concrete Placer; Concrete Tube Float; Cranes, all attachments; Cranes, Tower Cranes of all types: Creter Crane; Spider Crane; Crusher, Stone, etc.; Derricks, All; Derrick Boats; Derricks, Traveling; Dredges; Elevators, Outside type Rack & Pinion and Similar Machines; Formless Curb and Gutter Machine; Grader, Elevating; Grader, Motor Grader, Motor Patrol, Auto Patrol, Form Grader, Pull Grader, Subgrader; Guard Rail Post Driver Truck Mounted; Hoists, One, Two and Three Drum; Heavy Duty Self-Propelled Transporter or Prime Mover; Hydraulic Backhoes; Backhoes with shear attachments up to 40' of boom reach; Lubrication Technician; Manipulators; Mucking Machine; Pile Drivers and Skid Rig; Pre-Stress Machine; Pump Cretes Dual Ram; Rock Drill - Crawler or Skid Rig; Rock Drill - Truck Mounted; Rock/Track Tamper; Roto Mill Grinder; Slip-Form Paver; Snow Melters; Soil Test Drill Rig (Truck Mounted); Straddle Buggies; Hydraulic Telescoping Form (Tunnel); Operation of Tieback Machine; Tractor Drawn Belt Loader; Tractor Drawn Belt Loader (with attached pusher - two engineers); Tractor with Boom; Tractaire with Attachments; Traffic Barrier Transfer Machine; Trenching; Truck Mounted Concrete Pump with Boom; Raised or Blind Hole Drills (Tunnel Shaft); Underground Boring and/or Mining Machines 5 ft. in diameter and over tunnel, etc; Underground Boring and/or Mining Machines under 5 ft. in diameter; Wheel Excavator; Widener (APSCO).

Class 2. Batch Plant; Bituminous Mixer; Boiler and Throttle Valve; Bulldozers; Car Loader Trailing Conveyors; Combination Backhoe Front Endloader Machine (Less than 1 cu. yd. Backhoe Bucket or over or with attachments); Compressor and Throttle Valve; Compressor, Common Receiver (3); Concrete Breaker or Hydro Hammer; Concrete Grinding Machine; Concrete Mixer or Paver 7S Series to and including 27 cu. ft.; Concrete Spreader; Concrete Curing Machine, Burlap Machine, Belting Machine and Sealing Machine; Concrete Wheel Saw; Conveyor Muck Cars (Haglund or Similar Type); Drills, All; Finishing Machine - Concrete; Highlift Shovels or Front Endloader; Hoist - Sewer Dragging Machine; Hydraulic Boom Trucks (All Attachments); Hydro-Blaster; Hydro Excavating (excluding hose work); Laser Screed; All Locomotives, Dinky; Off-Road Hauling Units (including articulating) Non Self-Loading Ejection Dump; Pump Cretes: Squeeze Cretes - Screw Type Pumps, Gypsum Bulker and Pump; Roller, Asphalt; Rotary Snow Plows; Rototiller, Seaman, etc., self-propelled; Self-Propelled Compactor; Spreader - Chip - Stone, etc.; Scraper - Single/Twin Engine/Push and Pull; Scraper - Prime Mover in Tandem (Regardless of Size); Tractors pulling attachments, Sheeps Foot, Disc, Compactor, etc.; Tug Boats.

Class 3. Boilers; Brooms, All Power Propelled; Cement Supply Tender; Compressor, Common Receiver (2); Concrete Mixer (Two

Bag and Over); Conveyor, Portable; Farm-Type Tractors Used for Mowing, Seeding, etc.; Forklift Trucks; Grouting Machine; Hoists, Automatic; Hoists, All Elevators; Hoists, Tugger Single Drum; Jeep Diggers; Low Boys; Pipe Jacking Machines; Post-Hole Digger; Power Saw, Concrete Power Driven; Pug Mills; Rollers, other than Asphalt; Seed and Straw Blower; Steam Generators; Stump Machine; Winch Trucks with "A" Frame; Work Boats; Tamper-Form-Motor Driven.

Class 4. Air Compressor; Combination - Small Equipment Operator; Directional Boring Machine; Generators; Heaters, Mechanical; Hydraulic Power Unit (Pile Driving, Extracting, or Drilling); Light Plants, All (1 through 5); Pumps, over 3" (1 to 3 not to exceed a total of 300 ft.); Pumps, Well Points; Vacuum Trucks (excluding hose work); Welding Machines (2 through 5); Winches, 4 Small Electric Drill Winches.

Class 5. SkidSteer Loader (all); Brick Forklifts; Oilers.

Class 6. Field Mechanics and Field Welders

Class 7. Dowell Machine with Air Compressor; Gradall and machines of like nature.

OPERATING ENGINEER - FLOATING

Diver. Diver Wet Tender, Diver Tender, ROV Pilot, ROV Tender

TRUCK DRIVER - BUILDING, HEAVY AND HIGHWAY CONSTRUCTION

Class 1. Two or three Axle Trucks. A-frame Truck when used for transportation purposes; Air Compressors and Welding Machines, including those pulled by cars, pick-up trucks and tractors; Ambulances; Batch Gate Lockers; Batch Hopperman; Car and Truck Washers; Carry-alls; Fork Lifts and Hoists; Helpers; Mechanics Helpers and Greasers; Oil Distributors 2-man operation; Pavement Breakers; Pole Trailer, up to 40 feet; Power Mower Tractors; Self-propelled Chip Spreader; Skipman; Slurry Trucks, 2-man operation; Slurry Truck Conveyor Operation, 2 or 3 man; Teamsters; Unskilled Dumpman; and Truck Drivers hauling warning lights, barricades, and portable toilets on the job site.

Class 2. Four axle trucks; Dump Crets and Adgetors under 7 yards; Dumpsters, Track Trucks, Euclids, Hug Bottom Dump Turnapulls or Turnatrailers when pulling other than self-loading equipment or similar equipment under 16 cubic yards; Mixer Trucks under 7 yards; Ready-mix Plant Hopper Operator, and Winch Trucks, 2 Axles.

Class 3. Five axle trucks; Dump Crets and Adgetors 7 yards and over; Dumpsters, Track Trucks, Euclids, Hug Bottom Dump Turnatrailers or turnapulls when pulling other than self-loading equipment or similar equipment over 16 cubic yards; Explosives and/or Fission Material Trucks; Mixer Trucks 7 yards or over; Mobile Cranes while in transit; Oil Distributors, 1-man operation; Pole Trailer, over 40 feet; Pole and Expandable Trailers hauling material over 50 feet long; Slurry trucks, 1-man operation; Winch trucks, 3 axles or more; Mechanic--Truck Welder and Truck Painter.

Class 4. Six axle trucks; Dual-purpose vehicles, such as mounted crane trucks with hoist and accessories; Foreman; Master Mechanic; Self-loading equipment like P.B. and trucks with scoops on the front.

TERRAZZO FINISHER

The handling of sand, cement, marble chips, and all other materials that may be used by the Mosaic Terrazzo Mechanic, and the mixing, grinding, grouting, cleaning and sealing of all Marble, Mosaic, and Terrazzo work, floors, base, stairs, and wainscoting by hand or machine, and in addition, assisting and aiding Marble, Masonic, and Terrazzo Mechanics.

Other Classifications of Work:

For definitions of classifications not otherwise set out, the Department generally has on file such definitions which are available. If a task to be performed is not subject to one of the classifications of pay set out, the Department will upon being contacted state

which neighboring county has such a classification and provide such rate, such rate being deemed to exist by reference in this document. If no neighboring county rate applies to the task, the Department shall undertake a special determination, such special determination being then deemed to have existed under this determination. If a project requires these, or any classification not listed, please contact IDOL at 217-782-1710 for wage rates or clarifications.

LANDSCAPING

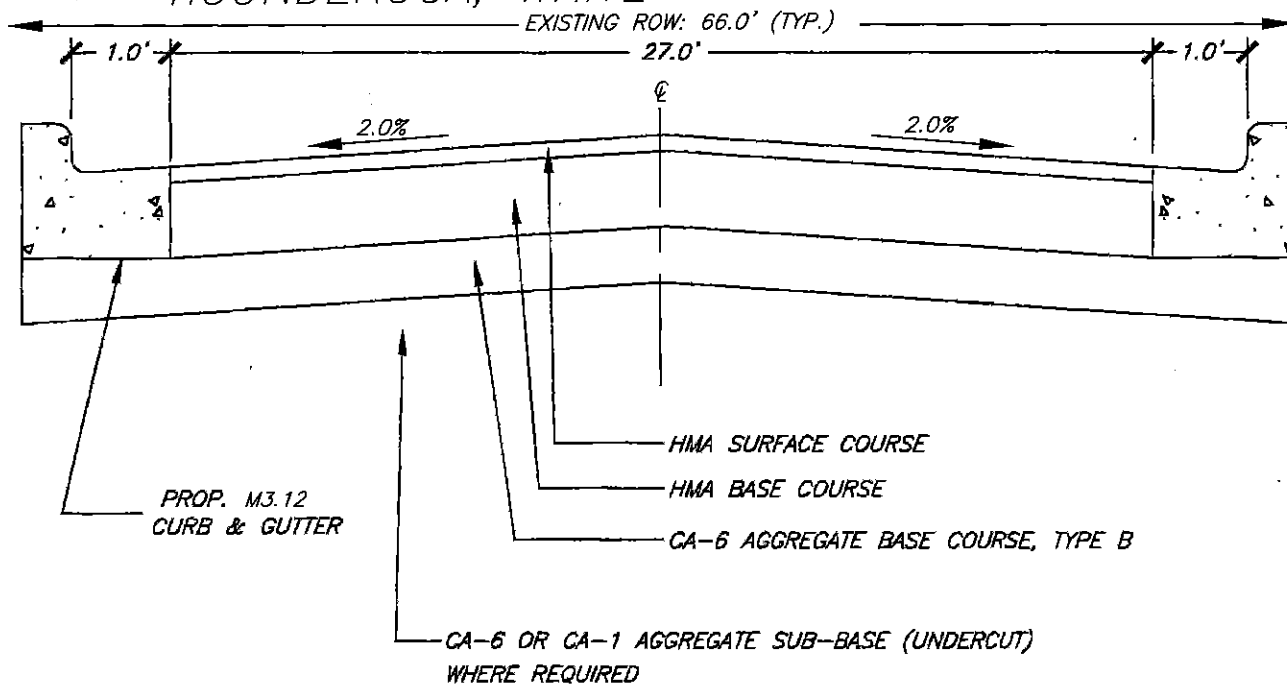
Landscaping work falls under the existing classifications for laborer, operating engineer and truck driver. The work performed by landscape plantsman and landscape laborer is covered by the existing classification of laborer. The work performed by landscape operators (regardless of equipment used or its size) is covered by the classifications of operating engineer. The work performed by landscape truck drivers (regardless of size of truck driven) is covered by the classifications of truck driver.

MATERIAL TESTER & MATERIAL TESTER/INSPECTOR I AND II


Notwithstanding the difference in the classification title, the classification entitled "Material Tester I" involves the same job duties as the classification entitled "Material Tester/Inspector I". Likewise, the classification entitled "Material Tester II" involves the same job duties as the classification entitled "Material Tester/Inspector II".

FOR STREETS:

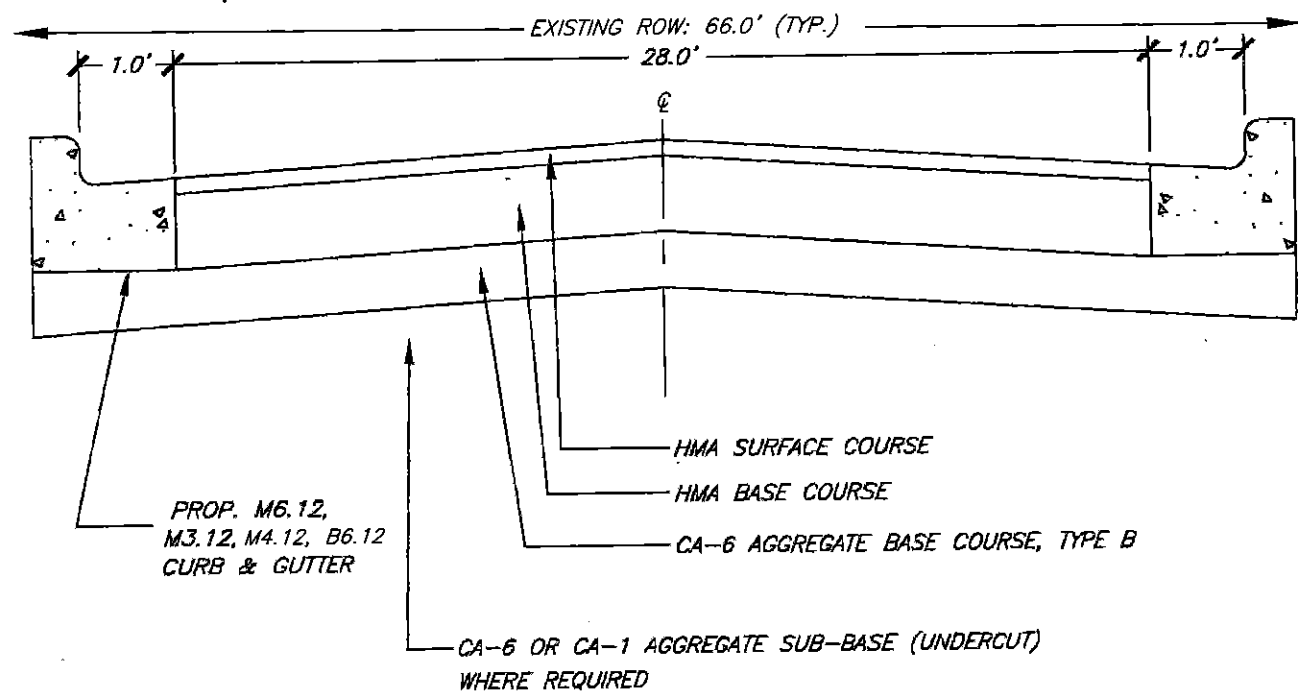
BATEMAN, BARRETT, BINDER, BORMAN,
 CAMDEN, DEVEREUX (CAMDEN TO BORMAN),
 GRAHAM, HALL, HATCH, HAWKINS, HUGHES,
 KIDWELL, KLEIN, ONEILL, POWELL, SELIG,
 TICONDEROGA, WHITE




	RESIDENTIAL STREETS
PAVEMENT WIDTH	27'
HMA SURFACE COURSE	2"
HMA BASE COURSE	6"
AGG. BASE COURSE	6"
AGG. SUB-BASE (UNDERCUT)	0 TO 24" AS DETERMINED BY ENGINEER

N.T.S.	DATE	REVISIONS	DRAWN BY	APPVD BY	STANDARD DETAIL
	01/01/19	//////	N.R.H.	J.M.W.	BITUMINOUS IMPROVED ROADWAY
DRAWING NO. PVT-13-05					
I:\LIBRARY\DETAILS\PAVEMENT\PVT-13					

FOR STREETS: 61ST, 62ND, 62ND CT, FAIRMOUNT,
 LYMAN, PARK, STRATFORD

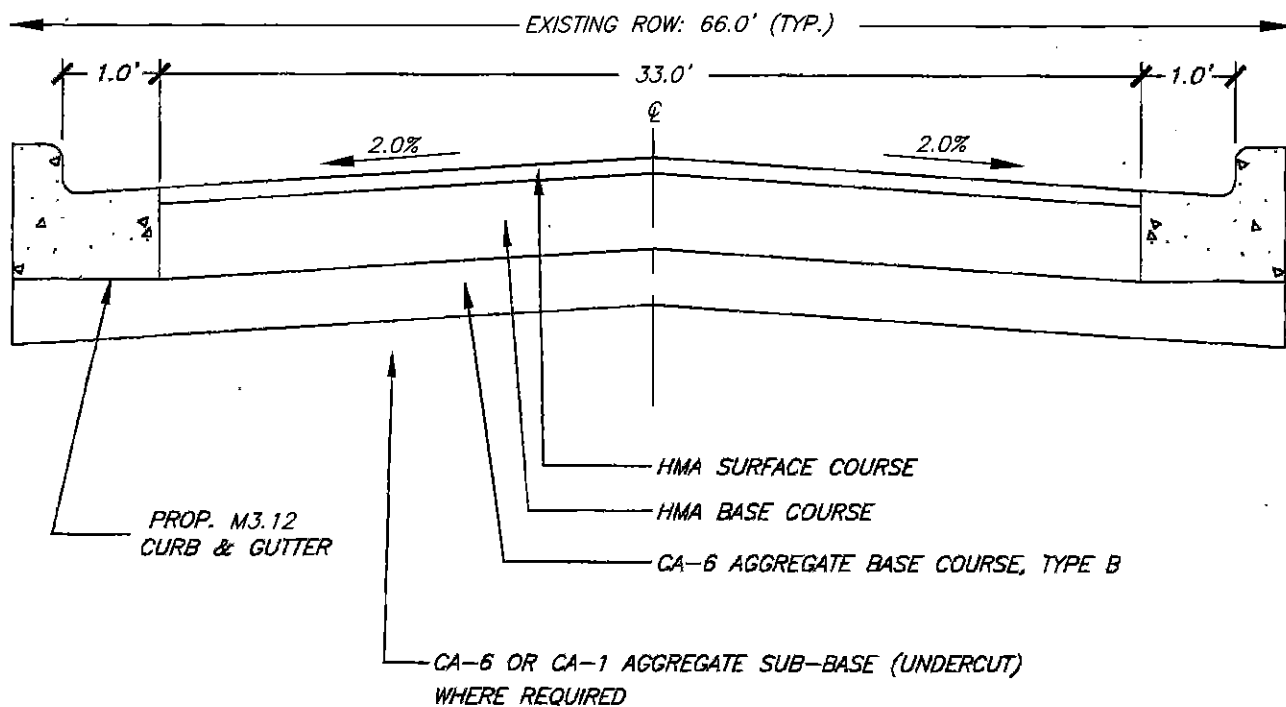


	RESIDENTIAL STREETS
PAVEMENT WIDTH	28'
HMA SURFACE COURSE	2"
HMA BASE COURSE	6"
AGG. BASE COURSE	6"
AGG. SUB-BASE (UNDERCUT)	0 TO 24" AS DETERMINED BY ENGINEER

N.T.S.	DATE	REVISIONS	DRAWN BY	APPVD BY	STANDARD DETAIL
	01/01/19	//////	N.R.H.	J.M.W.	BITUMINOUS IMPROVED ROADWAY
DRAWING NO. PVT-13-04					
I:\LIBRARY\DETAILS\PAVEMENT\PVT-13					

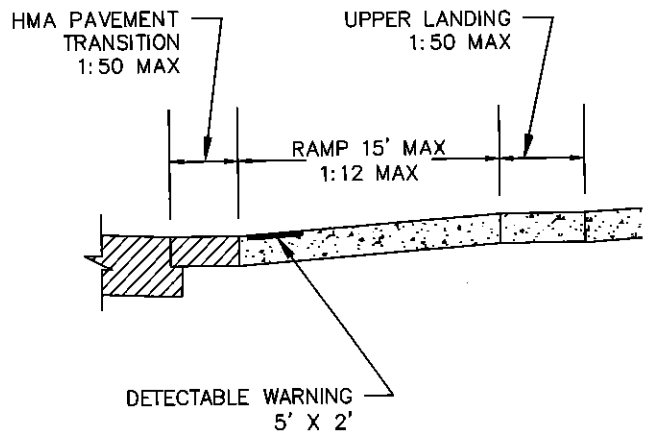
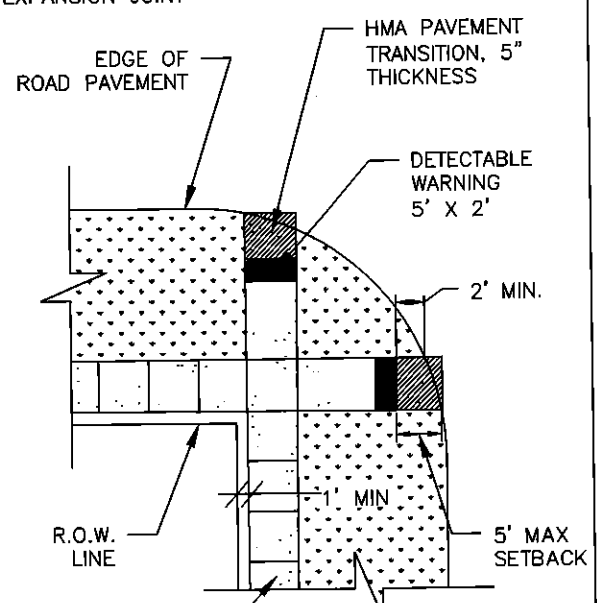
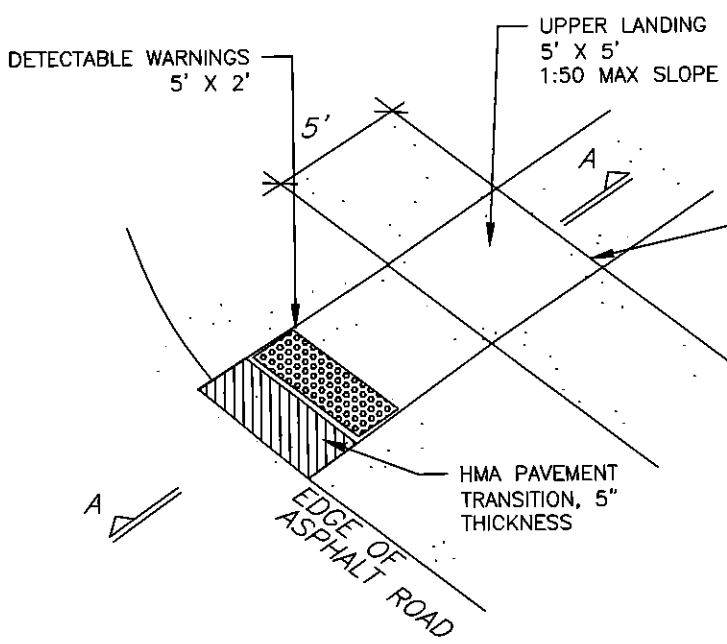
FOR STREETS:

DEVEREUX RD (BORMAN PL TO 75TH ST)



	RESIDENTIAL STREETS
PAVEMENT WIDTH	38.0'
HMA SURFACE COURSE	2"
HMA BASE COURSE	6"
AGG. BASE COURSE	6"
AGG. SUB-BASE (UNDERCUT)	0 TO 24" AS DETERMINED BY ENGINEER


N.T.S.	DATE	REVISIONS	DRAWN BY	APPVD BY	STANDARD DETAIL
	01/01/19		N.R.H.	J.M.W.	BITUMINOUS IMPROVED ROADWAY
DRAWING NO. PVT-13-05					
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RAMPS SHALL MEET REQUIREMENTS OF IDOT STD. DETAILS FOR CURB RAMPS.

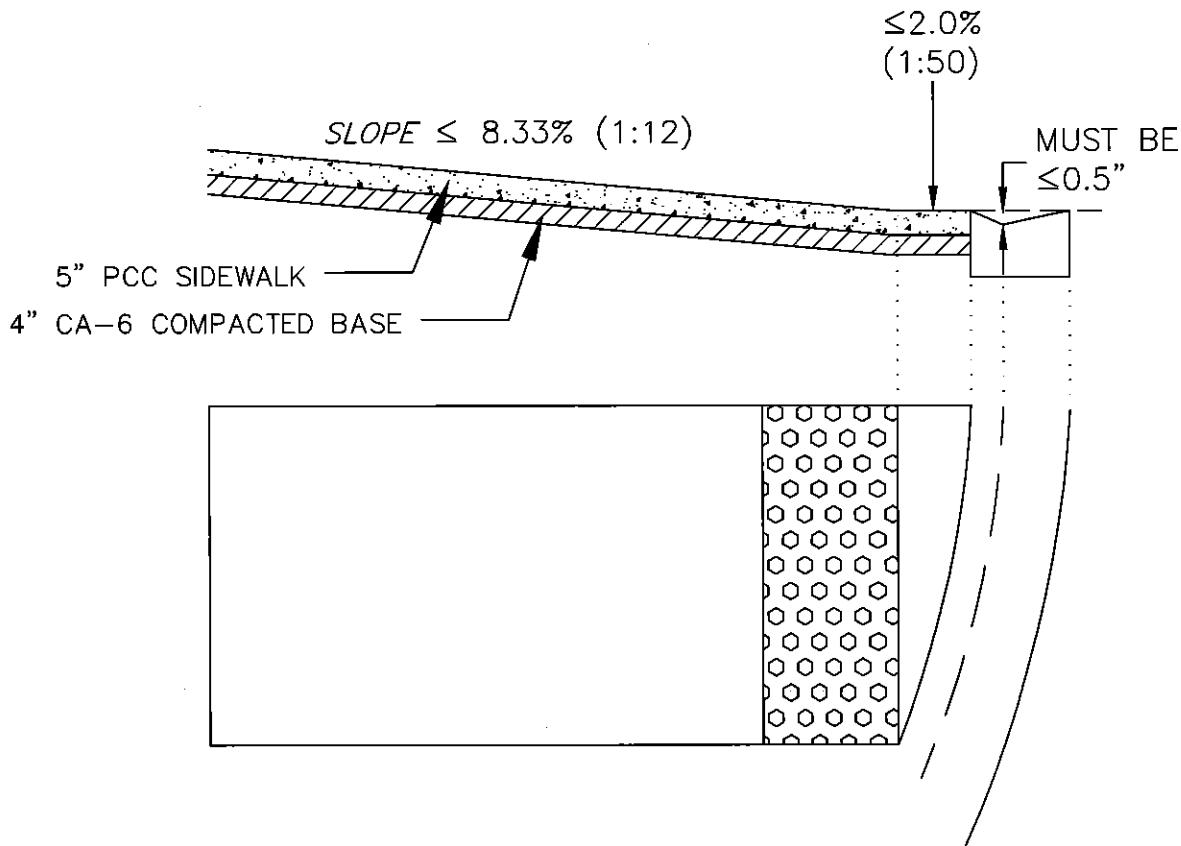
INSERT ADA DETECTABLE WARNING SURFACE TILE AS SPECIFIED. DYED CONCRETE NOT ALLOWED.


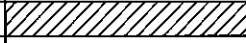
THE TYPE OF DETECTABLE WARNING TILE SHALL BE ARMOR-TILE, ACCESS TILE, OR TUFTILE TACTILE WARNING SYSTEMS.

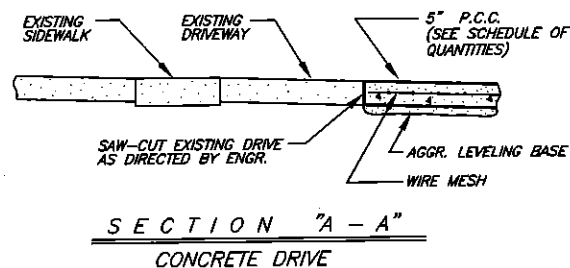
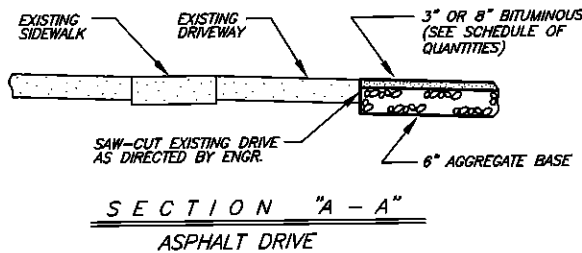
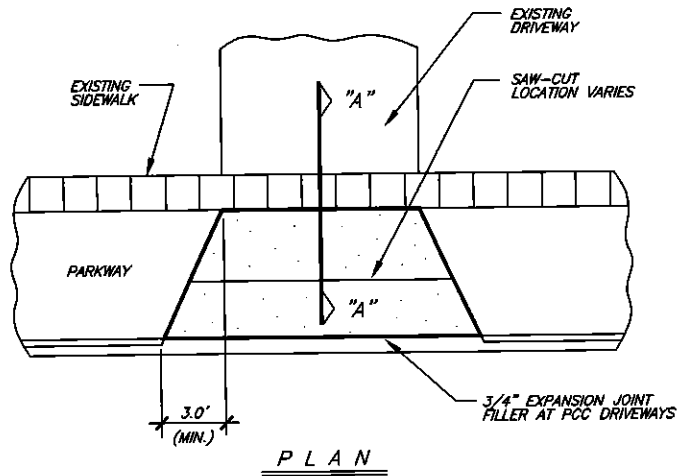
N.T.S.	DATE	REVISIONS	DRAWN BY	APPVD BY	STANDARD DETAIL
	03/25/11		S.A.V.	A.J.S.	A.D.A RAMPS ON NON-CURBED STREETS
	03/26/12		T.J.T.	A.J.S.	
	03/01/15		A.J.S.	A.J.S.	
	01/01/17		N.R.H.	J.M.W.	
	01/01/18		N.R.H.	J.M.W.	
	DRAWING NO.SWK-03				
I:\LIBRARY\DETAILS\SIDEWALK\SWK-03					

ADA SIDEWALK CORNER CONSTRUCTION REQUIREMENTS

1. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ILLINOIS DEPARTMENT OF TRANSPORTATION CONSTRUCTION DETAILS FOR CURB RAMPS.
2. SIDEWALK AT AND BEYOND DETECTABLE WARNING SHALL HAVE A SLOPE THAT DOES NOT EXCEED 1:12 (8.33%).
3. SIDEWALK BETWEEN DETECTABLE WARNING AND CURB SHALL HAVE A SLOPE THAT DOES NOT EXCEED 1:50 (2.0%).
4. VERTICAL RISE OF CURB THROUGH PEDESTRIAN ACCESS ROUTE AT CURB SHALL NOT EXCEED 0.5". CURB SHALL ALSO ESTABLISH A DEFINED FLOWLINE.



N.T.S.	DATE	REVISIONS	DRAWN BY	APPVD BY	STANDARD DETAIL
	11/17/20		S.W.G.	J.M.W.	ADA SIDEWALK CORNER CONSTRUCTION REQUIREMENTS
DRAWING NO. SWK-09					
I:\LIBRARY\DETAILS\SIDEWALK\SWK-09					



DRIVEWAY REMOVAL & REPLACEMENT

N.T.S.

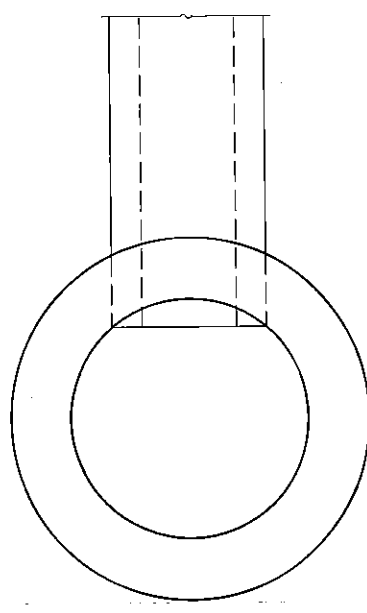
VILLAGE OF DOWNERS GROVE
PUBLIC WORKS ENGINEERING DIVISION

5101 WALNUT AVENUE 60515 TEL.(630) 434-5460

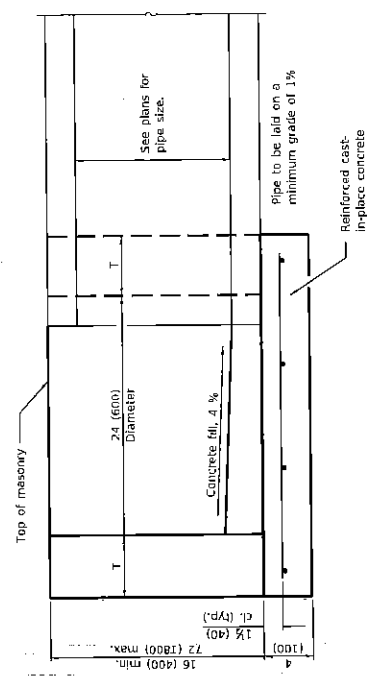


DETAILS

DRAWN BY: RWE	CHECKED BY: SLB	APPROVED BY: MDW
DATE: 05/03	PATH: C:\DWGFILES\RESURFACING\DETAILS	
Proj. No. #1-03	PAGE 4 of 4	

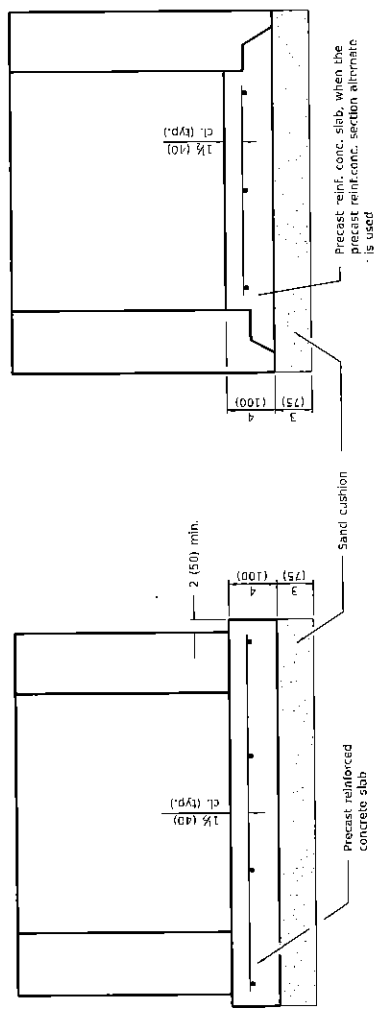


PLAN



ELEVATION

ALTERNATE MATERIALS FOR WALLS	T
BRICK MASONRY	8 (200)
CAST-IN-PLACE CONCRETE	6 (150)
CONCRETE MASONRY UNIT	5 (125)
PRECAST REINFORCED CONCRETE SECTION	3 (75)



ALTERNATE METHODS

GENERAL NOTES

Bottom slabs shall be reinforced with a minimum of 0.24 sq. in./ft. (510 sq. mm/m) in both directions with a maximum spacing of 10 (250).
 Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.
 All dimensions are in inches (millimeters) unless otherwise shown.

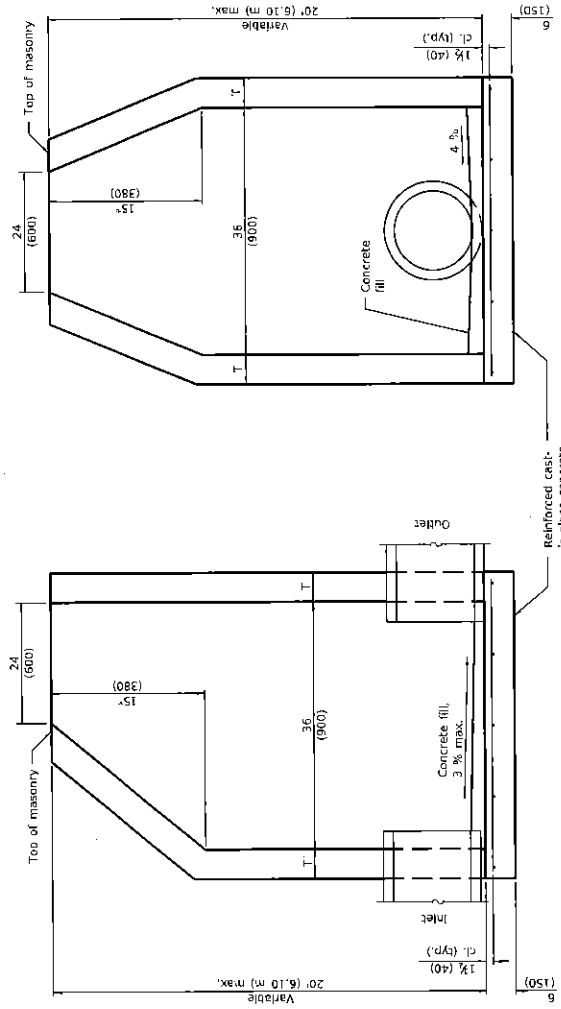
Illinois Department of Transportation
 ISSUED 1-1-97
 January 1, 2014
 Michael B. ...
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2014
 ENGINEER OF SURVEY AND ENVIRONMENT

DATE	REVISIONS
1-1-94	Increased height to 72 (1800) maximum.
1-1-11	Detailed rein. in slabs. Added max. limit to height. Added general notes.

INLET - TYPE A

STANDARD 602301-04

* For precast reinforced concrete sections, this dimension may vary from the dimension given to plus 6 (150).



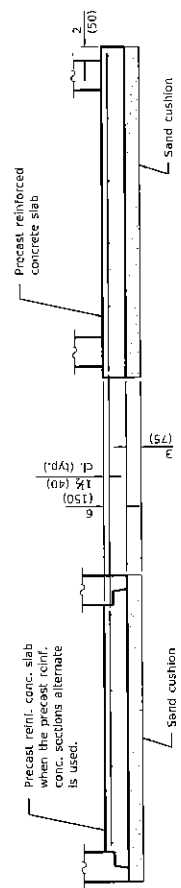
ELEVATION - CONCENTRIC

ELEVATION - ECCENTRIC

ALTERNATE MATERIALS FOR WALLS	T (min.)
Concrete Masonry Unit	5 (125)
Brick Masonry	8 (200)
Precast Reinforced Concrete Section	3 (75)
Cast-in-Place Concrete	6 (150)

GENERAL NOTES
 Bottom slabs shall be reinforced with a minimum of 0.20 sq. in./ft. (420 sq. mm/m) in both directions with a maximum spacing of 12 (300).
 Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

See Standard 602601 for optional Precast Reinforced Concrete Flat Slab Top.
 All dimensions are in inches (millimeters) unless otherwise shown.



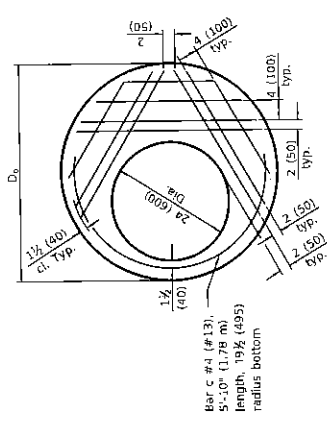
ALTERNATE BOTTOM SLAB

DATE	REVISIONS
1-1-11	Detailed rein. in slabs. Added max. limit to height.
	Revised general notes.
1-1-09	Switched units to English (metric).

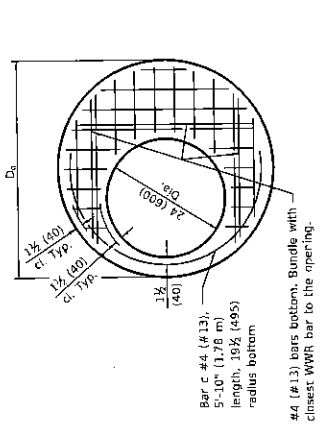
Illinois Department of Transportation
 ISSUED 1-1-07
 January 1, 2011
 Michael Bond
 ENGINEER OF PUBLIC WORKS AND PROCEDURES
 APPROVED
 January 1, 2011
 ENGINEER OF DESIGN AND ENVIRONMENT

INLET - TYPE B

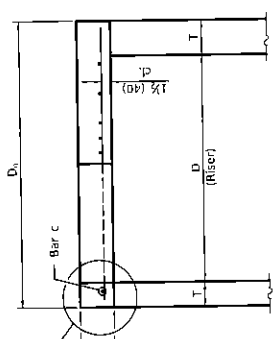
STANDARD 602306-03



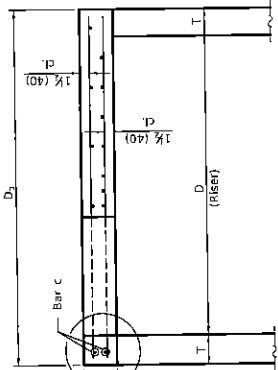
PLAN - FLAT SLAB TOP FOR D = 36 (900)
(Showing layout of reinforcement bars and c bars)



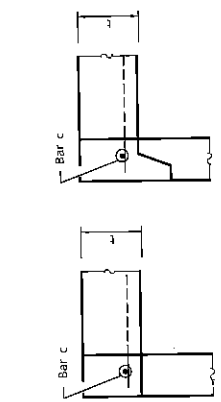
PLAN - FLAT SLAB TOP FOR D = 36 (900)
(Showing layout of welded wire reinforcement and c bars)



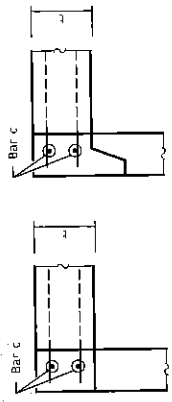
SECTION THRU FLAT SLAB TOP
FOR D = 36 (900) AND D = 4'-0\"/>



SECTION THRU FLAT SLAB TOP
FOR D = 5'-0\"/>



FLAT SLAB TOP JOINT CONFIGURATIONS
FOR D = 36 (900) AND D = 4'-0\"/>



FLAT SLAB TOP JOINT CONFIGURATIONS
D = 5'-0\"/>

TABLE

D	T	D _o (mm)	t
36 (900)	See applicable Standards	27	6 (150)
4'-0\"/>			

GENERAL NOTES

The flat slab top may be used in lieu of the tapered tops shown on sheets 602001, 602016, or 602066 at the option of the Contractor or when field conditions prohibit the use of tapered tops.

Lifting holes shall be located in the sections as per the manufacturer's recommendations.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Expanded / refined reinforcement options.
1-1-18	Revised for compliance with LRFD.

PRECAST REINFORCED CONCRETE FLAT SLAB TOP
(Sheet 1 of 2)

STANDARD 602601-06

Illinois Department of Transportation
 ISSUED 1-1-97
 PROJECT: _____ 2019
 ENGINEER OF POLICY AND PROCEDURES: _____
 APPROVED: _____ 2019
 ENGINEER OF PRECAST REINFORCEMENT: _____

FLAT SLAB TOP REINFORCEMENT FOR D = 36 (900)

Location	WWR (each direction)	As (min.)	As (max.)	Rebar Spacing (max.)	Bar Size
Bottom Mat	* 0.60 sq. in./ft. (1270 sq. mm/m)	6	6	See plan view for rebar orientation and spacing and this table for bar size	#4 (#13)

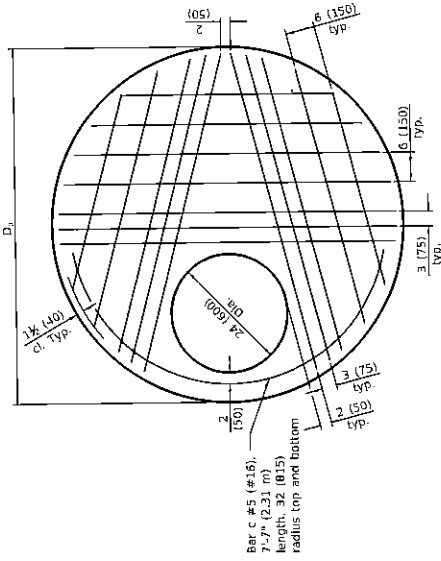
FLAT SLAB TOP REINFORCEMENT FOR D = 4'-0" (1.22 m)

Location	WWR (each direction)	As (min.)	As (max.)	Rebar Spacing (max.)	Bar Size
Bottom Mat	* 0.62 sq. in./ft. (1312 sq. mm/m)	6	6	See plan view for rebar orientation and spacing and this table for bar size	#5 (#16)

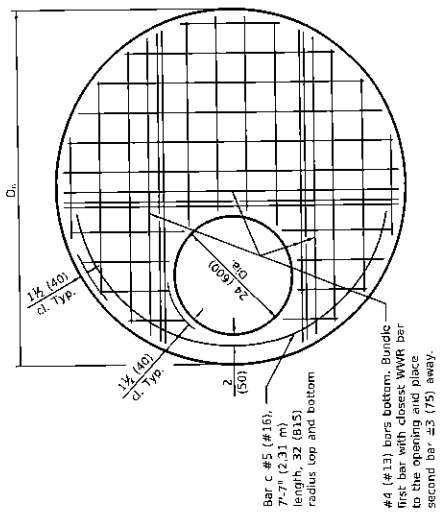
FLAT SLAB TOP REINFORCEMENT FOR D = 5'-0" (1.52 m)

Location	WWR (each direction)	As (min.)	As (max.)	Rebar Spacing (max.)	Bar Size
Top Mat	0.11 sq. in./ft. (233 sq. mm/m)	18	18	See plan view for rebar orientation and spacing and this table for bar size	#3 or #4 (#10) (#13)
Bottom Mat	* 0.40 sq. in./ft. (847 sq. mm/m)	6	6	See plan view for rebar orientation and spacing and this table for bar size	#4 (#13)

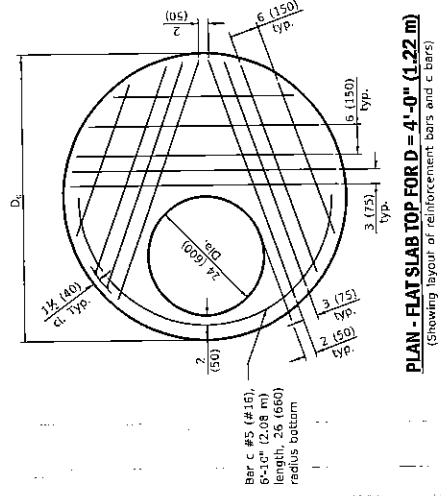
* Only one layer of WWR permitted to avoid congestion.



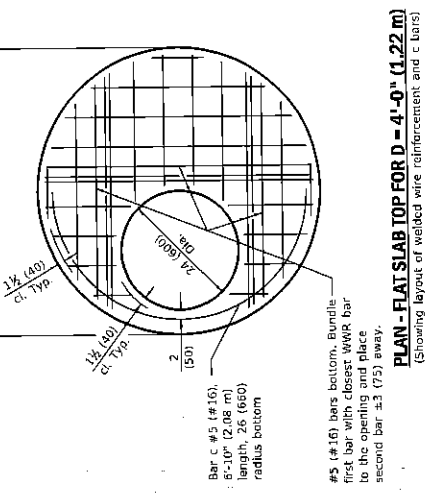
PLAN - FLAT SLAB TOP FOR D = 5'-0" (1.52 m)
(Showing layout of bottom reinforcement bars and c bars)



PLAN - FLAT SLAB TOP FOR D = 5'-0" (1.52 m)
(Showing layout of welded wire reinforcement and c bars)



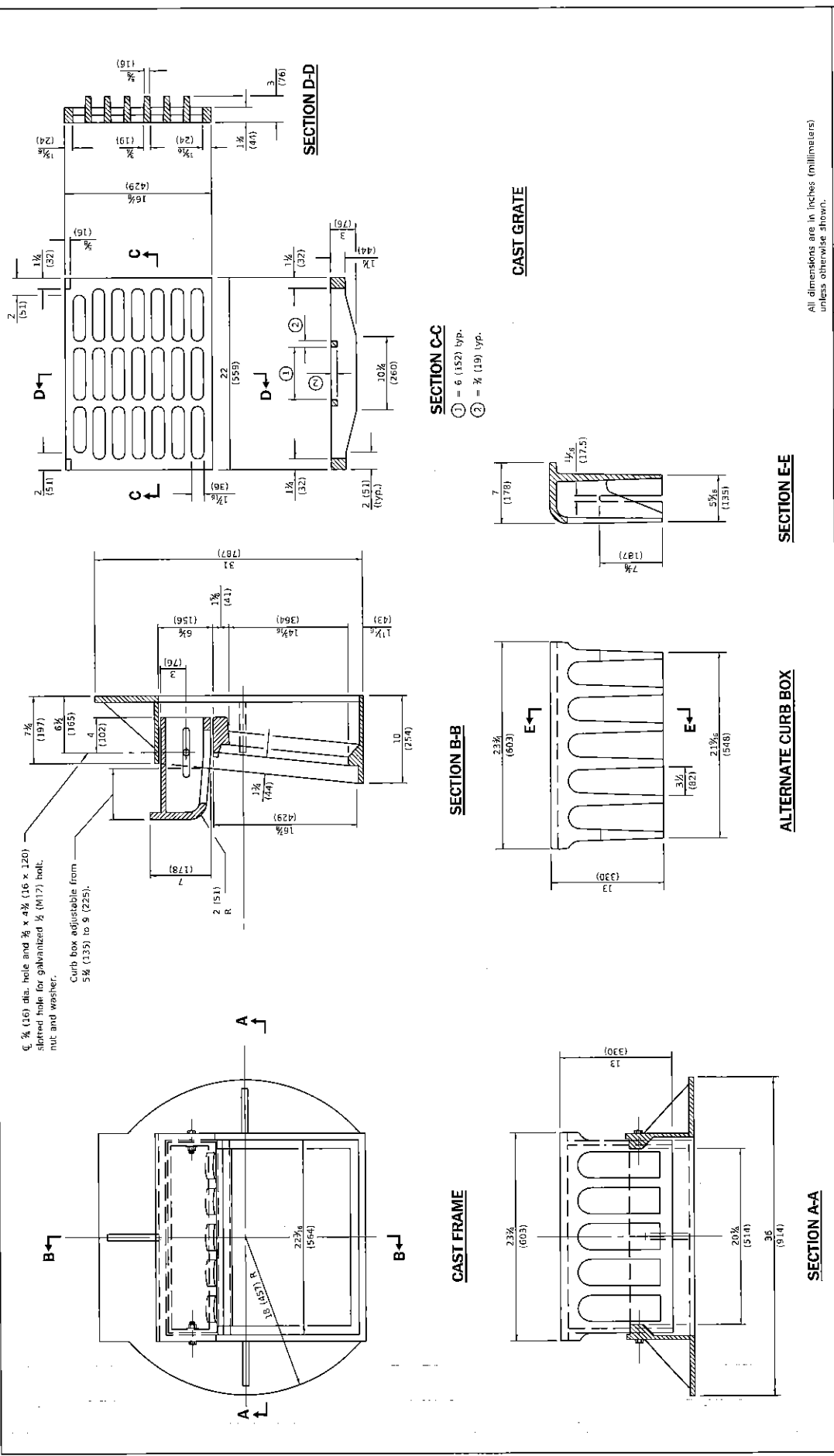
PLAN - FLAT SLAB TOP FOR D = 4'-0" (1.22 m)
(Showing layout of reinforcement bars and c bars)



PLAN - FLAT SLAB TOP FOR D = 4'-0" (1.22 m)
(Showing layout of welded wire reinforcement and c bars)

PRECAST REINFORCED CONCRETE FLAT SLAB TOP
(Sheet 2 of 2)
STANDARD 602601-06

Illinois Department of Transportation
 ISSUE 1-1-97
 ENGINEER OF POLICE AND PROCEDURES
 APPROVED
 ENGINEER OF DESIGN AND CONSTRUCTION



C. 3/8" dia. hole and 3/4" x 4 1/2" (116 x 120) slotted hole for galvanized 1/2" (M17) bolt, nut and washer.
 Curb box adjustable from 5 3/4" (135) to 9" (225).

SECTION C-C
 ① = 6 (152) typ.
 ② = 3/8" (19) typ.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
DATE	REVISED DIMENSIONS OF FRAME AND ALTERNATE CURB BOX
1-1-15	REVISED DIMENSIONS OF FRAME AND ALTERNATE CURB BOX.
1-1-09	SWITCHED UNITS TO ENGLISH (METRIC).

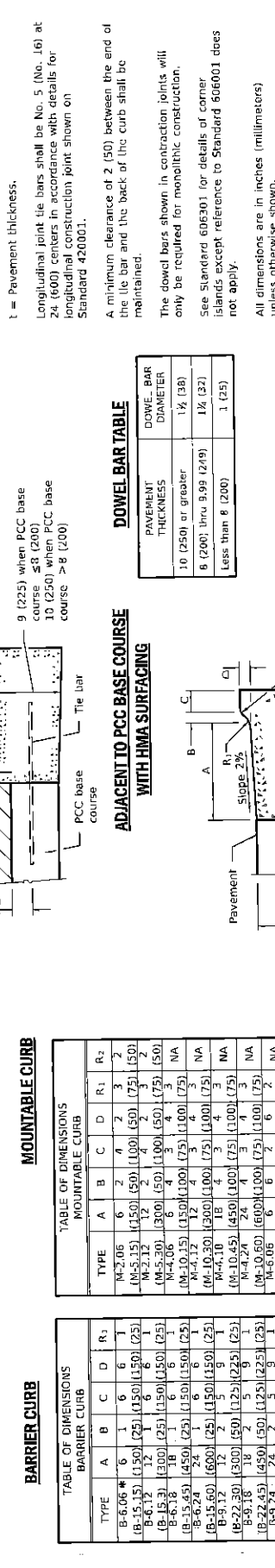
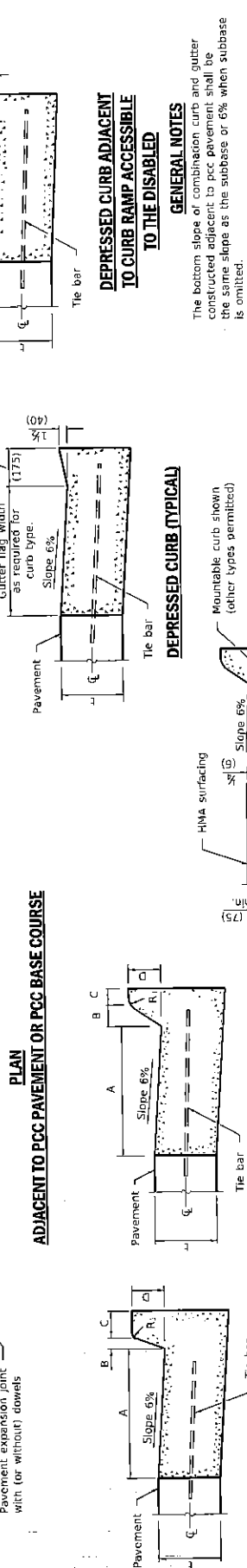
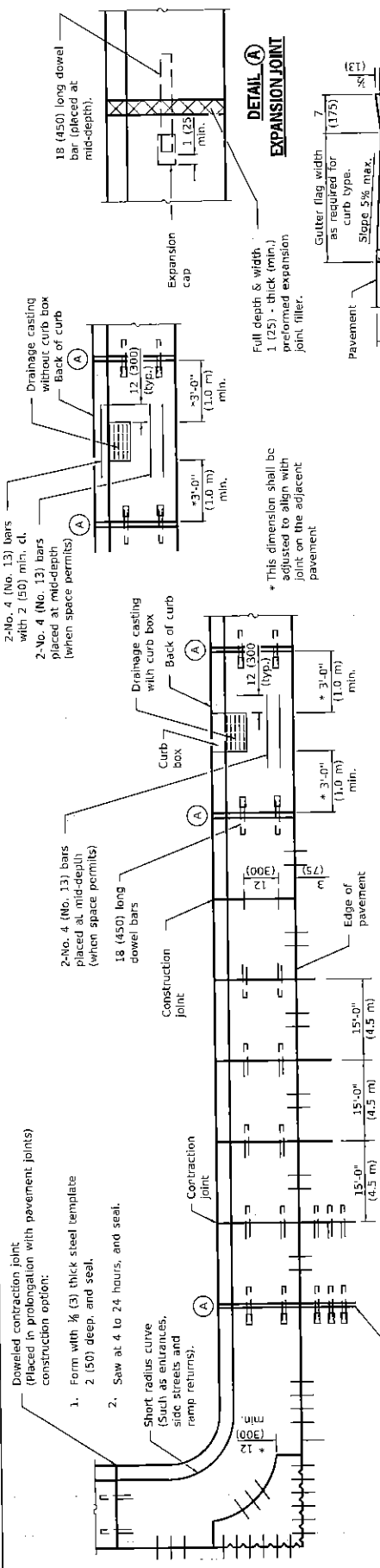
Illinois Department of Transportation
ISSUED 1-1-97

APPROVED BY: *[Signature]*
2015
 ENGINEER OF POLICY AND PROCEDURES

APPROVED BY: *[Signature]*
2015
 ENGINEER OF DESIGN AND ENVIRONMENT

**FRAME AND GRATE
TYPE 3**

STANDARD 604006-05



GENERAL NOTES

The bottom slope of combination curb and gutter constructed adjacent to pcc pavement shall be the same slope as the subbase or 6% when subbase is omitted.

t = Pavement thickness.

Longitudinal joint tie bars shall be No. 5 (No. 16) at 24 (600) centers in accordance with details for longitudinal construction joint shown on Standard 4240001.

A minimum clearance of 2 (50) between the end of the tie bar and the back of the curb shall be maintained.

The dowel bars shown in construction joints will only be required for monolithic construction.

See Standard 606301 for details of corner islands except reference to Standard 606001 does not apply.

All dimensions are in inches (millimeters) unless otherwise shown.

DOWEL BAR TABLE

PAVEMENT THICKNESS	DOWEL BAR DIAMETER
10 (250) or greater	1 1/4 (38)
8 (200) thru 9.99 (249)	1 1/8 (32)
less than 8 (200)	1 (25)

REVISIONS

DATE	REVISIONS
1-1-18	New standard.

TABLE OF DIMENSIONS MOUNTABLE CURB

TYPE	A	B	C	D	R ₁	R ₂
M-2.06	6	2	4	2	3	2
(M-5.15)	(150)	(50)	(100)	(50)	(75)	(50)
M-2.12	12	2	4	3	4	3
(M-5.30)	(300)	(50)	(100)	(50)	(75)	(50)
M-4.15	15	3	4	3	3	NA
(M-10.15)	(150)	(100)	(75)	(100)	(75)	NA
M-4.12	12	4	3	4	3	NA
(M-10.30)	(300)	(100)	(75)	(100)	(75)	NA
M-4.10	10	4	3	4	3	NA
(M-10.45)	(450)	(100)	(75)	(100)	(75)	NA
M-4.24	24	4	3	4	3	NA
(M-10.60)	(600)	(100)	(75)	(100)	(75)	NA
M-5.06	6	6	6	6	6	NA
(M-15.15)	(150)	(150)	(50)	(150)	(50)	NA
M-5.10	10	5	2	6	2	NA
(M-15.30)	(300)	(150)	(50)	(150)	(50)	NA
M-5.16	16	6	2	6	2	NA
(M-15.45)	(450)	(150)	(50)	(150)	(50)	NA
M-6.24	24	6	2	6	2	NA
(M-15.60)	(600)	(150)	(50)	(150)	(50)	NA

TABLE OF DIMENSIONS BARRIER CURB

TYPE	A	B	C	D	R ₁	R ₂
B-6.06*	6	1	6	6	1	1
(B-15.15)	(150)	(25)	(150)	(150)	(25)	(25)
B-6.12	12	1	6	6	1	1
(B-15.30)	(300)	(25)	(150)	(150)	(25)	(25)
B-6.18	18	1	6	6	1	1
(B-15.45)	(450)	(25)	(150)	(150)	(25)	(25)
B-6.24	24	1	6	6	1	1
(B-15.60)	(600)	(25)	(150)	(150)	(25)	(25)
B-9.12	12	2	5	9	1	1
(B-22.30)	(300)	(50)	(125)	(225)	(25)	(25)
B-9.18	18	2	5	9	1	1
(B-22.45)	(450)	(50)	(125)	(225)	(25)	(25)
B-9.24	24	2	5	9	1	1
(B-22.60)	(600)	(50)	(125)	(225)	(25)	(25)

* For corner islands only.

CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
(Sect. 1 of 2)

B.I.R.-28

M-2.06 (M-5.15) and M-2.12 (M-5.30)

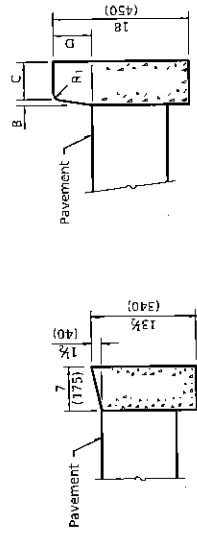
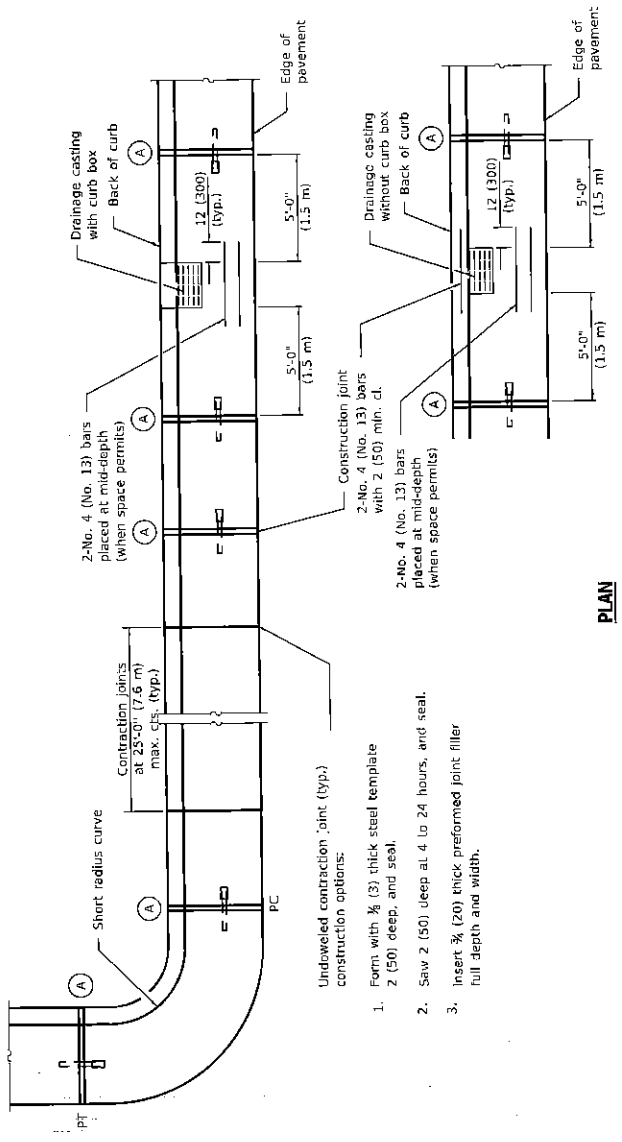
ISSUED 1-1-18

ENGINEER OF LOCAL ROADS AND STREETS

APPROVED BY

ILLINOIS DEPARTMENT OF TRANSPORTATION

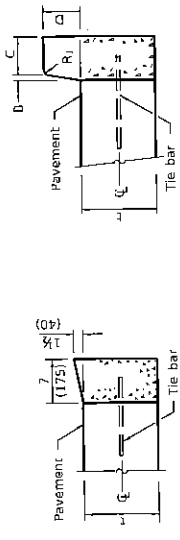
FOR CORNER ISLANDS ONLY



DEPRESSED CURB

DEPRESSED CURB

ADJACENT TO FLEXIBLE PAVEMENT



DEPRESSED CURB

BARRIER CURB

ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE

CONCRETE CURB TYPE B

ON UNDISTURBED SUBGRADE

ON DISTURBED SUBGRADE

ADJACENT TO FLEXIBLE PAVEMENT

**CONCRETE CURB TYPE B
AND COMBINATION
CONCRETE CURB AND GUTTER**
(Sheet 2 of 2)

B.L.R. 28

Illinois Department of Transportation

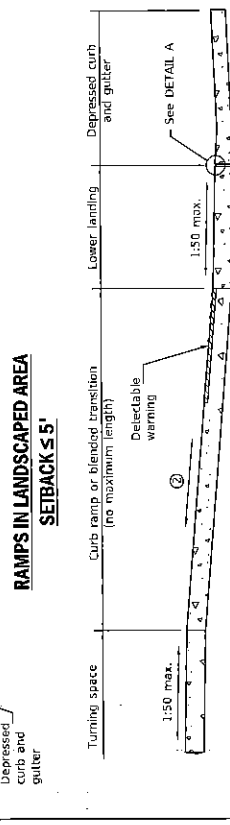
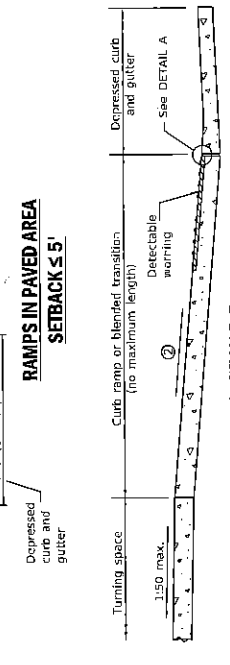
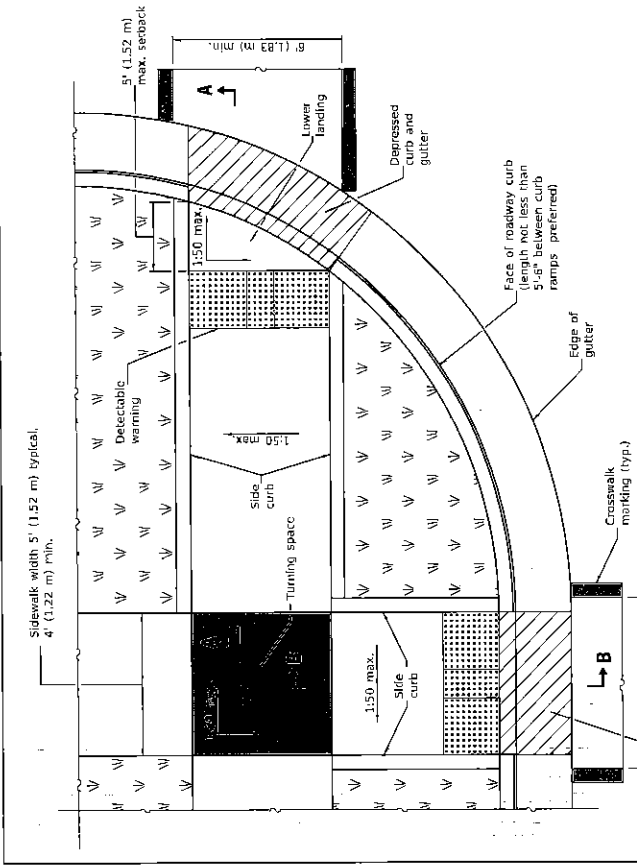
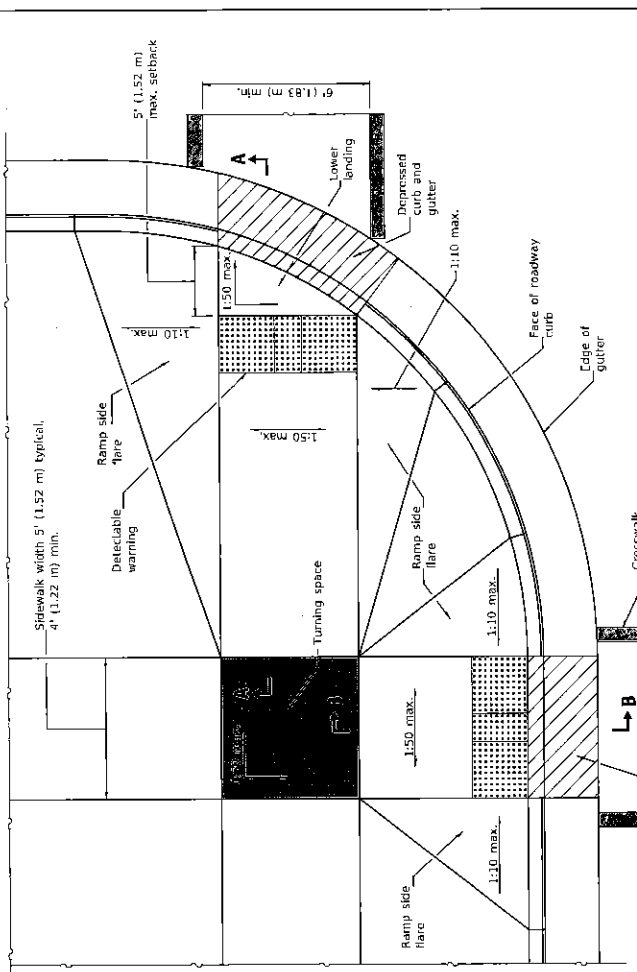
ISSUED 1-1-18

DESIGNED BY: [Signature]

APPROVED BY: [Signature]

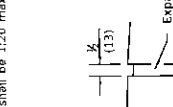
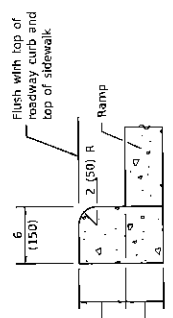
ENGINEER OF ROADWAYS AND STREETS

REGISTERED PROFESSIONAL ENGINEER



SECTION B-B
 ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

SECTION A-A
 ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



See Sheet 2 for GENERAL NOTES.

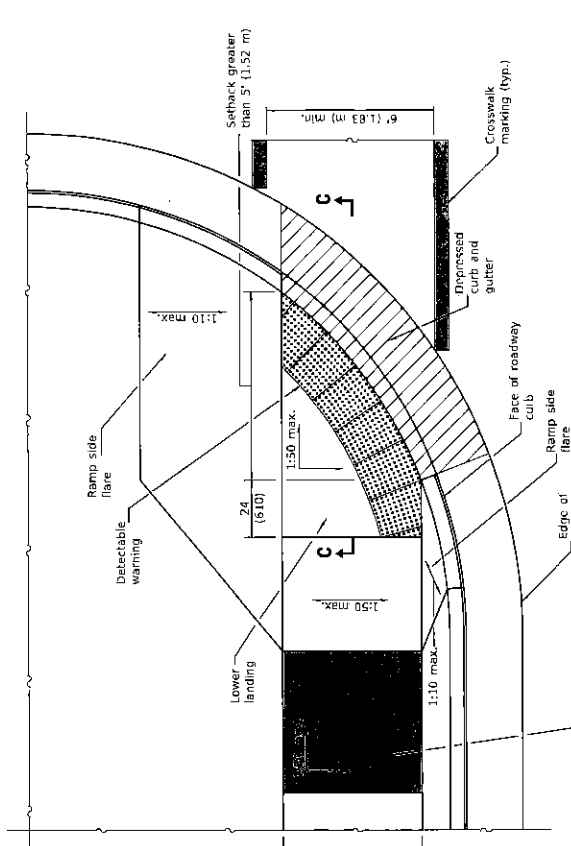
PERPENDICULAR CURB RAMPS FOR SIDEWALKS	
DATE	REVISIONS
1-1-19	Removed "15-foot rule", added "blended transitions" and placement tolerances for detectable warnings.
1-1-18	Chaired diagonal slope at turning spaces and lower landings.

STANDARD 424001-11
 (Sheet 1 of 2)

Illinois Department of Transportation
 ISSUED 1-1-97

APPROVED: [Signature] 2019
 ENGINEER OF POLICE AND TRANSPORTATION

APPROVED: [Signature] 2018
 ENGINEER OF DESIGN AND ENVIRONMENT



GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Slide Blocker - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

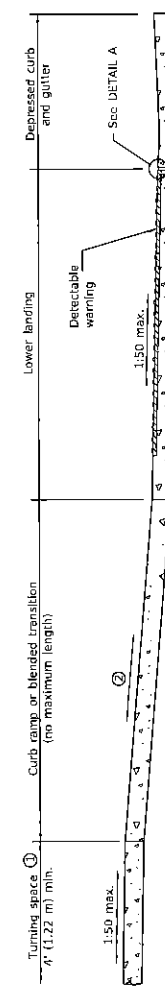
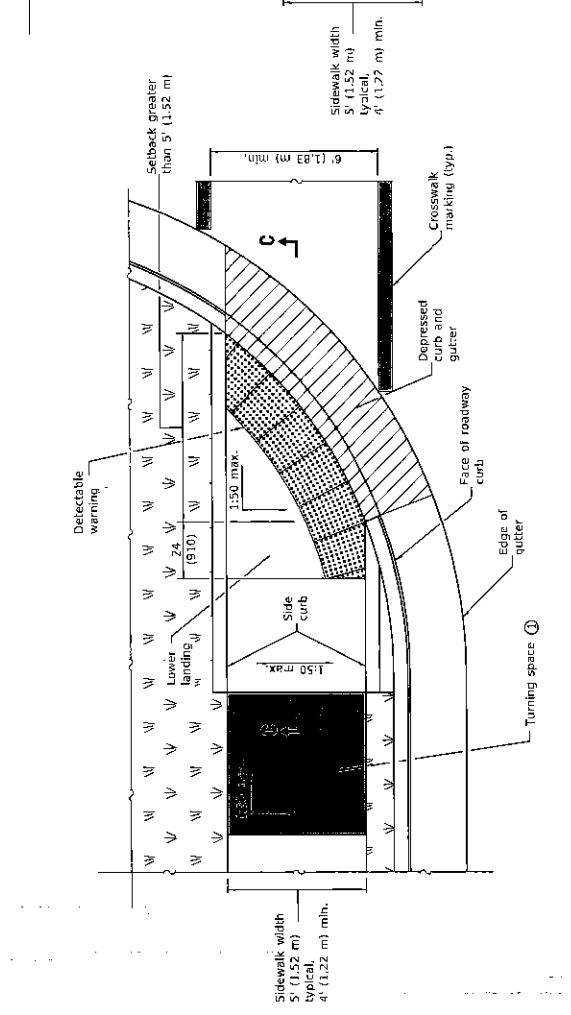
Curb Setback - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

**RAMP IN LANDSCAPED AREA
SETBACK > 5'**

**RAMP IN PAVED AREA
SETBACK > 5'**



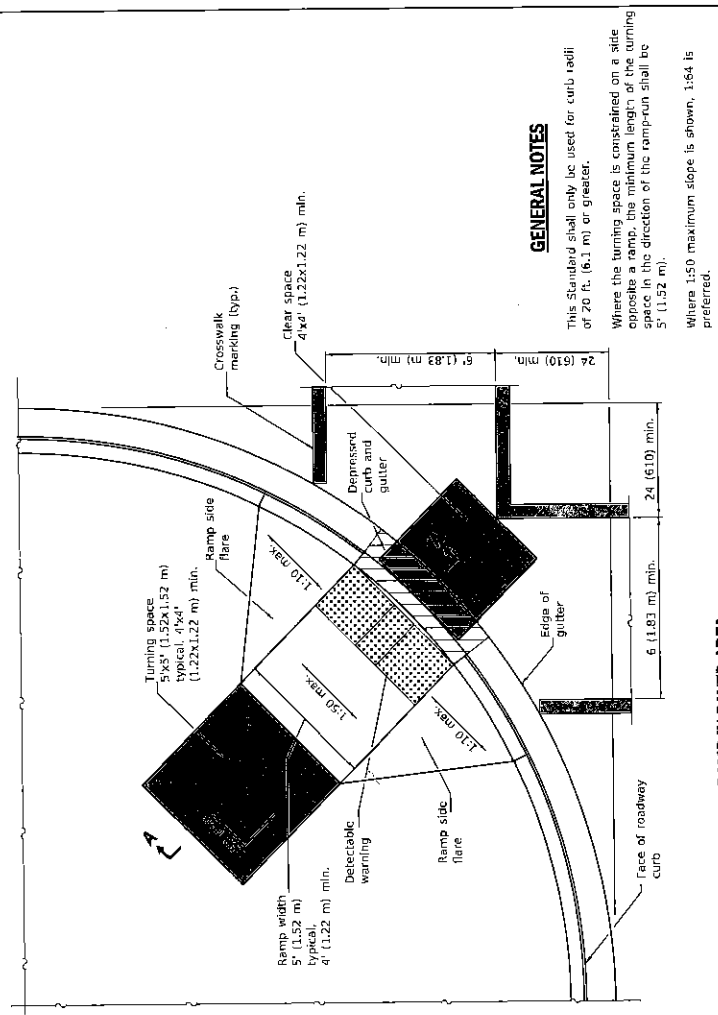
SECTION C-C

- ① This turning space not required for blended transitions.
- ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

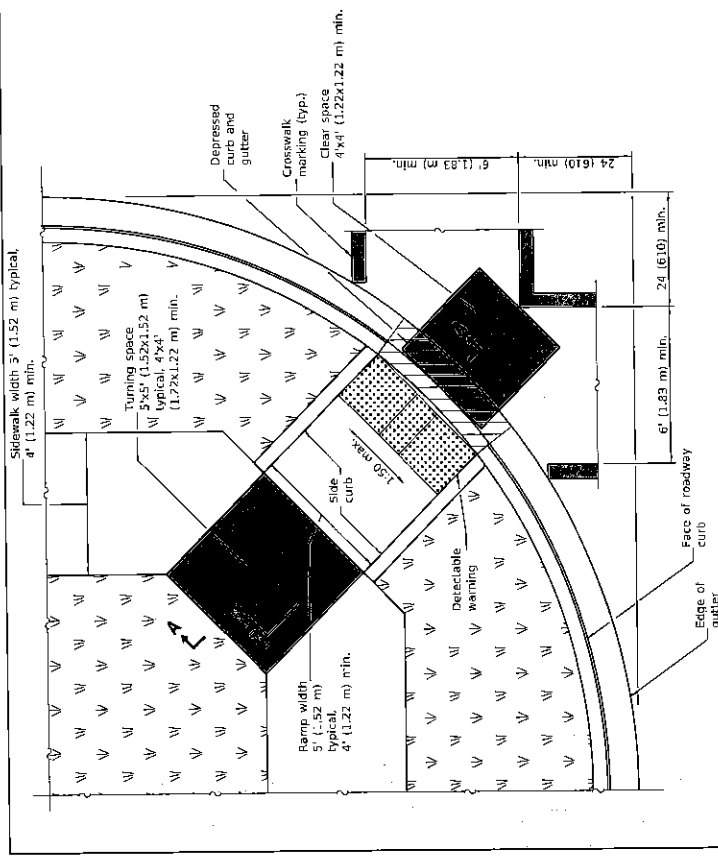
**PERPENDICULAR CURB RAMPS
FOR SIDEWALKS**
(Sheet 2 of 2)

STANDARD 424001-11

Illinois Department of Transportation PASSEY ENGINEER OF POLICY AND PROCEDURES APPROVED ENGINEER OF DESIGN AND ENVIRONMENT	ISSUED 1-1-97
	JANUARY 1, 2019
	PASSEY
	PASSEY



RAMP IN LANDSCAPED AREA



RAMP IN PAVED AREA

GENERAL NOTES

This Standard shall only be used for curb radii of 20 ft. (6.1 m) or greater.

Where the turning space is constrained on a site opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following tolerances are allowed.

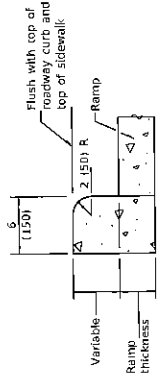
Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2' (0.61 m) in width is allowed.

Curb Setback - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

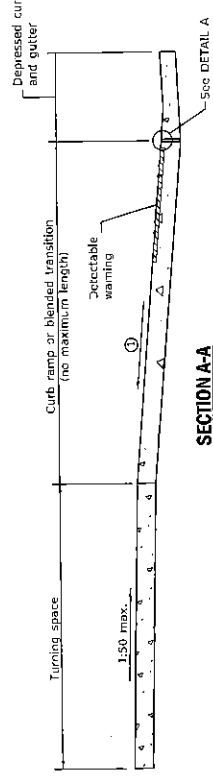
All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

See Standard 606001 for details of depressed curb adjacent to curb ramp.

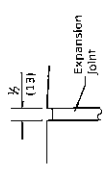
All dimensions are in inches (millimeters) unless otherwise shown.



SIDE CURB DETAIL



SECTION A-A



DETAIL A

DATE	REVISIONS
1-1-21	Cleared minimum crosswalk width and locations.
1-1-19	Removed "15-foot rule", added "blended transitions" and placement tolerances for detectable warnings.

DIAGONAL CURB RAMPS FOR SIDEWALKS

STANDARD 424006-05

Illinois Department of Transportation

ISSUED 1-1-12

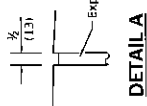
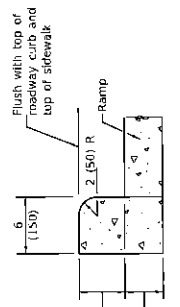
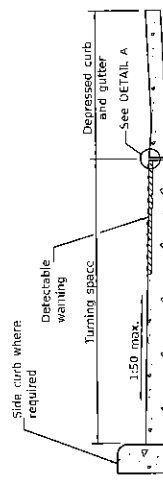
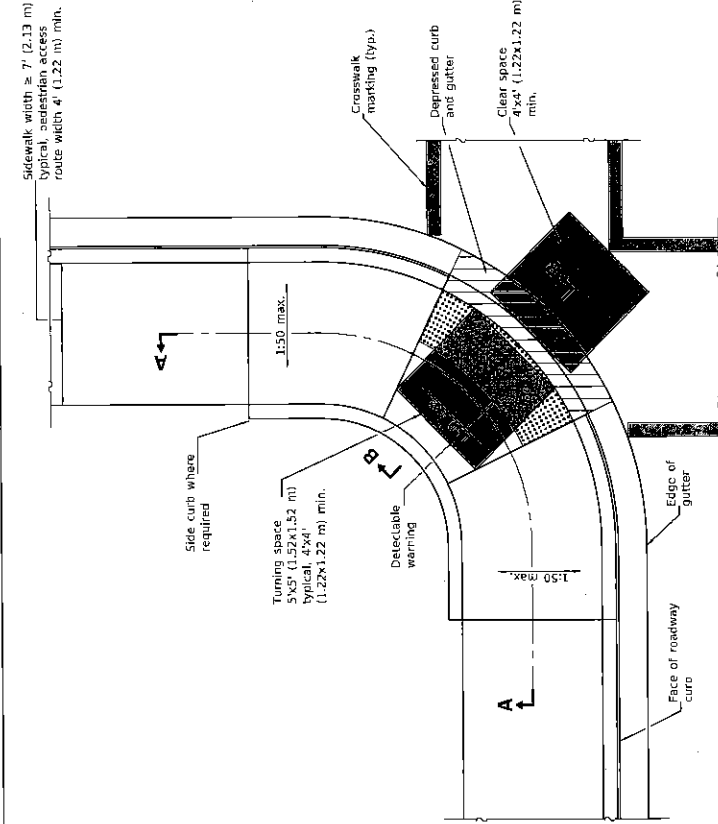
PROJECT: *ALP* 2971

ENGINEER OF POLICE AND RECORDS: *[Signature]*

APPROVED: *[Signature]* 2021

DESIGNED BY: *[Signature]*

ENGINEER OF DESIGN AND ENVIRONMENT: *[Signature]*



GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

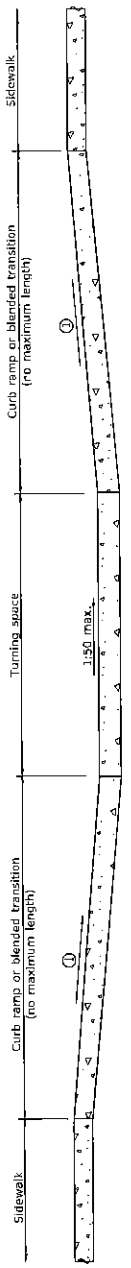
Side Border - Detectable warnings should extend the full width of the walking surface (excluding feroce sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

CORNER PARALLEL CURB RAMP



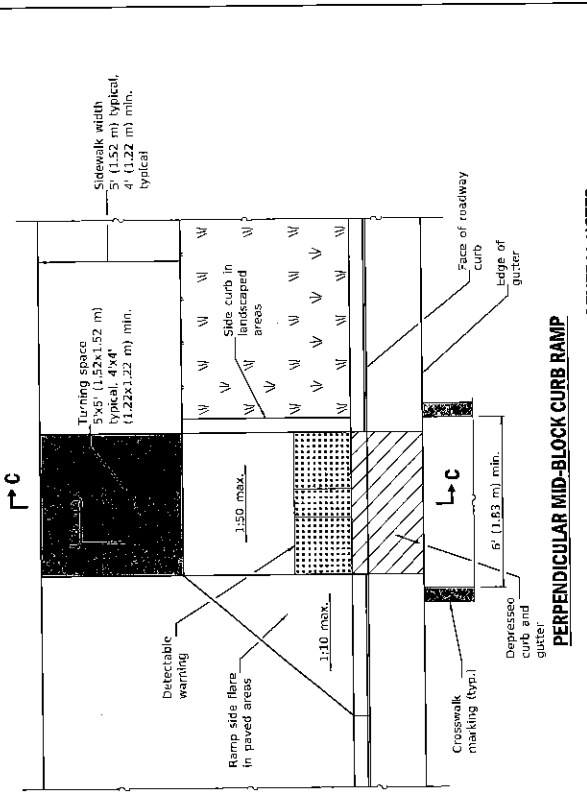
① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The turning slope or a blended transition shall be 1:20 max.

Illinois Department of Transportation DIVISION OF TRANSPORTATION ENGINEER OF POLICY AND PROCEDURES APPROVED ENGINEER OF DESIGN AND ENVIRONMENT		ISSUED 1-1-12 2018 2019
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DATE	REVISIONS
1-1-19	Removed upper landing, added blended transition and detectable warning tolerances.
1-1-17	Revised sidewalk width to include 24 (6:10) buffer behind curb.

CORNER PARALLEL CURB RAMPS FOR SIDEWALKS

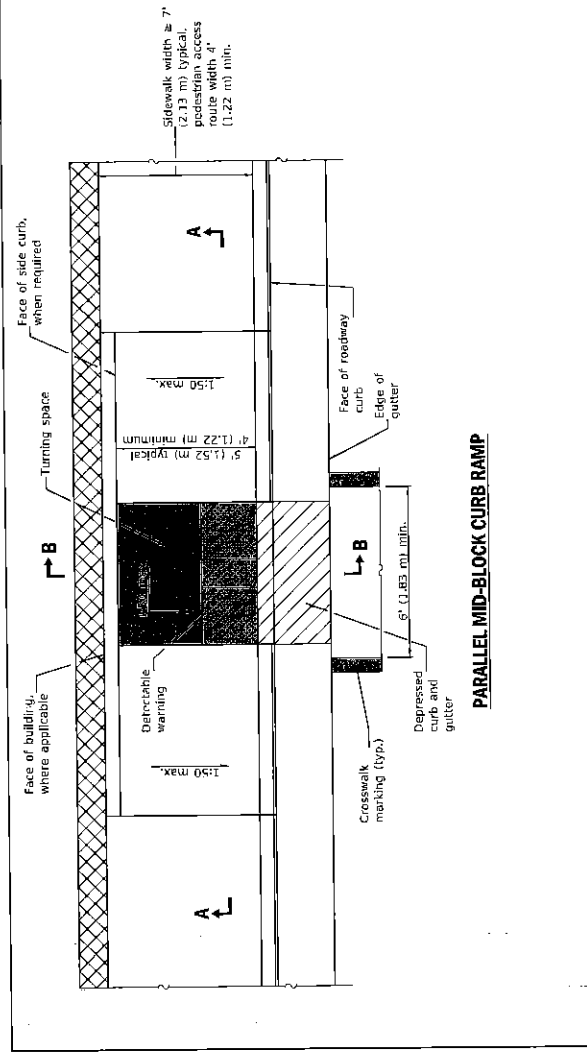
STANDARD 424011-04



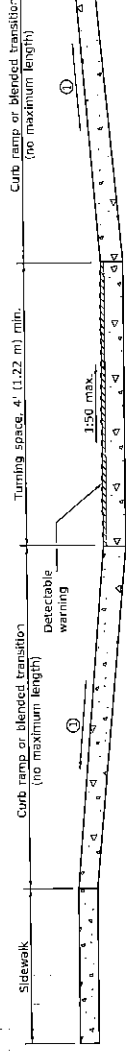
PERPENDICULAR MID-BLOCK CURB RAMP

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:1H).
 Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).
 Where 1:50 maximum slope is shown, 1:64 is preferred.
 Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.
 Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.
 Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but may go up to 6 in. (150 mm) behind the curb is allowed.
 See Standard 606001 for details of depressed curb adjacent to curb ramp.
 All dimensions are in inches (millimeters) unless otherwise shown.



PARALLEL MID-BLOCK CURB RAMP

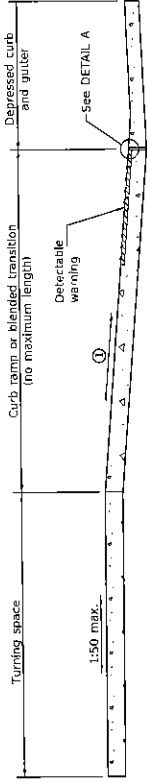


SECTION A-A

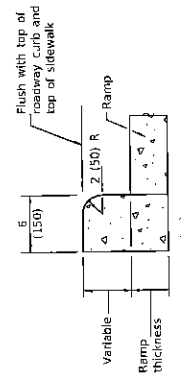
① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



SECTION B-B



SECTION C-C



SIDE CURB DETAIL



DETAIL A

DATE	REVISIONS
1-1-19	Removed upper landing, added blended transitions and detectable warning tolerances.
1-1-18	Omitted diagonal slope at turning spaces and upper landings.

MID-BLOCK CURB RAMPS FOR SIDEWALKS

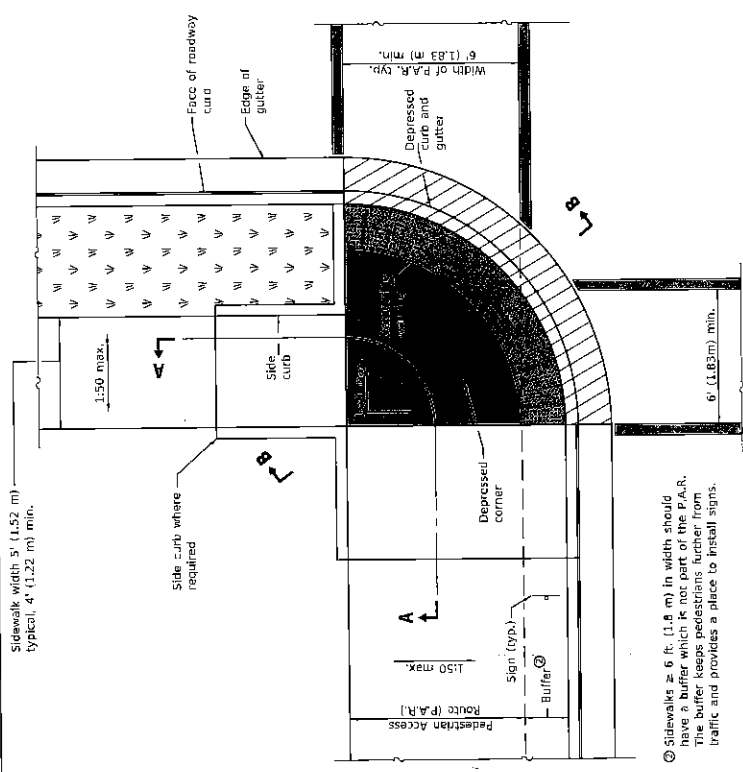
STANDARD 424016-05

ISSUED 1-1-12

Illinois Department of Transportation

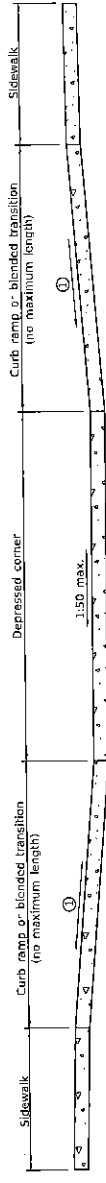
APPROVED: *[Signature]* 2019
 ENGINEER OF POLICY AND PROCEDURES

APPROVED: *[Signature]* 2019
 ENGINEER OF DESIGN AND ENVIRONMENT



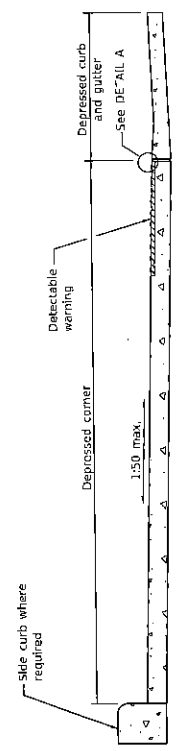
① Sidewalk \geq 6 ft. (1.8 m) in width should have a buffer which is not part of the P.A.R. The buffer keeps pedestrians further from traffic and provides a place to install signs.

DEPRESSION CORNER

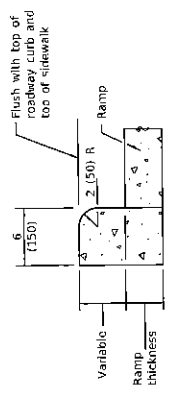


SECTION A-A

① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



SECTION B-B



SIDE CURB DETAIL

DETAIL A

GENERAL NOTES

This standard shall only be used for curb radii of 6 ft. (1.83 m) or greater.
 All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
 Where 1:50 maximum slope is shown, 1:64 is preferred.
 Detectable warnings are shown in their ideal tolerances but the following placement tolerances are allowed.
Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.
Curb Set-Back - Detectable warnings located at the back of curb should extend a distance of 2 in. (50 mm) up to 6 in. (150 mm) behind the curb is allowed.
 See Standard 606001 for details of depressed curb adjacent to curb ramp.

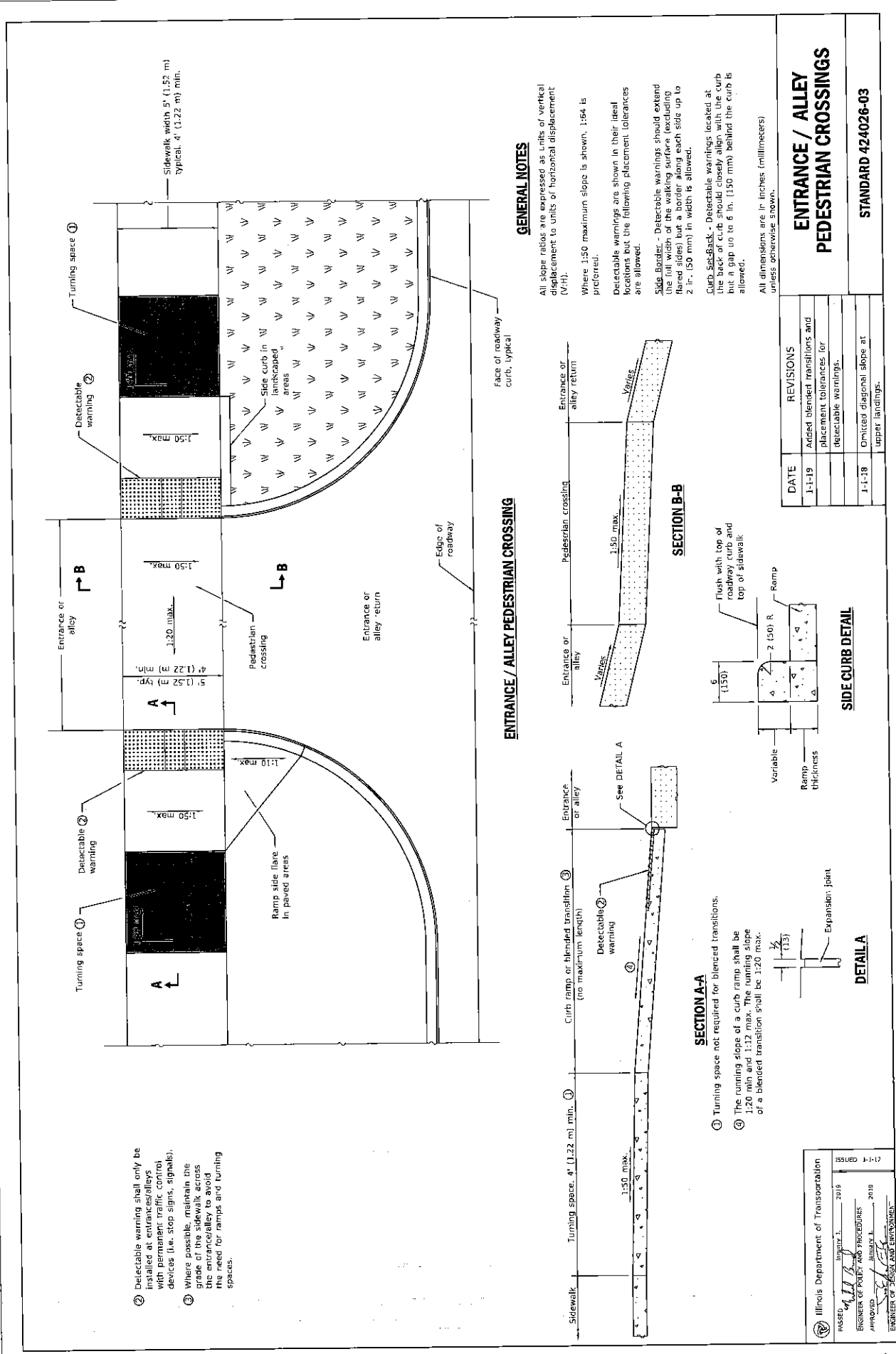
All dimensions are in inches (millimeters) unless otherwise shown.

DEPRESSION CORNER FOR SIDEWALKS

DATE	REVISIONS
1-1-21	Added crosswalk striping and a "buffer" for wide sidewalks.
1-1-19	Removed upper landings, added blended transition and detectable warning tolerances.

STANDARD 424021-06

Illinois Department of Transportation
 ISSUED 1-1-12
 PROJECT NUMBER: 2071
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED: [Signature]
 ENGINEER OF POLICY AND ENVIRONMENT



- ② Detectable warning shall only be installed at entrances/alleys with permanent traffic control devices (i.e. stop signs, signals). Where possible, maintain the grade of the sidewalk to avoid the need for ramps and turning spaces.
- ③

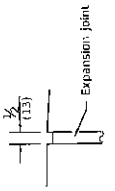
GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
 Where 1:50 maximum slope is shown, 1:64 is preferred.
 Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.
Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.
Curb Set-back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.
 All dimensions are in inches (millimeters) unless otherwise shown.

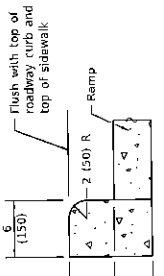
ENTRANCE / ALLEY PEDESTRIAN CROSSING

SECTION A-A

- ① Turning space not required for blended transitions.
- ② The running slope of a curb ramp shall be 1:20 min and 1:12 max. The running slope of a blended transition shall be 1:20 max.



SECTION B-B

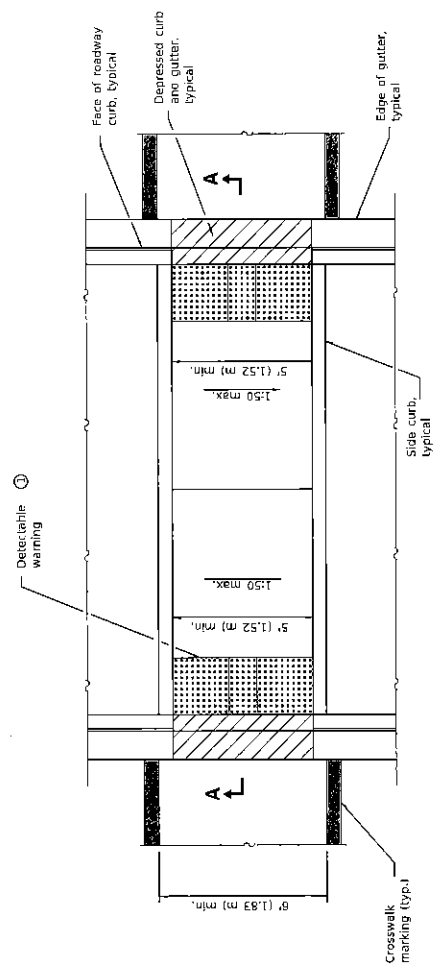


DATE	REVISIONS
1-1-19	Added blended transitions and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at upper landings.

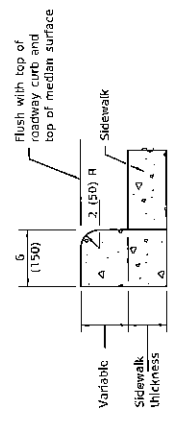
ENTRANCE / ALLEY PEDESTRIAN CROSSINGS

STANDARD 424026-03

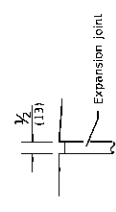
Illinois Department of Transportation
 ISSUED 1-1-17
 2019
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED 1-1-17
 2019
 ENGINEER OF DESIGN SUPERVISOR



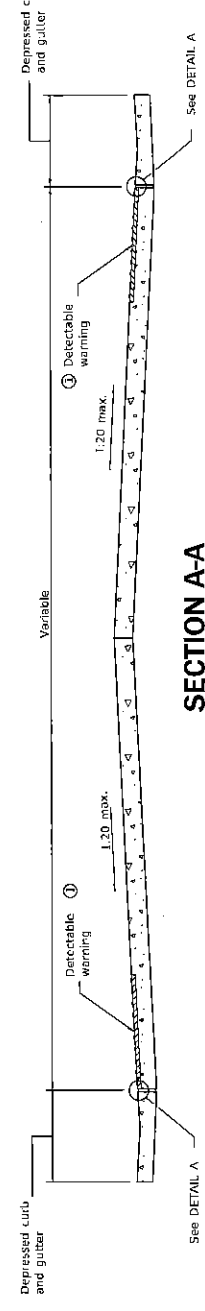
MEDIAN PEDESTRIAN CROSSING



SIDE CURB DETAIL



DETAIL A



SECTION A-A

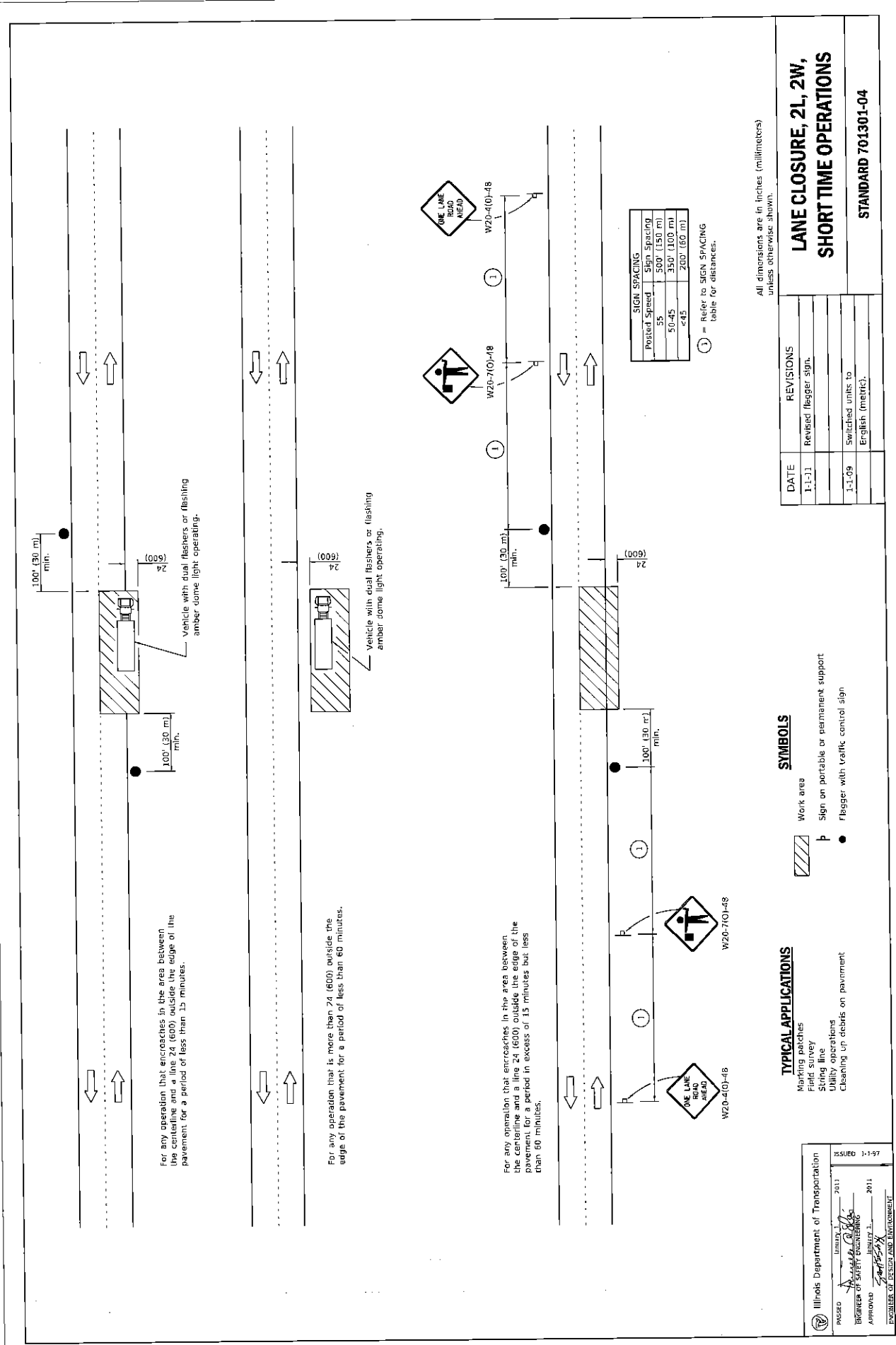
① Omit detectable warnings when distance between back of curbs is less than 6' (1.83 m).

GENERAL NOTES

- All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
- Where 1:50 maximum slope is shown, 1:64 is preferred.
- Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.
- Side Border - Detectable warnings should extend the full width of the walking surface (excluding depressed curb) but a tapering each side up to 2 in. (50 mm) in width is allowed.
- Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.
- See Standard 600001 for details of depressed curb adjacent to curb ramp.
- All dimensions are in inches (millimeters) unless otherwise shown.

MEDIAN PEDESTRIAN CROSSINGS	
DATE	REVISIONS
1-1-19	Added placement tolerances for detectable warnings.
1-1-12	Widened crosswalk to 6' (1.83 m) min. inside dimension.
	Revised General Notes.
STANDARD 424031-02	

Illinois Department of Transportation
 PASSED: [Signature] January 1, 2019
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED: [Signature] January 1, 2019
 ENGINEER OF DESIGN AND ENVIRONMENT



For any operation that encroaches in the area between the centerline and a line 24 (600) outside the edge of the pavement for a period of less than 75 minutes.

For any operation that is more than 74 (600) outside the edge of the pavement for a period of less than 60 minutes.

For any operation that encroaches in the area between the centerline and a line 24 (600) outside the edge of the pavement for a period in excess of 15 minutes but less than 60 minutes.

All dimensions are in inches (millimeters) unless otherwise shown.

LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS

STANDARD 701301-04

DATE	REVISIONS
1-1-11	Revised flagger sign.
1-1-09	Switched units to English (metric).

SYMBOLS

- Work area
- Sign on portable or permanent support
- Flagger with traffic control sign

TYPICAL APPLICATIONS

- Marking patches
- Field survey
- String line
- Utility operations
- Cleaning up debris on pavement

ISSUED 1-1-97

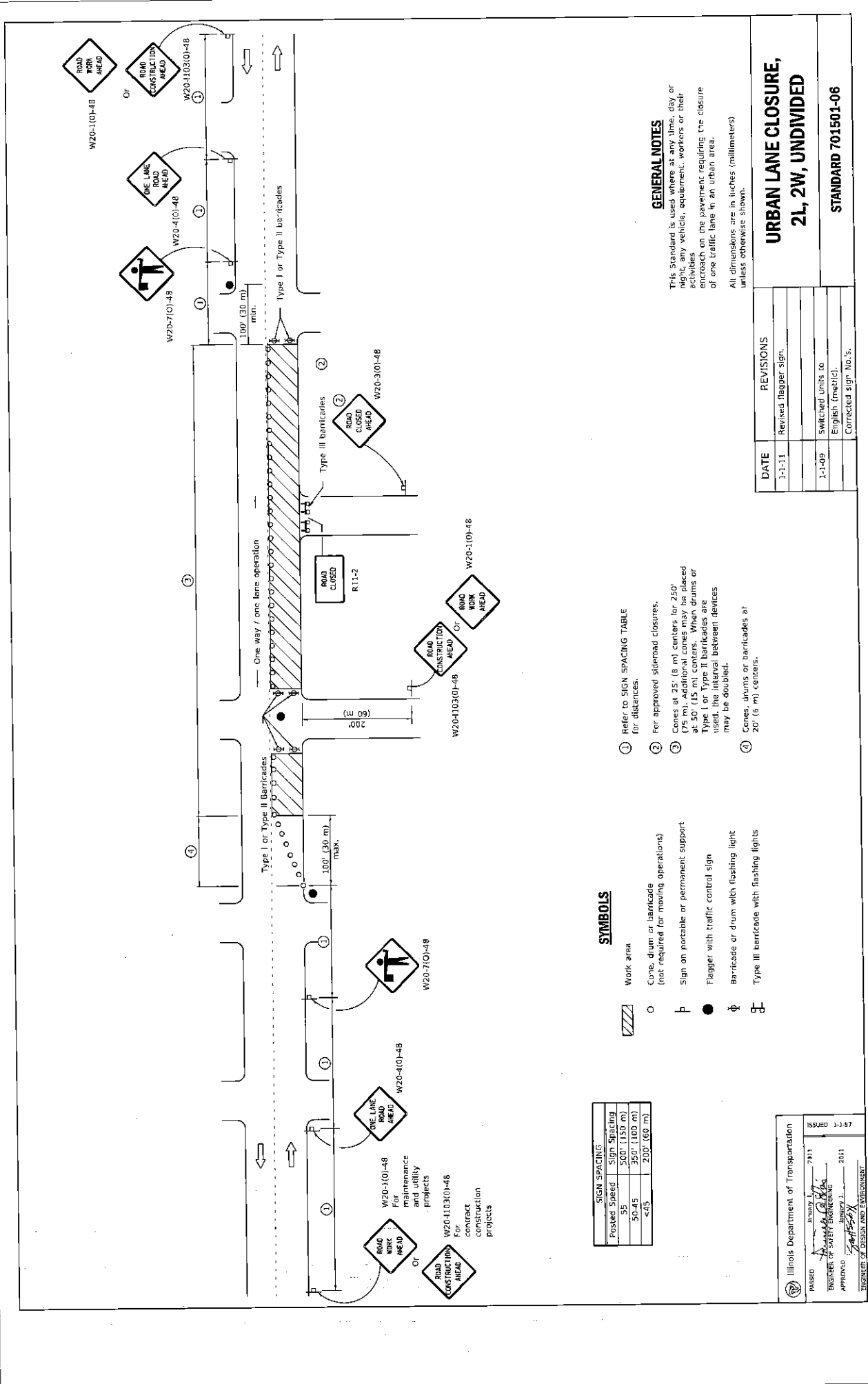
ILLINOIS Department of Transportation

APPROVED *[Signature]* 2011

ENGINEER OF SAFETY

APPROVED *[Signature]* 2011

ENGINEER OF DESIGN AND ENVIRONMENT



GENERAL NOTES
 This Standard is used where at any time, day or night, any vehicle, equipment, workers or their activities encroach on the pavement requiring the closure of one traffic lane in an urban area.
 All dimensions are in inches (millimeters) unless otherwise shown.

- 1 Refer to SIGN SPACING TABLE for distances.
- 2 For approved sidemount closures.
- 3 Cones at 25' (8 m) centers for 250' (75 m). Additional cones may be placed at 50' (15 m) centers. When drums or Type I or Type II barricades are used, the interval between devices may be doubled.
- 4 Cones, drums or barricades at 20' (6 m) centers.

SYMBOLS

- Work area
- Cone, drum or barricade (not required for moving operations)
- Sign on portable or permanent support
- Flagger with traffic control sign
- Barricade or drum with flashing light
- Type III barricade with flashing lights

SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

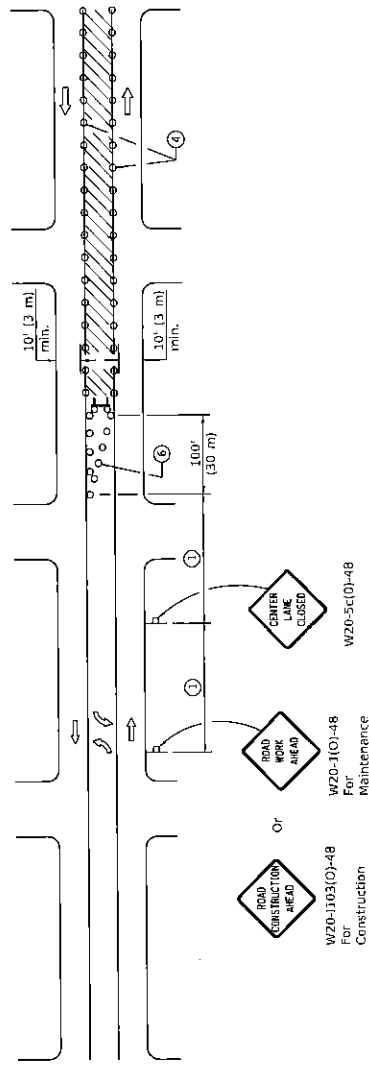
**URBAN LANE CLOSURE,
 2L, 2W, UNDIVIDED**

DATE	REVISIONS
1-1-11	Revised flagger sign.
1-1-09	Switched units to English (metric). Corrected sign No.'s.

STANDARD 701501-06

Illinois Department of Transportation
 PASSED: [Signature] 7/11
 REGISTERED PROFESSIONAL ENGINEER IN SAFETY
 APPROVAL: [Signature] 2/11
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED: 1-1-97



CASE I
(Signs required for both directions)

Posted Speed	Sign Spacing
55	500' (150 m)
30-45	350' (100 m)
<45	200' (60 m)

- 1 Refer to SIGN SPACING TABLE for distances.
- 2 Required for speeds > 40 mph (70 km/h).
- 3 Required if work exceeds 500' (164 m) or 1 block.
- 4 Cones at 25' (8 m) centers for 250' (75 m) on approach. Additional cones may be placed at 50' (15 m) centers. When drums or type I or II barricades are used, the interval between devices may be doubled.
- 5 For approved sternoad closures.
- 6 Cones, drums or barricades at 20' (6 m) centers in taper.
- 7 Use flagger sign only when flagger is present.

SYMBOLS

- Work area
- Barricade or drum with flashing light
- Flagger with traffic control sign
- Cone, drum or barricade
- Sign on portable or permanent support
- Type III barricade with flashing lights

GENERAL NOTES

This Standard is used to close one lane of an urban, two lane, two way roadway with a bidirectional turn lane.
 Case I applies when no workers are present. When workers are present, two lanes shall be closed and traffic control shall be according to Standard 70150L.

Calculate L as follows:

SPEED LIMIT	FORMULAS
40 mph (70 km/h)	English (Metric)
or less:	$L = \frac{WS^2}{60}$ $L = \frac{WS^2}{150}$
45 mph (80 km/h)	$L = (W)(S)$ $L = 0.65(W)(S)$
or greater:	$W =$ Width of offset in feet (meters).
$S =$ Normal posted speed mph (km/h).	

All dimensions are in inches (millimeters) unless otherwise shown.

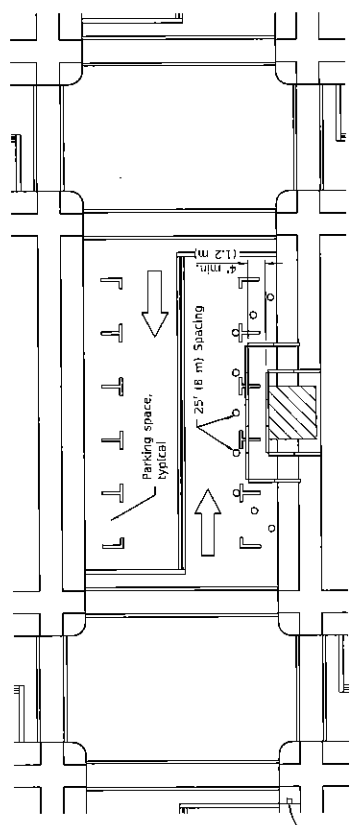
URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE	
DATE	REVISIONS
1-1-19	Revised to allow cones at night.
1-1-18	Corrected sign number for TWO WAY TRAFFIC sign for CASE II.
STANDARD 701502-09 (Sheet 1 of 2)	

Illinois Department of Transportation

APPROVED: *[Signature]* 7918
 ENGINEER OF SAFETY AND ENVIRONMENT

ISSUED: 1-1-01

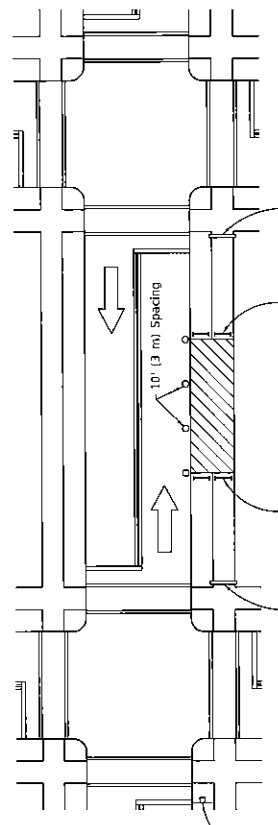
APPROVED: *[Signature]* 2019
 ENGINEER OF SAFETY AND ENVIRONMENT



W20-1103(0)-48 for contract construction projects

Or
W20-1101-48 for maintenance and utility projects

SIDEWALK DIVERSION



W20-1103(0)-48 for contract construction projects

Or
W20-1101-48 for maintenance and utility projects

SIDEWALK CLOSURE

SYMBOLS

- Work area
- Sign on postable or permanent support
- Barricade or drum
- Cone, drum or barricade
- Type III barricade
- Detectable pedestrian channelizing barricade

GENERAL NOTES

- This Standard is used where, at any time, pedestrian traffic must be rerouted due to work being performed.
- This Standard must be used in conjunction with other Traffic Control & Protection Standards when roadway traffic is affected.
- Temporary facilities shall be detectable and accessible.
- The temporary pedestrian facilities shall be provided on the same side of the closed facilities whenever possible.
- The SIDEWALK CLOSED / USE OTHER SIDE sign shall be placed at the nearest crosswalk or intersection to each end of the closure. Where the closure occurs at a corner, the signs shall be erected on the corners across the street from the closure. The SIDEWALK CLOSED signs shall be used at the ends of the actual closures.
- Type III barricades and R11-2-4630 signs shall be positioned as shown. ROAD CLOSED TO ALL TRAFFIC detail on Standard 701901.
- All dimensions are in inches (millimeters) unless otherwise shown.

① Omit whenever duplicated by road work traffic control.

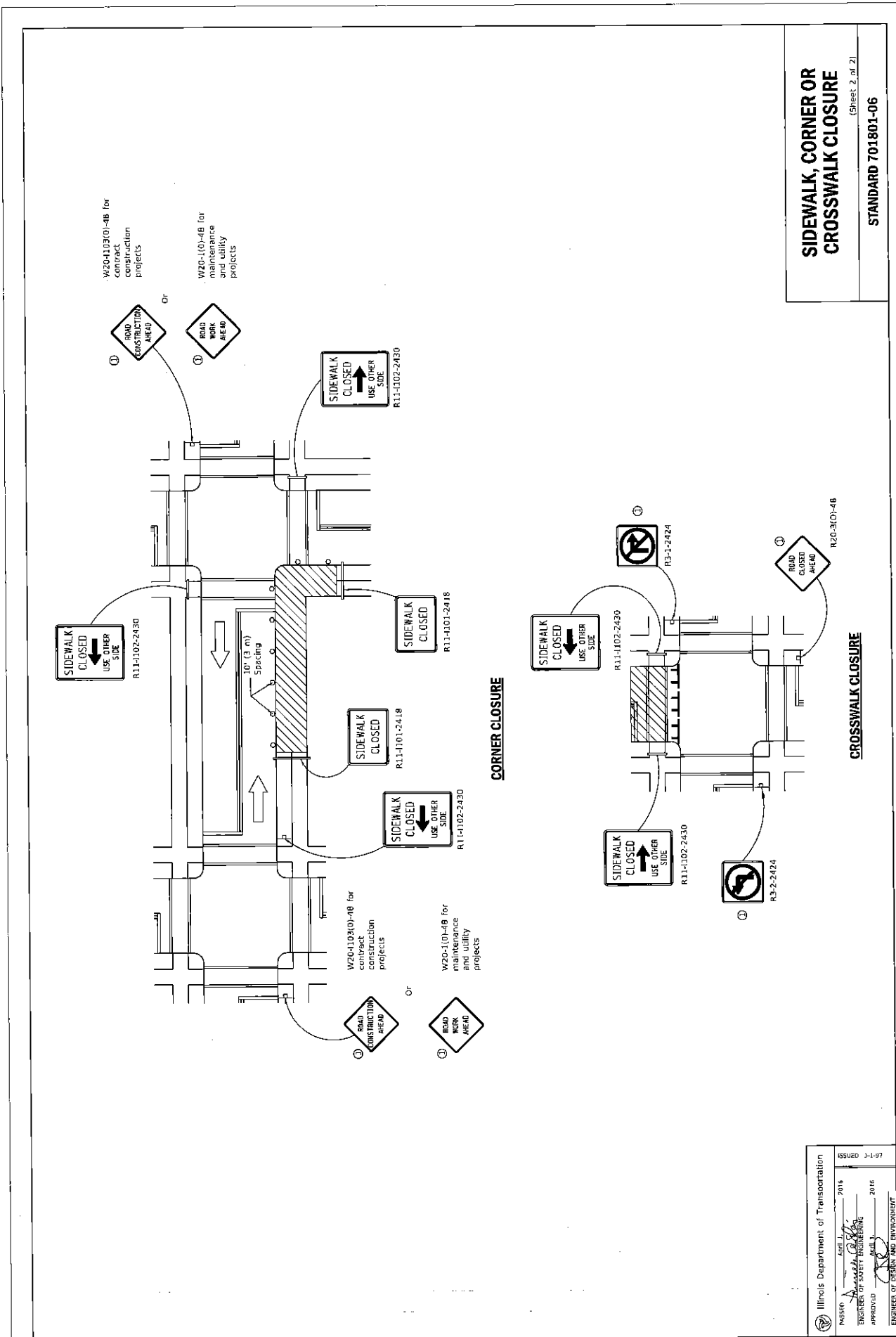
DATE	REVISIONS
4-1-16	Omitted orange safety fence from standard as this is covered in the std. spec.
1-1-12	Added SIDEWALK DIVERSION. Modified appearance of plan Views. Renamed SK.

SIDEWALK, CORNER OR CROSSWALK CLOSURE

(Sheet 1 of 2)

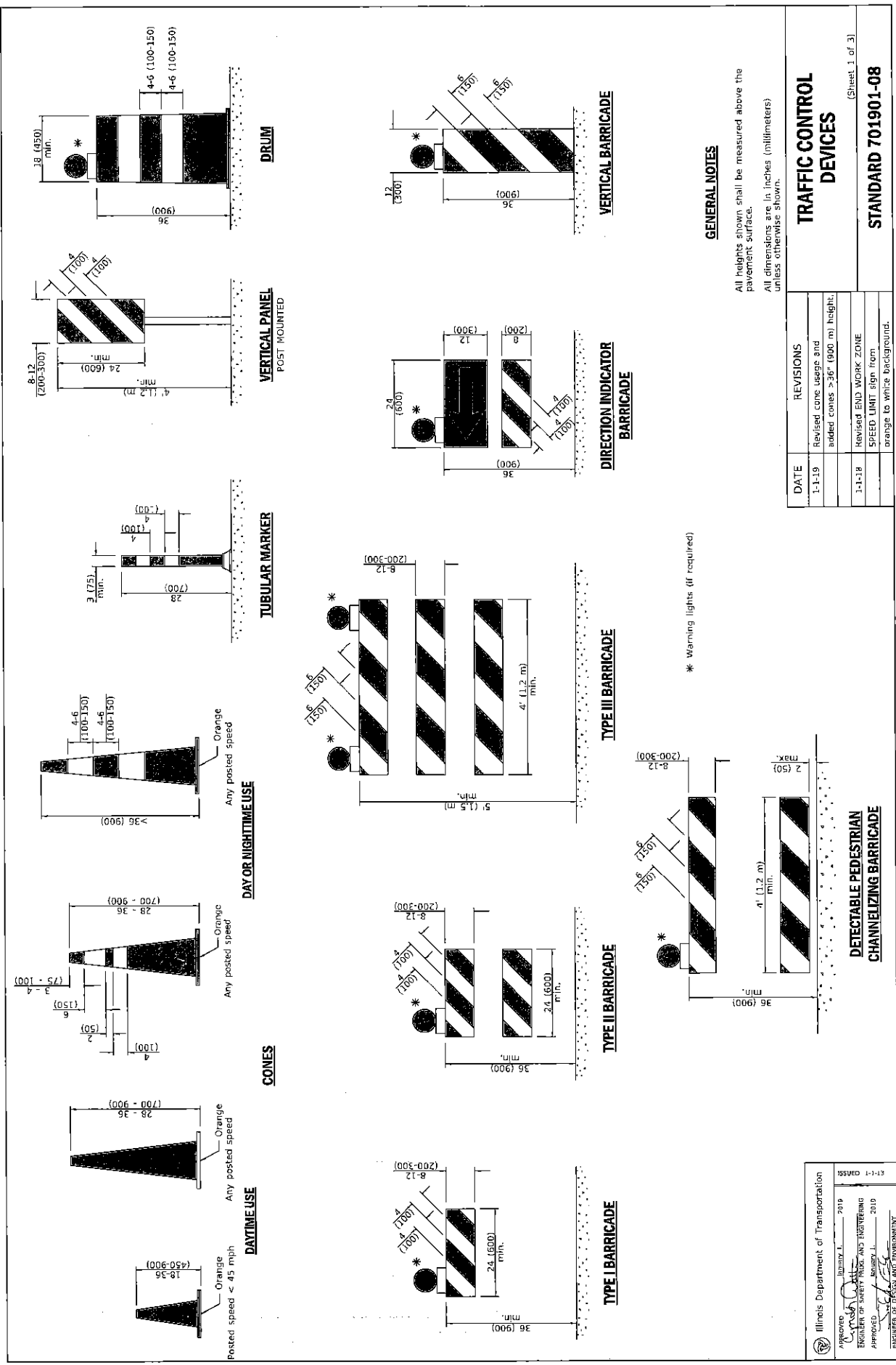
STANDARD 701801-06

Illinois Department of Transportation
 PASSED April 1, 2016
 APPROVED April 1, 2016
 ENGINEER OF SAFETY ENGINEERING
 ENGINEER OF DESIGN AND ENVIRONMENT



SIDEWALK, CORNER OR CROSSWALK CLOSURE
 (Sheet 2 of 2)
STANDARD 701801-06

Illinois Department of Transportation PREPARED BY: <i>[Signature]</i> ENGINEER OF SAFETY ENGINEERING APPROVED BY: <i>[Signature]</i> ENGINEER OF DESIGN AND ENVIRONMENT	ISSUED 1-1-97
	2014
	2015
	2016



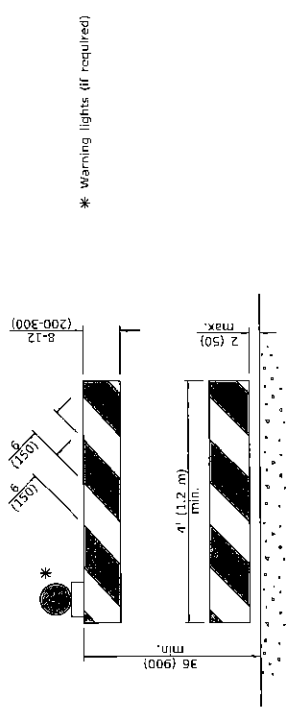
GENERAL NOTES

All heights shown shall be measured above the pavement surface.
All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Revised cone usage and added cones > 36" (900 m) height.
1-1-18	Revised END WORK ZONE SPEED LIMIT sign from orange to white background.

TRAFFIC CONTROL DEVICES	
(Sheet 1 of 3)	
STANDARD 701901-08	

DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE



* Warning lights (if required)

Illinois Department of Transportation
ISSUED 1-1-13

Approved: *[Signature]* January 1, 2019
ENGINEER OF SAFETY PROGRAMS AND ENGINEERING

Approved: *[Signature]* January 1, 2019
ENGINEER OF DESIGN AND ENVIRONMENT

ROAD CONSTRUCTION NEXT X MILES
G20-1104(0)-6036

END CONSTRUCTION
G20-1105(0)-6024

This signing is required for all projects 2 miles (3200 m) or more in length.
ROAD CONSTRUCTION NEXT X MILES sign shall be placed 500' (150 m) in advance of project limits.
END CONSTRUCTION sign shall be erected at the end of the job unless another job is within 2 miles (3200 m).
Dual sign displays shall be utilized on multi-lane highways.

WORK LIMIT SIGNING

WORK ZONE	W21-1415(0)-3618
SPEED LIMIT	R2-1-3648
XX	
PHOTO ENFORCED	R10-1109p-3618 ***
XXXX FINE MINIMUM	R231-106p-3618

Sign assembly as shown on Standards or as allowed by District Operations.

END WORK ZONE SPEED LIMIT
G20-1103-6036

This sign shall be used when the above sign assembly is used.

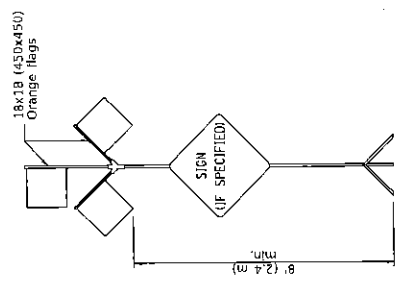
HIGHWAY CONSTRUCTION SPEED ZONE SIGNS

*** R10-1109p shall only be used along roadways under the jurisdiction of the State.

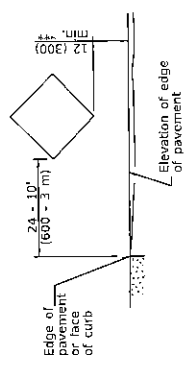
TRAFFIC CONTROL DEVICES

(Sheet 2 of 3)

STANDARD 701901-08

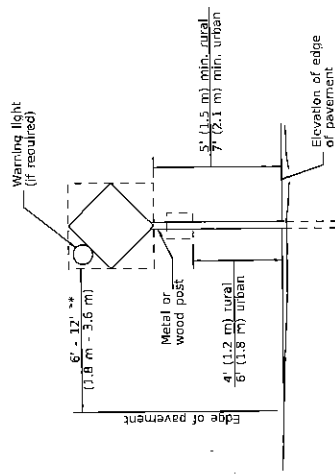


HIGH LEVEL WARNING DEVICE



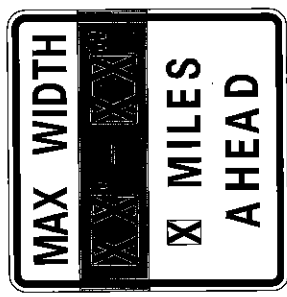
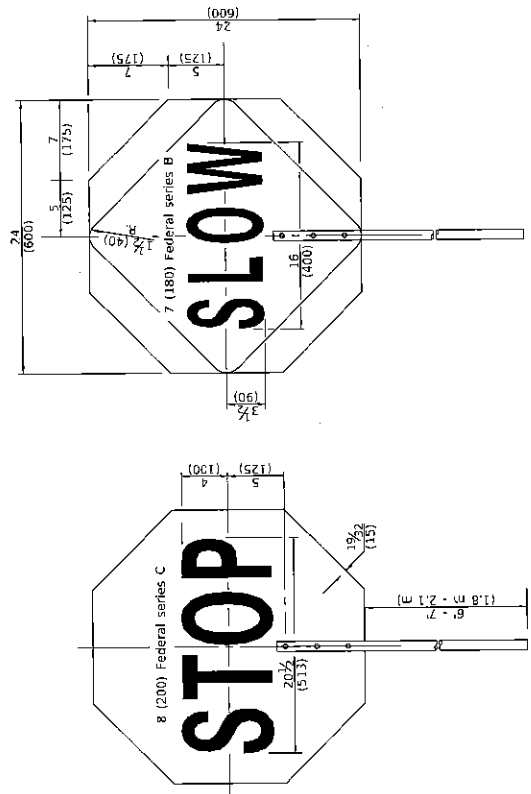
SIGNS ON TEMPORARY SUPPORTS

*** When work operations exceed 5' (1.5 m) min. If located behind other devices, the height shall be sufficient to be seen completely above the devices.



POST MOUNTED SIGNS

*** When curb or paved shoulder are present this dimension shall be 24' (600) to the face of curb or 6' (1.8 m) to the outside edge of the paved shoulder.



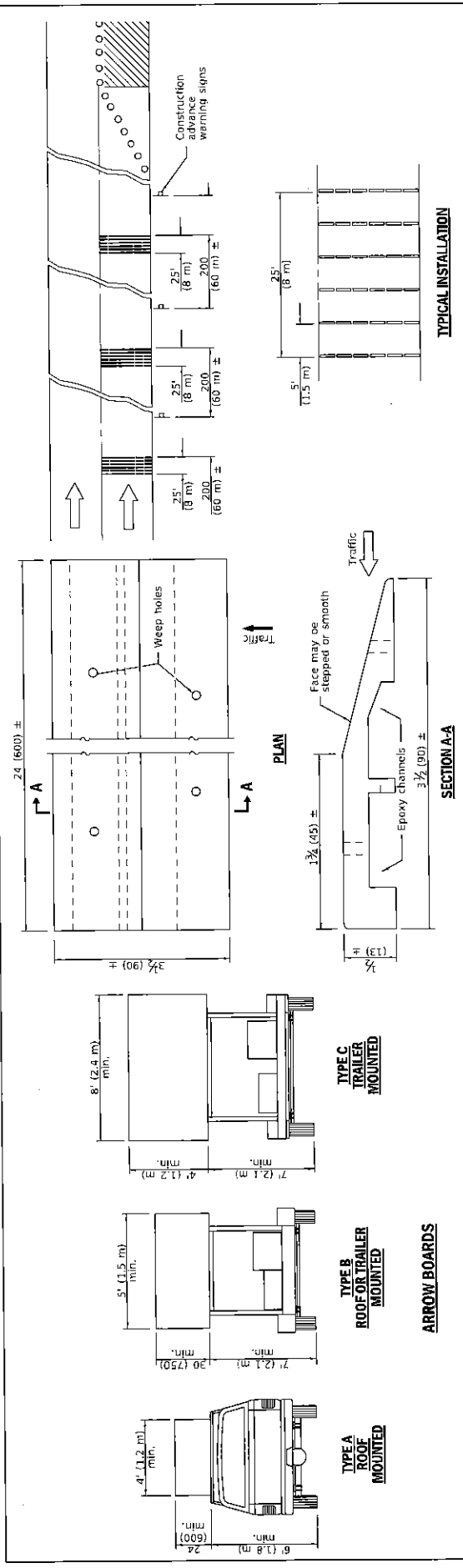
W12-1103-4848

WIDTH RESTRICTION SIGN

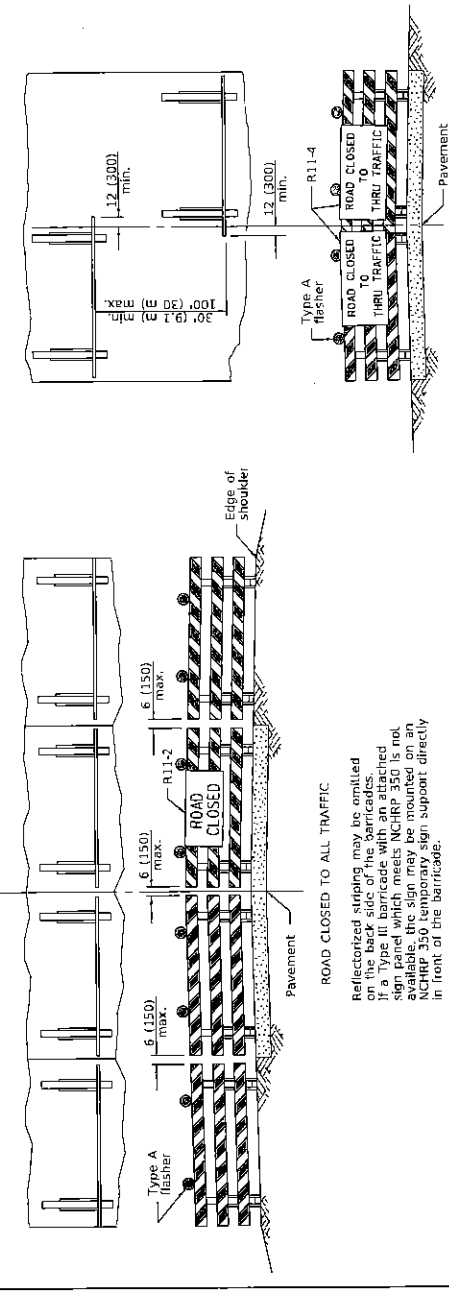
XX-xxx* width and X miles are variable.

FLAGGER TRAFFIC CONTROL SIGN

Illinois Department of Transportation
ISSUED 1-1-13
APPROVED: [Signature]
REGISTERED PROFESSIONAL ENGINEER
PROJECT OF HIGHWAY AND ENVIRONMENT



TEMPORARY RUMBLE STRIPS

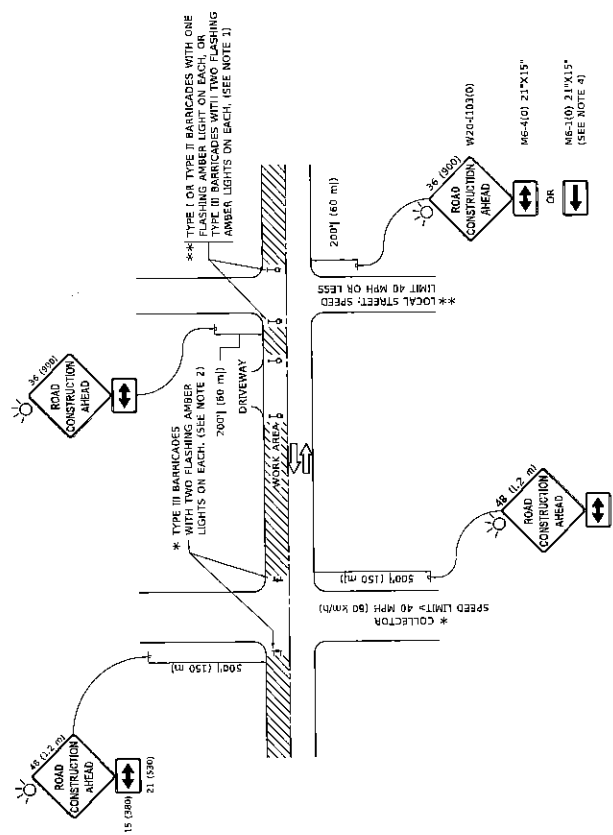


TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD

Illinois Department of Transportation

APPROVED	ISSUED
January 1, 2019	1-1-13
ENGINEER OF SAFETY PROGRAMS AND ENGINEERING	
APPROVED	
ENGINEER OF DESIGN AND ENVIRONMENT	

TRAFFIC CONTROL DEVICES
 STANDARD 701901-08
 (Sheet 3 of 3)



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (910x910) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY TYPE III BARRICADES WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CLOSED SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (12.2 m x 12.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CLOSED SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE HEIGHT.

WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE STOPPING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARDS. THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, DRIVEWAYS AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS ON ITEMS.

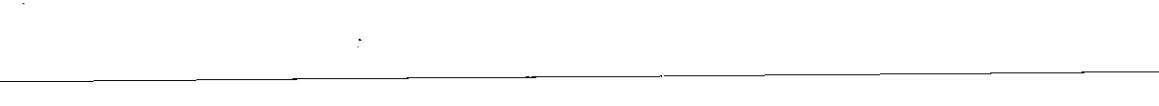
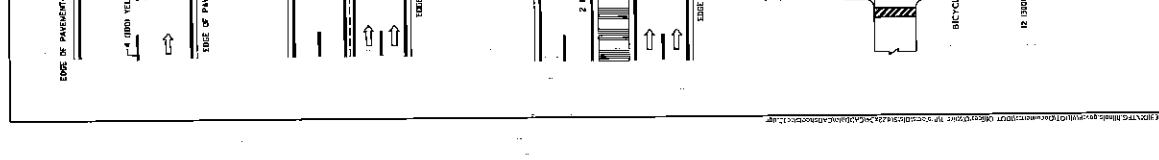
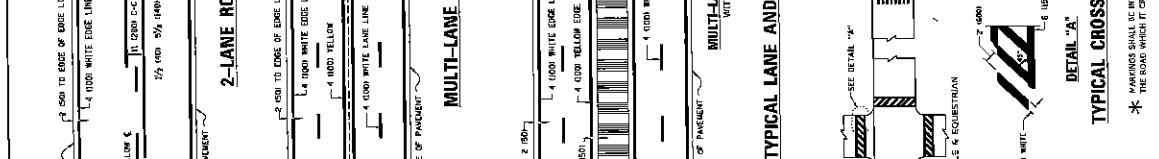
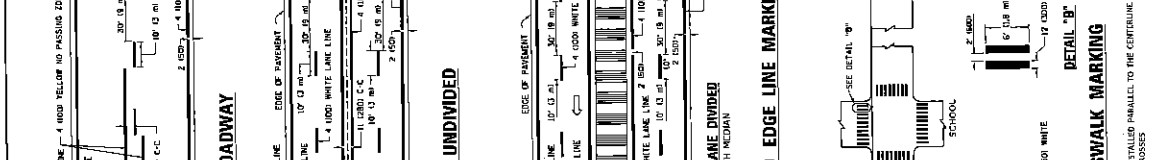
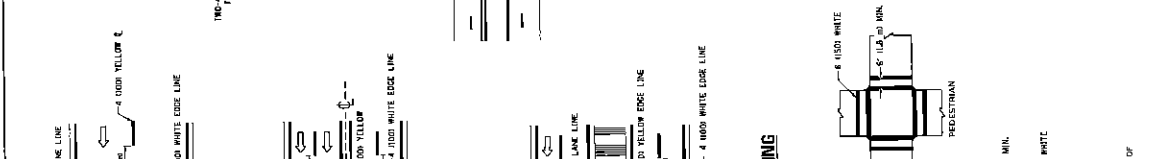
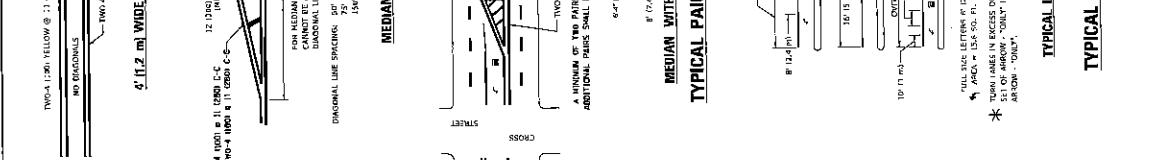
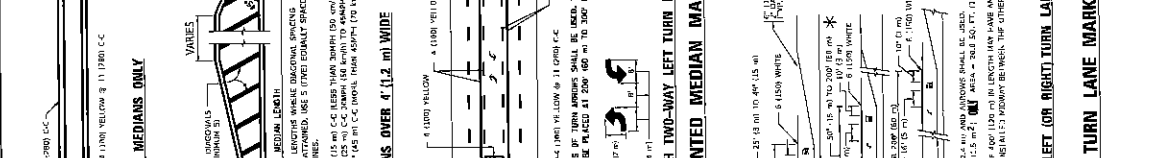
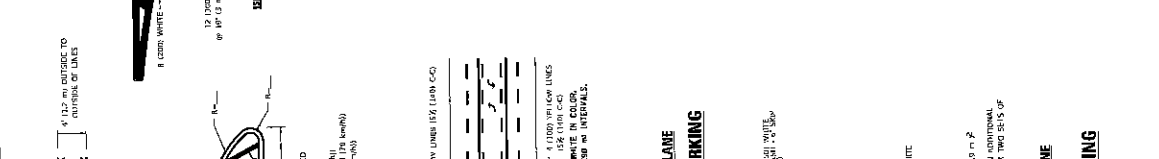
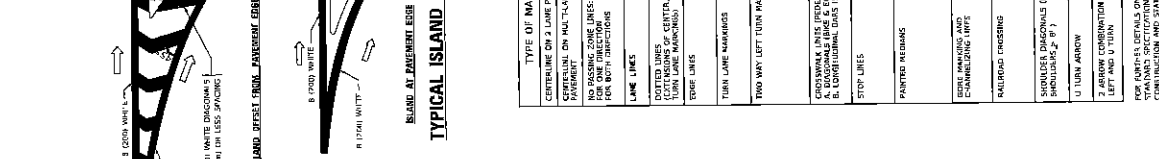
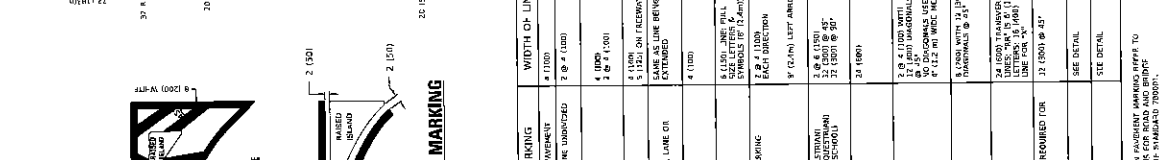
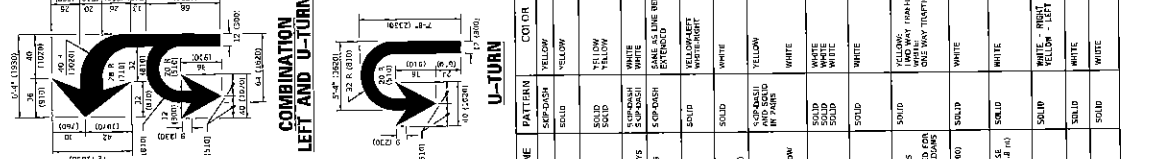
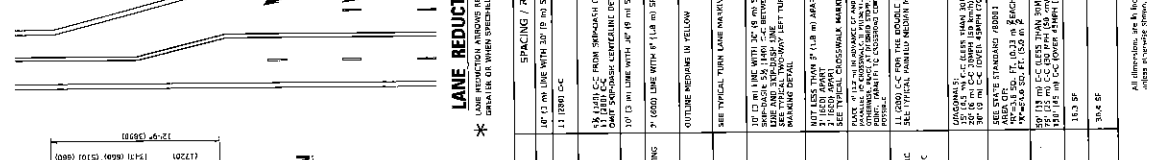
All dimensions are in inches (millimeters) unless otherwise shown.

DESIGNED - A. HOSCHER, ID-1528		SCALE: NONE		TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		TOTAL SHEETS: 106	
DRAWN - T. RANWACHER, D-105-00		SHEET: 1 OF 1		SECTION: TC-40		COUNTY: ILLINOIS	
CHECKED - A. SCHREIBER, P-01-12		DATE: 06-09		SHEET: 1 OF 1		CONTRACT NO.	
REVISED - A. SCHREIBER, 07-15-18				SHEET: 1 OF 1		CONTRACT NO.	
REVISED - A. SCHREIBER, 07-15-18				SHEET: 1 OF 1		CONTRACT NO.	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DESIGNED - A. HOSCHER, ID-1528		SCALE: NONE		TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		TOTAL SHEETS: 106	
DRAWN - T. RANWACHER, D-105-00		SHEET: 1 OF 1		SECTION: TC-40		COUNTY: ILLINOIS	
CHECKED - A. SCHREIBER, P-01-12		DATE: 06-09		SHEET: 1 OF 1		CONTRACT NO.	
REVISED - A. SCHREIBER, 07-15-18				SHEET: 1 OF 1		CONTRACT NO.	
REVISED - A. SCHREIBER, 07-15-18				SHEET: 1 OF 1		CONTRACT NO.	

DEPTH	SPEED LIMIT
3/8"	35
1/2"	40
3/4"	45
1"	50
1 1/4"	55
1 3/4"	60
2"	65
2 1/2"	70



LANE REDUCTION TRANSITION
LANE REDUCTION MARKINGS BEGINNING AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPEEDS ARE PLACED.

U-TURN

COMBINATION LEFT AND U-TURN

TYPICAL ISLAND MARKING

TYPICAL PAINTED MEDIAN MARKING

TYPICAL LEFT OR RIGHT TURN LANE MARKING

TYPICAL TURN LANE MARKING

TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CONTINUOUS OR 2 LANE PAVEMENT	4 (100)	SOLID	YELLOW	10' ON LINE WITH 20' IN SPACE
CONTINUOUS OR 2 LANE UNPAVED PAVEMENT	2 (100)	SOLID	YELLOW	11' 8" ON C-C
NO PASSING OR LANE LINES FOR BOTH DIRECTIONS	4 (100)	SOLID	YELLOW	13' 1/2" ON C-C FROM SHARPCURVE CENTERLINE
LANE LINES	4 (100)	SOLID	WHITE	10' ON LINE WITH 40' IN SPACE
DOTTED LINES (LATERAL LINES OR LANE LANE MARKINGS)	4 (100)	SOLID	WHITE	10' ON LINE WITH 40' IN SPACE
TURN LANE MARKINGS	4 (100)	SOLID	YELLOW/LEFT WHITE/RIGHT	6" ON LINE WITH 10' IN SPACE
TWO WAY LEFT TURN MARKING	4 (100)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
CROSSWALK MARKING (A. CROSSWALK MARKING B. CROSSWALK MARKING C. CROSSWALK MARKING)	4 (100)	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAIL
STOP LINES	24 (100)	SOLID	WHITE	NOT LESS THAN 2' (1.8 m) FROM FACE OF CURB OR SIDEWALK
PAINTED MEDIAN	4 (100)	SOLID	YELLOW	SEE TYPICAL PAINTED MEDIAN MARKING
EDGE MARKING AND CHANNELIZING LINE	4 (100)	SOLID	WHITE	SEE TYPICAL EDGE MARKING DETAIL
RAILROAD CROSSING	4 (100)	SOLID	WHITE	SEE TYPICAL RAILROAD CROSSING MARKING
SURVEY CROSSINGS	4 (100)	SOLID	WHITE	SEE TYPICAL SURVEY CROSSING MARKING
U-TURN ARROW	4 (100)	SOLID	WHITE	SEE TYPICAL U-TURN ARROW MARKING

DISTRICT ONE	TYPICAL PAVEMENT MARKINGS	TO STA.	FROM STA.	SHEET	OF	SHEETS	SCALE	NO. OF	DATE	BY	CHK'D	APP'D	CONTRACT NO.	TOTAL SHEETS

FOR PARTIAL DETAILS ON PAVEMENT MARKINGS REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD DRAWINGS.

All dimensions are in feet (millimeters) unless otherwise shown.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REVISION	DATE	BY	CHK'D	APP'D
REVISION				
REVISION				
REVISION				

MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTRALLINE OF THE ROAD WHICH IT CROSSES.

DESIGNED BY: [Name]
DRAWN BY: [Name]
CHECKED BY: [Name]
DATE: [Date]

STREET	FROM	TO	HMA SURF CSE MIX D (TON)	LEVEL BIND (TON)	TACK COAT (LB)	LONG. JT SEAL (FT)	HMA REM. & REPL. 4" (SY)	HMA REM. & REPL. 6" (SY)	PGE SPECIAL (CY)
HUGHES AVE	ONEIL RD	POWELL ST	99	99	796	393	59.0	24.0	0
BARRETT ST	KLEIN AVE	71ST ST	38	38	308	152	23.0	10.0	0
KLEIN AVE	KIDWELL AVE	POWELL ST	169	169	1361	672	101.0	41.0	0
ONEILL RD	HUGHES AVE	HAWKINS AVE	174	174	1397	690	104.0	42.0	0
BATEMAN ST	POWELL ST	HAWKINS AVE	142	142	1144	565	86.0	34.0	0
HAWKINS AVE	KIDWELL RD	POWELL ST	236	236	1893	935	103.0	57.0	0
KIDWELL RD	KLEIN AVE	HAWKINS AVE	244	244	1960	968	0.0	59.0	0
POWELL ST	71ST ST	HAWKINS ST	277	277	2228	1100	145.0	66.0	0
HATCH PL	CAMDEN RD	CUL DE SAC	93	93	749	370	155.0	23.0	0
HATCH ST	CAMDEN RD	BINDER RD	129	129	1033	510	96.0	31.0	0
BINDER RD	HATCH ST	71ST ST	78	78	628	310	101.0	19.0	0
BINDER RD	GRAHAM	HAWKINS ST	166	166	1337	660	0.0	40.0	0
HAWKINS AVE	CAMDEN RD	BINDER RD	136	136	1094	540	126.0	33.0	0
GRAHAM AVE	DEVEREUX RD	BINDER RD	91	91	729	360	136.0	22.0	0
TICONDEROGA PL	DEVEREUX RD	N. END	117	117	944	466	136.0	28.0	0
TICONDEROGA RD	DEVEREUX RD	HALL ST	188	188	1509	745	136.0	45.0	0
HALL PL	DEVEREUX RD	W END	83	83	668	330	512.0	20.0	0
HALL ST	TICONDEROGA RD	DEVEREUX RD	149	149	1195	590	40.0	36.0	0
SELIG PL	DEVEREUX RD	S END	66	66	527	260	229.0	16.0	0
DEVEREUX RD	TICONDEROGA RD	75TH ST	408	408	3278	1570	261.0	98.0	0
CAMDEN RD	71ST ST	DEVEREUX RD	320	320	2572	1270	66.0	77.0	0
BORMAN PL	DEVEREUX RD	W END	96	96	770	380	182.0	23.0	0
WHITE PL	CAMDEN RD	W END	81	81	648	320	48.0	20.0	0
61ST ST	LYMAN AVE	FAIRMOUNT AVE	508	508	4081	1920	303.0	121.0	0
62ND ST	MAIN ST	BLODGETT AVE	699	699	5618	2675	417.0	167.0	4
STRATFORD LN	MAIN ST	WASHINGTON ST	252	252	2027	965	151.0	61.0	0
WASHINGTON ST	S END	61ST ST	249	249	1999	952	149.0	60.0	0
LYMAN AVE	63RD ST	61ST ST	347	347	2787	1315	207.0	83.0	6
FAIRMOUNT AVE	63RD ST	N END	325	325	2615	1245	194.0	78.0	0
62ND CT	FAIRMOUNT AVE	CUL DE SAC	117	117	944	434	70.0	28.0	0
PARK AVE	63RD ST	62ND ST	191	191	1533	730	114.0	46.0	0
BURLINGTON AVE	MAIN ST	WASHINGTON ST	225	225	1811	690	135.0	54.0	0
MOCHHEL DR	S END	BURLINGTON AVE	466	466	3744	1500	278.0	111.0	0
			6959	6959	55923	26582	4863	1673	10

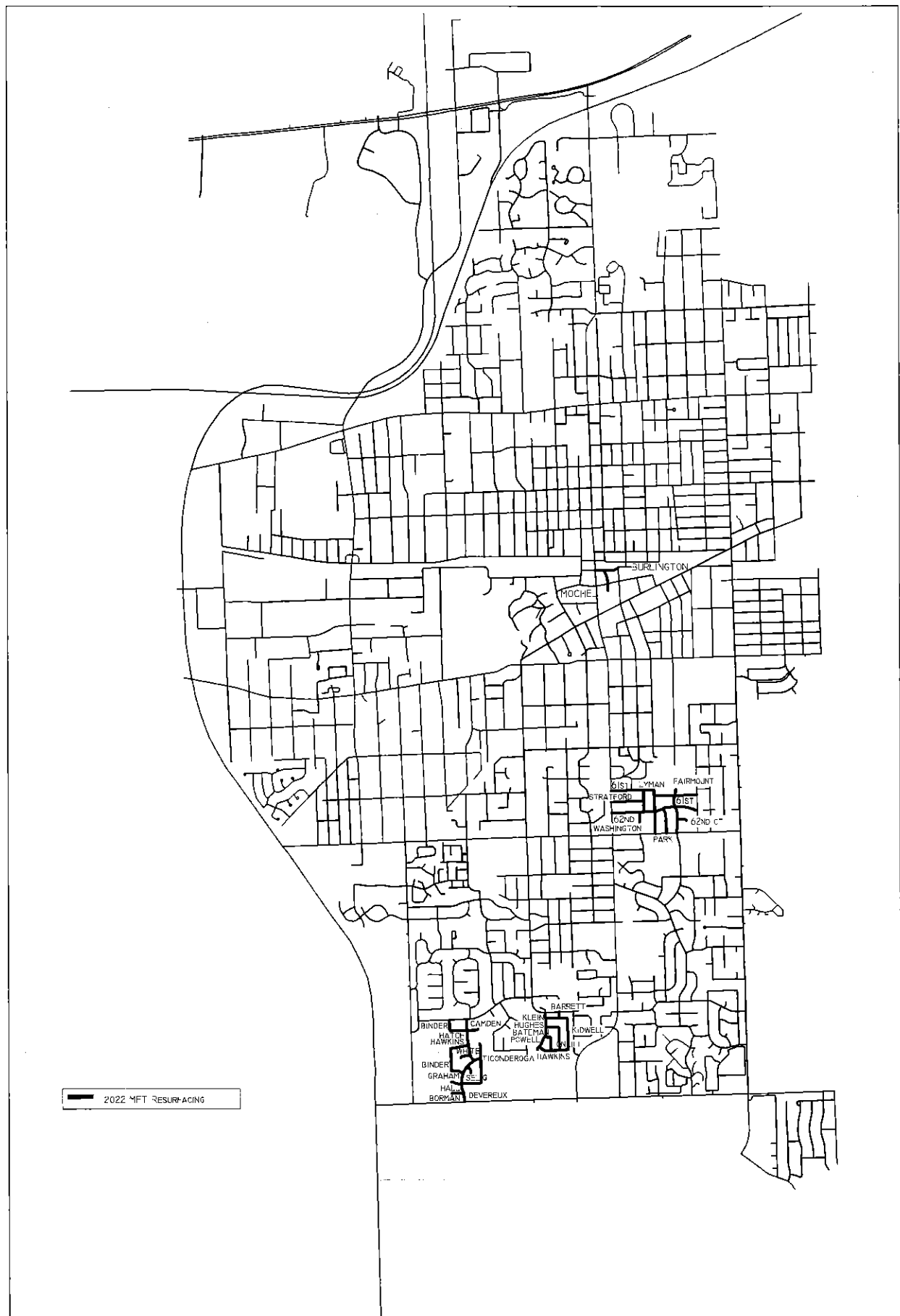
STREET	REM. & DISP. OF		GEOTECH FAB.		EARTH		AGG. BASE		AGG. FOR TEMP		CURB REM.		C & G TY		C & G TY	
	UNSUIT. MATL (CY)	GROUND STAB. (SY)	EXC. (CY)	COURSE, 4" (CY)	AGG. FOR TEMP ACCESS (TON)	(LF)	M-3,12 (LF)	M-4,12 (LF)	M-6,12 (LF)							
HUGHES AVE	0	0	0.0	0.0	0	16	16	0	0							
BARRETT ST	0	0	10.0	0.0	0	110	110	0	0							
KLEIN AVE	0	0	5.0	1.0	0	465	465	0	0							
ONEILL RD	0	0	0.0	0.0	0	305	305	0	0							
BATEMAN ST	0	0	0.0	0.0	0	345	345	0	0							
HAWKINS AVE	0	0	9.5	2.0	0	345	345	0	0							
KIDWELL RD	0	0	0.0	0.0	0	300	300	0	0							
POWELL ST	0	0	11.0	4.0	3	1255	1255	0	0							
HATCH PL	0	0	0.0	0.0	0	268	268	0	0							
HATCH ST	0	0	0.0	0.0	0	277	277	0	0							
BINDER RD	0	0	2.5	1.0	3	246	246	0	0							
BINDER RD	0	0	9.0	0.0	0	230	230	0	0							
HAWKINS AVE	0	0	0.0	0.0	0	160	160	0	0							
GRAHAM AVE	0	0	0.0	0.0	0	230	230	0	0							
TICONDEROGA PL	0	0	0.0	0.0	0	80	80	0	0							
TICONDEROGA RD	0	0	10.0	0.0	0	235	235	0	0							
HALL PL	0	0	0.0	0.0	0	155	155	0	0							
HALL ST	0	0	0.0	0.0	0	390	390	0	0							
SELIG PL	0	0	5.0	0.0	0	185	185	0	0							
DEVEREUX RD	0	0	14.0	4.0	3	810	810	0	0							
CAMDEN RD	0	0	8.5	2.0	3	1205	1205	0	0							
BORMAN PL	0	0	0.0	0.0	0	210	210	0	0							
WHITE PL	0	0	0.0	0.0	0	248	248	0	0							
61ST ST	0	0	4.5	1.0	3	1085	1085	0	1085							
62ND ST	4	150	11.0	2.0	3	1950	697	0	993							
STRATFORD LN	0	0	2.0	0.0	0	680	0	0	680							
WASHINGTON ST	0	0	0.0	0.0	0	428	55	108	0							
LYMAN AVE	6	180	0.0	0.0	0	180	0	0	180							
FAIRMOUNT AVE	0	0	8.0	1.0	0	1170	0	0	820							
62ND CT	0	0	0.0	0.0	0	152	0	0	152							
PARK AVE	0	0	10.0	0.0	0	517	0	0	517							
BURLINGTON AVE	0	0	0.0	0.0	0	0	0	0	0							
MOCHEL DR	0	0	0.0	0.0	0	0	0	0	0							
	10	330	120.0	18	18	14232	8822	108	4427							

STREET	C & G TY		TY B CURB,		M.H. ADJ.		M.H. ADJ.		FR & LID		FR & LID		FR & GRATE		M.H.		IN. ADJ.		IN.		VALVE	
	B-6.12	(LF)	SPEC.	(FT)	(EA)	(EA)	SPECIAL	(EA)	TYPE 1 OL	(EA)	TYPE 1 CL	(EA)	TYPE 3	(EA)	RECON	(EA)	(EA)	RECON	(EA)	BOX	ADJ.	
HUGHES AVE	0		0		0		0		0		0		0		0		0		0		0	
BARRETT ST	0		40		0		0		0		0		0		0		0		0		0	
KLEIN AVE	0		0		0		0		0		0		0		3		0		0		0	
ONEILL RD	0		0		1		0		0		0		0		2		0		0		0	
BATEMAN ST	0		0		0		0		0		0		0		0		0		0		0	
HAWKINS AVE	0		40		1		0		0		1		0		5		0		1		0	
KIDWELL RD	0		0		0		0		0		0		0		1		0		0		0	
POWELL ST	0		0		1		0		1		0		0		10		0		1		0	
HATCH PL	0		0		0		0		0		0		0		0		0		0		0	
HATCH ST	0		0		0		0		0		0		0		2		0		0		0	
BINDER RD	0		0		0		0		0		0		0		0		0		0		0	
BINDER RD	0		0		0		0		0		0		0		7		0		0		0	
HAWKINS AVE	0		0		0		0		0		0		0		7		0		0		0	
GRAHAM AVE	0		0		0		0		0		0		0		1		0		0		0	
TICONDEROGA PL	0		0		0		0		0		0		0		1		0		0		0	
TICONDEROGA RD	0		0		0		0		0		0		0		5		0		0		0	
HALL PL	0		0		0		0		0		0		0		4		0		0		0	
HALL ST	0		0		0		0		0		0		0		2		0		0		0	
SELIG PL	0		0		0		0		0		0		0		0		0		0		0	
DEVEREUX RD	0		40		1		0		1		1		1		8		0		1		0	
CAMDEN RD	0		40		0		0		1		0		1		15		0		1		0	
BORMAN PL	0		0		0		0		0		0		0		2		0		0		0	
WHITE PL	0		0		0		0		0		0		0		0		0		0		0	
61ST ST	0		0		1		0		1		0		0		5		0		0		0	
62ND ST	260		0		4		2		1		0		1		16		0		1		3	
STRATFORD LN	0		0		0		0		0		0		0		3		0		0		0	
WASHINGTON ST	265		80		0		0		0		0		0		0		0		0		0	
LYMAN AVE	0		0		0		0		0		0		0		0		0		0		0	
FAIRMOUNT AVE	350		120		3		9		0		1		0		9		1		1		1	
62ND CT	0		0		0		1		0		0		0		2		0		0		0	
PARK AVE	0		0		0		0		0		0		0		1		0		0		0	
BURLINGTON AVE	0		0		0		5		0		1		0		0		0		0		0	
MICHEL DR	0		0		0		12		0		1		0		0		0		0		0	
	875		360		12		29		5		5		3		111		1		6		5	

STREET	INLET FILTERS (EA)	INLET FILTERS CLEANING (EA)	HMA SURF. REM. 3" (SY)	HMA SURF. REM. BT JT (SY)	SIDEWALK REMOVE (SF)	SIDEWALK 5" (SF)	SIDEWALK 6" (SF)	DETECTABLE WARNINGS (SF)	DÉCOR PAVER DRIVE (SY)	PKWY REST (SY)
HUGHES AVE	2	2	1179	0	250	250	0	0	0	90
BARRETT ST	2	2	456	18	0	0	0	0	0	18
KLEIN AVE	4	4	2016	0	807	907	0	40	0	433
ONEILL RD	4	4	2070	0	0	0	0	0	0	96
BATEMAN ST	2	2	1695	0	0	0	0	0	0	133
HAWKINS AVE	2	2	2805	0	1526	1726	0	80	0	542
KIDWELL RD	6	6	2904	0	0	0	0	0	0	159
POWELL ST	12	12	3300	18	2585	2932	51	150	0	1165
HATCH PL	0	0	1110	0	88	88	0	0	0	131
HATCH ST	6	6	1530	0	0	0	0	0	0	107
BINDER RD	5	5	930	18	780	780	0	50	0	280
BINDER RD	0	0	1980	0	315	315	0	0	0	190
HAWKINS AVE	4	4	1620	0	0	0	0	0	0	73
GRAHAM AVE	2	2	1080	0	0	0	0	0	0	84
TICONDEROGA PL	0	0	1398	0	0	0	0	0	0	28
TICONDEROGA RD	8	8	2235	0	220	220	0	0	0	117
HALL PL	0	0	990	0	0	0	0	0	0	87
HALL ST	8	8	1770	0	0	0	0	0	0	189
SELIG PL	0	0	780	0	0	0	0	0	5	92
DEVEREUX RD	11	11	4857	22	4352	4267	85	300	0	1416
CAMDEN RD	5	5	3810	18	2980	2922	59	160	0	1131
BORMAN PL	4	4	1140	0	0	0	0	0	0	81
WHITE PL	2	2	960	0	0	0	0	0	0	134
61ST ST	10	10	6046	76	1326	80	26	100	10	759
62ND ST	19	19	8322	38	2975	3083	57	200	5	1262
STRATFORD LN	7	7	3002	19	725	725	0	40	0	360
WASHINGTON ST	0	0	2962	0	0	0	0	0	0	89
LYMAN AVE	0	0	4129	0	0	0	0	0	0	60
FAIRMOUNT AVE	2	2	3873	57	900	960	0	60	0	560
62ND CT	2	2	1398	0	0	0	0	0	0	24
PARK AVE	1	1	2271	19	295	295	0	20	5	251
BURLINGTON AVE	9	9	2683	38	0	0	0	0	0	0
MOCHEL DR	29	29	5546	74	0	0	0	0	0	0
	168	168	82848	415	20124.0	19550.0	278.0	1200.0	25.0	10142


STREET	GROWTH-INH.		SUPPLEMENT WATER (UNIT)	ROOT PRUNE (EA)	HMA DRIVE REMOVE (SY)		HMA DRIVE 3", (SY)		PCC DRIVE REMOVE (SY)		PCC DRIVE 6", (SY)		SHORT TERM PAVE MARK (LF)		SHORT TERM MARK REM (SF)		PAVT. MARK LINE 4" (LF)
	ER CONT BLKT (SY)																
HUGHES AVE	45.2		0	0	0.0	0.0	0.0	0.0	31.0	31.0	31.0	31.0	0	0	0	0	0
BARRETT ST	8.8		0	0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0	0
KLEIN AVE	216.7		0	1	0.0	0.0	0.0	0.0	250.0	250.0	250.0	250.0	0	0	0	0	0
ONEILL RD	47.9		0	0	0.0	0.0	0.0	0.0	93.0	93.0	93.0	93.0	0	0	0	0	0
BATEMAN ST	66.7		0	0	0.0	0.0	0.0	0.0	155.0	155.0	155.0	155.0	0	0	0	0	0
HAWKINS AVE	271.1		1	2	0.0	0.0	0.0	0.0	93.0	93.0	93.0	93.0	0	0	0	0	0
KIDWELL RD	79.5		0	0	0.0	0.0	0.0	0.0	220.0	220.0	220.0	220.0	0	0	0	0	0
POWELL ST	582.7		2	6	0.0	0.0	0.0	0.0	434.0	434.0	434.0	434.0	0	0	0	0	0
HATCH PL	65.4		1	0	0.0	0.0	0.0	0.0	124.0	124.0	124.0	124.0	0	0	0	0	0
HATCH ST	53.5		0	0	0.0	0.0	0.0	0.0	124.0	124.0	124.0	124.0	0	0	0	0	0
BINDER RD	140.0		0	0	0.0	0.0	0.0	0.0	31.0	31.0	31.0	31.0	0	0	0	0	0
BINDER RD	95.1		0	0	0.0	0.0	0.0	0.0	124.0	124.0	124.0	124.0	0	0	0	0	0
HAWKINS AVE	36.3		0	0	0.0	0.0	0.0	0.0	93.0	93.0	93.0	93.0	0	0	0	0	0
GRAHAM AVE	41.9		0	0	0.0	0.0	0.0	0.0	93.0	93.0	93.0	93.0	0	0	0	0	0
TICONDEROGA PL	14.2		0	0	0.0	0.0	0.0	0.0	31.0	31.0	31.0	31.0	0	0	0	0	0
TICONDEROGA RD	58.4		0	0	0.0	0.0	0.0	0.0	31.0	31.0	31.0	31.0	0	0	0	0	0
HALL PL	43.7		0	0	0.0	0.0	0.0	0.0	124.0	124.0	124.0	124.0	0	0	0	0	0
HALL ST	94.3		0	0	0.0	0.0	0.0	0.0	250.0	250.0	250.0	250.0	0	0	0	0	0
SELIG PL	46.1		2	0	0.0	0.0	0.0	0.0	124.0	124.0	124.0	124.0	0	0	0	0	0
DEVEREUX RD	708.0		0	0	0.0	0.0	0.0	0.0	62.0	62.0	62.0	62.0	0	0	0	0	0
CAMDEN RD	565.3		2	0	0.0	0.0	0.0	0.0	155.0	155.0	155.0	155.0	0	0	0	0	0
BORMAN PL	40.3		0	0	0.0	0.0	0.0	0.0	93.0	93.0	93.0	93.0	0	0	0	0	0
WHITE PL	66.8		1	0	0.0	0.0	0.0	0.0	186.0	186.0	186.0	186.0	0	0	0	0	0
61ST ST	379.7		0	1	0.0	0.0	0.0	0.0	403.0	403.0	403.0	403.0	0	0	0	0	0
62ND ST	631.2		1	2	250.0	250.0	250.0	250.0	155.0	155.0	155.0	155.0	0	0	0	0	0
STRATFORD LN	179.8		0	0	186.0	186.0	186.0	186.0	62.0	62.0	62.0	62.0	0	0	0	0	0
WASHINGTON ST	44.7		0	0	93.0	93.0	93.0	93.0	31.0	31.0	31.0	31.0	0	0	0	0	0
LYMAN AVE	30.1		0	0	0.0	0.0	0.0	0.0	62.0	62.0	62.0	62.0	0	0	0	0	0
FAIRMOUNT AVE	279.8		0	1	31.0	31.0	31.0	31.0	220.0	220.0	220.0	220.0	0	0	0	0	0
62ND CT	12.2		0	0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0	0
PARK AVE	125.6		0	0	93.0	93.0	93.0	93.0	155.0	155.0	155.0	155.0	0	0	0	0	0
BURLINGTON AVE	0		0	0	0	0	0	0	0	0	0	0	4380	1460	1350	1350	1350
MOCHER DR	0		0	0	0	0	0	0	0	0	0	0	7020	2340	2340	2340	2340
	5071		10	13	653	653	653	653	4009	4009	4009	4009	11400	3800	3690	3690	3690

STREET	TRAFFIC CONTROL 801 (L-S)
HUGHES AVE	
BARRETT ST	
KLEIN AVE	
ONEILL RD	
BATEMAN ST	
HAWKINS AVE	
KIDWELL RD	
POWELL ST	
HATCH PL	
HATCH ST	
BINDER RD	
BINDER RD	
HAWKINS AVE	
GRAHAM AVE	
TICONDEROGA PL	
TICONDEROGA RD	
HALL PL	
HALL ST	
SELIG PL	
DEVEREUX RD	
CAMDEN RD	
BORMAN PL	
WHITE PL	
61ST ST	
62ND ST	
STRATFORD LN	
WASHINGTON ST	
LYMAN AVE	
FAIRMOUNT AVE	
62ND CT	
PARK AVE	
BURLINGTON AVE	
MOCHEL DR	
	1.00



2022 MFT MAP- 5.04 MILES

2022 ST-004B RESURFACING PROJECT

SCALE	 NORTH
NTS	
DATE: XX/XX/XX	
DRWN BY: AAA	
CHKD BY: AAA	
DRAWING NO.	
EXHIBIT 1	

2022 Village of Downers Grove Road Resurfacing (B): Pavement Profile Information Sheet

2022 ROAD RESURFACING CONTRACT B (ST-004B)			
STREET	FROM	TO	CRACK CONTROL FABRIC FOUND IN CORE
HUGHES AVE	ONEIL RD	POWELL ST	NO
BARRETT ST	KLEIN AVE	71ST ST	NO
KLEIN AVE	KIDWELL AVE	POWELL ST	NO
ONEILL RD	HUGHES AVE	HAWKINS AVE	NO
BATEMAN ST	POWELL ST	HAWKINS AVE	NO
HAWKINS AVE	KIDWELL RD	POWELL ST	NO
KIDWELL RD	KLEIN AVE	HAWKINS AVE	NO
POWELL ST	71ST ST	HAWKINS ST	NO
HATCH PL	CAMDEN RD	CUL DE SAC	INFORMATION UNKNOWN
HATCH ST	CAMDEN RD	BINDER RD	NO
BINDER RD	HATCH ST	71ST ST	INFORMATION UNKNOWN
BINDER RD	GRAHAM	HAWKINS ST	NO
HAWKINS AVE	CAMDEN RD	BINDER RD	NO
GRAHAM AVE	DEVEREUX RD	BINDER RD	INFORMATION UNKNOWN
TICONDEROGA PL	DEVEREUX RD	N. END	INFORMATION UNKNOWN
TICONDEROGA RD	DEVEREUX RD	HALL ST	YES
HALL PL	DEVEREUX RD	W END	INFORMATION UNKNOWN
HALL ST	TICONDEROGA RD	DEVEREUX RD	INFORMATION UNKNOWN
SELIG PL	DEVEREUX RD	S END	INFORMATION UNKNOWN
DEVEREUX RD	TICONDEROGA RD	75TH ST	NO
CAMDEN RD	71ST ST	DEVEREUX RD	YES
BORMAN PL	DEVEREUX RD	W END	YES
WHITE PL	CAMDEN RD	W END	INFORMATION UNKNOWN
61ST ST	LYMAN AVE	FAIRMOUNT AVE	YES
62ND ST	MAIN ST	BLODGETT AVE	YES
STRATFORD LN	MAIN ST	WASHINGTON ST	YES
WASHINGTON ST	S END	61ST ST	YES
LYMAN AVE	63RD ST	61ST ST	YES
FAIRMOUNT AVE	63RD ST	N END	INFORMATION UNKNOWN
62ND CT	FAIRMOUNT AVE	CUL DE SAC	INFORMATION UNKNOWN
PARK AVE	63RD ST	62ND ST	INFORMATION UNKNOWN



Apprenticeship and Training Program Certification



Local Public Agency	County	Street Name/Road Name	Section Number
Village of Downers Grove	DuPage	Various	22-00000-01-GM

All contractors are required to complete the following certification

For this contract proposal or for all bidding groups in this deliver and install proposal.

For the following deliver and install bidding groups in this material proposal.

Illinois Department of Transportation policy, adopted in accordance with the provisions of the Illinois Highway Code, requires this contract to be awarded to the lowest responsive and responsible bidder. The award decision is subject to approval by the Department. In addition to all other responsibility factors, this contract or deliver and install proposal requires all bidders and all bidder's subcontractors to disclose participation in apprenticeship or training programs that are (1) approved by and registered with the United States Department of Labor's Bureau of Apprenticeship and Training, and (2) applicable to the work of the above indicated proposals or groups. Therefore, all bidders are required to complete the following certification:

1. Except as provided in paragraph 4 below, the undersigned bidder certifies that it is a participant, either as an individual or as part of a group program, in an approved apprenticeship or training program applicable to each type of work or craft that the bidder will perform with its own employees.

2. The undersigned bidder further certifies, for work to be performed by subcontract, that each of its subcontractors either (A) is, at the time of such bid, participating in an approved, applicable apprenticeship or training program; or (B) will, prior to commencement of performance of work pursuant to this contract, establish participation in an approved apprenticeship or training program applicable to the work of the subcontract.

3. The undersigned bidder, by inclusion in the list in the space below, certifies the official name of each program sponsor holding the Certificate of Registration for all of the types of work or crafts in which the bidder is a participant and that will be performed with the bidder's employees. Types of work or craft that will be subcontracted shall be included and listed as subcontract work. The list shall also indicate any type of work or craft job category for which there is no applicable apprenticeship or training program available.

Striping & Traffic Control: Teamsters 673; Curb & Gutter, Flatwork: Cement
 Masons 11, 502; Laborers 76; Sewer: Laborers 76, Operating Engineers 150,
 Teamsters 673; Electrical: Electricians 9, 196, 701; Landscaping: Operating
 Engineers 150; Crack Control: National Allied Workers 831; Guardian/Fence: Ironworkers 63

4. Except for any work identified above, if any bidder or subcontractor shall perform all or part of the work of the contract or deliver and install proposal solely by individual owners, partners or members and not by employees to whom the payment of prevailing rates of wages would be required, check the following box, and identify the owner/operator workforces and positions of ownership.

The requirements of this certification and disclosure are a material part of the contract, and the contractor shall require this certification provision to be included in all approved subcontracts. The bidder is responsible for making a complete report and shall make certain that each type of work or craft job category that will be utilized on the project is accounted for and listed. The Department at any time before or afterward may require the production of a copy of each applicable Certificate of Registration issued by the United States Department of Labor evidencing such participation by the contractor and any of its subcontractors. In order to fulfill the participation requirement, it shall not be necessary that any applicable program sponsor be currently taking or that it will take applications for apprenticeship, training or employment during the performance of the work of this contract or deliver and install proposal.

Bidder	Signature	Date
K-Five Construction Corporation		2/16/2022
Title		
President		
Address	City	State Zip Code
999 Oakmont Plaza Drive, Suite 200	Westmont	IL 60559



Illinois Department of Transportation

Affidavit of Illinois Business Office



Local Public Agency	County	Street Name/Road Name	Section Number
Village of Downers Grove	DuPage	Various	22-00000-01-GM

I, Robert G. Krug of Westmont, Illinois,
Name of Affiant City of Affiant State of Affiant

being first duly sworn upon oath, state as follows:

1. That I am the President of K-Five Construction Corporation.
Officer or Position Bidder
2. That I have personal knowledge of the facts herein stated.
3. That, if selected under the proposal described above, K-Five Construction Corporation, will maintain a business office in the State of Illinois, which will be located in DuPage County, Illinois.
Bidder
4. That this business office will serve as the primary place of employment for any persons employed in the construction contemplated by this proposal.
5. That this Affidavit is given as a requirement of state law as provided in Section 30-22(8) of the Illinois Procurement Code.



Signature	Date
	2/16/2022
Print Name of Affiant	
Robert G. Krug	

Notary Public

State of IL
 County DuPage

Signed (or subscribed or attested) before me on 2/16/2022 by
(date)

Robert G. Krug, authorized agent(s) of
(name/s of person/s)

K-Five Construction Corporation
Bidder



Signature of Notary Public

My commission expires 7/29/24



999 Oakmont Plaza Drive • Suite 200 • Westmont, IL 60559
(630) 257-5600 Fax (630) 257-6788
www.k-five.net

January 18, 2022

RE: Affidavit of Availability

To Whom It May Concern:

Per the attached "Rules for Prequalification of Contractors and Issuance of Plans and Proposals" as published by the Illinois Department of Transportation ('IDOT'), Section 650.310 Item (d) K-Five Construction Corporation hereby requests to forgo the filing of an "Affidavit of Availability" based upon the "Super Unlimited" financial rating (d1) and "Unlimited" HMA Plant Mix rating (d2) as shown on the attached "Certificate of Eligibility."

This method of substitution for the "Affidavit of Availability" has been approved by IDOT's Prequalification Engineer and has been accepted by IDOT for contract Lettings. The Unit Chief of Prequalification Services in IDOT's (Springfield) Bureau of Construction is Frank Lowry. If there are any questions, he can be reached at (217) 782-3413.

Sincerely,
K-Five Construction Corporation

A handwritten signature in black ink, appearing to read 'Nick Eller', is written over the typed name and title.

Nicholas J. Eller
Senior Estimator



Illinois Department of Transportation

**RULES FOR
PREQUALIFICATION OF CONTRACTORS,
AUTHORIZATION TO BID,
AND
SUBCONTRACTOR REGISTRATION**

44 IL. ADM.CODE SEC. 650

ADOPTED JULY 2, 1994

AMENDED May 8, 2008

- c) All remaining work categories are calculated by the secondary formula.

WR = PF X CP X 1.2 (Secondary Formula)

- d) The secondary formula does not utilize an equipment factor because of the immeasurable productive capacity of the equipment or plant facility; however, equipment must be owned by or leased to the applicant. See Section 650. Appendix A of this Part for a listing of equipment or plant facilities. The secondary formula includes a factor of 1.2 to provide a margin for growth.
- e) An applicant's capacity to perform may exceed the calculated equipment factor. This can occur by good management, efficiency and additional hours of work. When this occurs, the primary and advanced formulas will be replaced by the secondary formula.
- f) The work rating in any given category may not exceed the financial rating of the applicant.
- g) A work rating may be designated as "Illinois Work Only." This work rating indicates the dollar value of work which the applicant's own forces can perform within the State of Illinois in one construction season. This rating will be established by the Department if the applicant does work in more than one state or outside the continental United States and it would be impractical to verify all outstanding work.
- h) Prior to any consideration for establishing a work rating value, the applicant shall provide a list of all technical, supervisory and key personnel who would manage a project awarded by the Department. This list should include the individual's job title and number of years of construction experience. The Department may also require the submittal of resumes of the above individuals. Applicants prequalifying with the Department for the first time shall be required to submit resumes. Insufficient personnel may be justification for a reduction in the rating of a work category as determined by the primary, advanced or secondary formula. Hiring of additional personnel may be justification for an increase in the rating of a work category. Applicants without experienced personnel for a requested work category may be denied the rating.
- i) Methods to Improve a Work Rating
- 1) Hiring of additional personnel.
 - 2) Purchase, lease or rental of additional equipment.
 - 3) Completion of additional work.
- j) A contractor may request additional rating in a work category at any time during the prequalification period by submitting a revised application or supplemental information.

(Source: Amended at 24 Ill. Reg. 18775, effective December 7, 2000)

SUBPART B: AUTHORIZATION TO BID

Section 650.290 Advertising for Bids

The procedures for procuring contracts are set out in the Department's rules for contract procurement found at 44 Ill. Adm. Code 660. The procedures of this Subpart B govern the granting of authority to bid on contracts advertised for bids in the Transportation Bulletin in accordance with the Department's rules for contract procurement.

(Source: Amended at 24 Ill. Reg. 18775, effective December 7, 2000)

Section 650.300 Request for Authorization to Bid or Not for Bid Status

A Request for Authorization to Bid or Not for Bid Status (Form BD-124 INT) is published with the Transportation Bulletin. The Form BD-124 INT shall be used by contractors to request proposals and plans

on CD-ROM and to request formal authorization to bid on contracts advertised in the Transportation Bulletin. Anyone may obtain proposal forms and plans regardless of prequalification status. An Authorization to Bid must be granted in accordance with this Part before a prequalified contractor may submit a bid.

(Source: Amended at 30 Ill. Reg. 16373, effective October 10, 2006)

Section 650.310 Affidavit of Availability

- a) An Affidavit of Availability (Form BC-57) is attached to the Transportation Bulletin and must be submitted with a request for Authorizations to Bid. It is a sworn statement concerning the contractor's present and pending contract commitments. The contractor shall not omit or misrepresent its work outstanding. When the contractor has uncompleted or pending work as a party of a joint venture, the contractor's responsible portion of the work shall be shown. The affidavit shall be signed by an officer or director of a corporate contractor, and otherwise, an owner shall sign. The affidavit is not required when Authorization to Bid is not being requested. The affidavit shall include:
- 1) The amount of all uncompleted work, by type, either as a principal or subcontractor together with the name of the agency under whose jurisdiction the work is being performed. All uncompleted work shall be based upon the engineer's or owner's most recent estimate.
 - 2) The commitment of equipment and personnel on a payroll or rental basis even though no formal contract exists.
 - 3) All work on which the contractor is the low bidder and which has not yet been awarded.
 - 4) A listing of all subcontractors and the value of work sublet.
- b) Prospective bidders shall notify the Department within two working days of any low bids pending award or contracts awarded after submission of the affidavit.
- c) Facsimiles of the affidavit will be accepted for analysis purposes. Authorization to Bid will not be issued without a correct, signed and notarized original affidavit in the Department's Central Bureau of Construction's possession by the cut-off date specified in the Transportation Bulletin

- d) A contractor may request to forego filing an affidavit if it has a financial rating at either of the following levels. The Prequalification Section will grant such a request provided the contractor's existing contracts with the Department are not behind approved contract progress schedules and provided the most recent performance evaluation rating is not less than 6.0 in the performance factor calculation. (See Section 650.240 of this Part.)

- 1) A financial rating of \$300 million.
- 2) A financial rating of at least \$150 million or a Department calculated net worth of at least \$40 million, either in conjunction with two or more work ratings calculated to equal or exceed \$50 million each.

(Source: Amended at 24 Ill. Reg. 18775, effective December 7, 2000)

Section 650.315 Disclosure of Other Procurement Relationships

- a) Section 50-35(h) of the Illinois Procurement Code [30 ILCS 500/50-35(h)] requires that all bids of more than \$10,000 be accompanied by disclosure of all current or pending contracts, proposals, leases, or other ongoing procurement relationships the contractor has with any other unit of State government. This disclosure is required in addition to the financial interest disclosure provided at Section 650.80(d) of this Part.



**Illinois Department
of Transportation**

Certificate of Eligibility

Contractor No 3069
(1st Revision)

K-Five Construction Corporation
999 Oakmont Plaza Drive Suite 200 WESTMONT, IL 60559

WHO HAS FILED WITH THE DEPARTMENT AN APPLICATION FOR PREQUALIFICATION STATEMENT OF EXPERIENCE, EQUIPMENT AND FINANCIAL CONDITION IS HEREBY QUALIFIED TO BID AT ANY OF DEPARTMENT OF TRANSPORTATION LETTINGS IN THE CLASSES OF WORK AND WITHIN THE AMOUNT AND OTHER LIMITATIONS OF EACH CLASSIFICATION, AS LISTED BELOW, FOR SUCH PERIOD AS THE UNCOMPLETED WORK FROM ALL SOURCES DOES NOT EXCEED SUPER UNLIMITED

001	EARTHWORK	\$6,275,000
002	PCC PAVING	\$25,475,000
003	HMA PLANT MIX	Unlimited
012	DRAINAGE	\$1,450,000
017	CONCRETE CONSTRUCTION	\$30,475,000
031	PAV'T. TEXTUR. & SURF. REM.	\$2,125,000
032	COLD MILL, PLAN. & ROTOMILL	\$8,775,000
08A	AGGREGATE BASES & SURF. (A)	\$4,925,000
09A	HIGHWAY STRUCTURES	\$1,850,000

THIS CERTIFICATE OF ELIGIBILITY IS VALID FROM 7/9/2021 TO 6/30/2022 INCLUSIVE, AND SUPERSEDES ANY CERTIFICATE PREVIOUSLY ISSUED, BUT IS SUBJECT TO REVISION OR REVOCATION, IF AND WHEN CHANGES IN THE FINANCIAL CONDITION OF THE CONTRACTING FIRM OR OTHER FACTS JUSTIFY SUCH REVISIONS OR REVOCATION. ISSUED AT SPRINGFIELD, ILLINOIS ON 7/9/2021.

Tim Kell
Engineer of Construction



Brown & Brown of Illinois, Inc.
263 Shuman Blvd. Suite 110
Naperville, IL 60563
P: (630) 245-4600 | F: (630) 245-4601
Rachel.Hernandez@bbrown.com

February 4, 2022

Village of Downers Grove
5105 Walnut Avenue
Downers Grove, IL 60515

RE: K-Five Construction Corporation

Project: Various Routes, Section Number 22-00117-00-RS

To Whom It May Concern:

Please be advised that we are the bonding agent for K-Five Construction Corporation. This contractor obtains their bonds through Continental Casualty company, 151 N. Franklin St., Chicago, IL 60606. As stated in the A.M. Best Rating Guide Continental Casualty company has a rating of A XV.

Continental Casualty company would be willing to entertain a performance and payment bond request on behalf of K-Five Construction Corporation. Both the surety and the contractor would issue the bond in 100% of the total contract price on the required forms.

This letter shall serve to verify that the information provided by K-Five Construction Corporation, in the pre-qualification form in regard to the surety and bonding information is correct. I trust that this information satisfies your inquiry. Should you require anything further, please do not hesitate to call.

Sincerely,

Brown & Brown of Illinois

A handwritten signature in cursive script that reads 'Rachel Hernandez'.

Rachel Hernandez
Bond Administrator



Local Agency Proposal Bid Bond

Route Various

County DuPage

Local Agency Village of Downers Grove

Section 22-00117-00-RS

RETURN WITH BID

PAPER BID BOND

WE K-Five Construction Corporation 999 Oakmont Plaza Drive, Suite 200, Westmont, IL 60559 as PRINCIPAL,
and Continental Casualty Company 151 N. Franklin Street, Chicago, IL 60606 as SURETY,

are held jointly, severally and firmly bound unto the above Local Agency (hereafter referred to as "LA") in the penal sum of 5% of the total bid price, or for the amount specified in the proposal documents in effect on the date of invitation for bids whichever is the lesser sum. We bind ourselves, our heirs, executors, administrators, successors, and assigns, jointly pay to the LA this sum under the conditions of this instrument.

WHEREAS THE CONDITION OF THE FOREGOING OBLIGATION IS SUCH that, the said PRINCIPAL is submitting a written proposal to the LA acting through its awarding authority for the construction of the work designated as the above section.

THEREFORE if the proposal is accepted and a contract awarded to the PRINCIPAL by the LA for the above designated section and the PRINCIPAL shall within fifteen (15) days after award enter into a formal contract, furnish surety guaranteeing the faithful performance of the work, and furnish evidence of the required insurance coverage, all as provided in the "Standard Specifications for Road and Bridge Construction" and applicable Supplemental Specifications, then this obligation shall become void; otherwise it shall remain in full force and effect.

IN THE EVENT the LA determines the PRINCIPAL has failed to enter into a formal contract in compliance with any requirements set forth in the preceding paragraph, then the LA acting through its awarding authority shall immediately be entitled to recover the full penal sum set out above, together with all court costs, all attorney fees, and any other expense of recovery.

IN TESTIMONY WHEREOF, the said PRINCIPAL and the said SURETY have caused this instrument to be signed by their respective officers this 16th day of February, 2022

K-Five Construction Corporation (Company Name) _____ (Company Name)

By: Robert G. Krug President (Signature and Title) _____ (Signature and Title)

(If PRINCIPLE is a joint venture of two or more contractors, the company names and authorized signatures of each contractor must be affixed.)

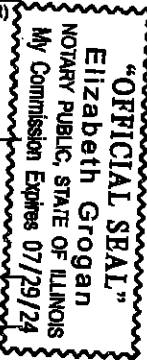
Continental Casualty Company (Name of Surety) _____
By: Rachel E. Hernandez (Signature of Attorney-in-Fact)

STATE OF IL
COUNTY OF DuPage
I, Elizabeth Grogan, a Notary Public in and for said county,
do hereby certify that Robert G. Krug

(Insert names of individuals signing on behalf of PRINCIPAL) _____
who are each personally known to me to be the same persons whose names are subscribed to the foregoing instrument on behalf of PRINCIPAL _____, appeared before me this day in person and acknowledged respectively, that they signed and delivered said instruments as their free and voluntary act for the uses and purposes therein set forth.

Given under my hand and notarial seal this 16th day of February, 2022

My commission expires 7/29/24 _____
Elizabeth Grogan (Notary Public)



ELECTRONIC BID

Electronic bid bond is allowed (box must be checked by LA if electronic bid bond is allowed)
The Principal may submit an electronic bid bond, in lieu of completing the above section of the Proposal Bid Bond Form. By providing an electronic bid bond ID code and signing below, the Principal is ensuring the identified electronic bid bond has been executed and the Principal and Surety are firmly bound unto the LA under the conditions of the bid bond as shown above. (If PRINCIPAL is a joint venture of two or more contractors, an electronic bid bond ID code, company/Bidder name title and date must be affixed for each contractor in the venture.)

Electronic Bid Bond ID Code

(Company/Bidder Name)

(Signature and Title)

Date

ss

STATE OF ILLINOISCOUNTY OF DuPage

I, Kimberly R. Holmes Notary Public of DuPage County, in the State of Illinois do hereby certify that Rachel E. Hernandez Attorney-in-Fact, of the Continental Casualty Company who is personally known to me to be the same person whose name is subscribed to the foregoing instrument, appeared before me this day in person, and acknowledged that she signed, sealed and delivered said instrument, for and on behalf of the Continental Casualty Company for the used and purposes therein set forth.

Given under my hand and notarial seal at my office in the City of Naperville in said County, this 16th day of February A.D., 2022

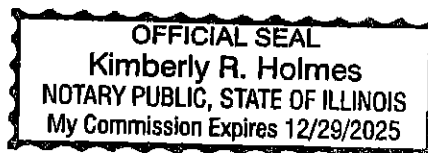


(Notary Public)

Kimberly R. Holmes

My Commission expires: 12/29/2025

Notary Seal:



POWER OF ATTORNEY APPOINTING INDIVIDUAL ATTORNEY-IN-FACT

Know All Men By These Presents, That Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company (herein called "the CNA Companies"), are duly organized and existing insurance companies having their principal offices in the City of Chicago, and State of Illinois, and that they do by virtue of the signatures and seals herein affixed hereby make, constitute and appoint

Karen A Ryan, William Cahill, Kimberly Sawicki, Ann Marie Waters, Brent R Wagner, Kimberly R Holmes, Rachel E Hernandez, Leigh Ann Francis, Richard A Freebourn Jr, Individually

of Naperville, IL, their true and lawful Attorney(s)-in-Fact with full power and authority hereby conferred to sign, seal and execute for and on their behalf bonds, undertakings and other obligatory instruments of similar nature

- In Unlimited Amounts -

and to bind them thereby as fully and to the same extent as if such instruments were signed by a duly authorized officer of their insurance companies and all the acts of said Attorney, pursuant to the authority hereby given is hereby ratified and confirmed.

This Power of Attorney is made and executed pursuant to and by authority of the By-Law and Resolutions, printed on the reverse hereof, duly adopted, as indicated, by the Boards of Directors of the insurance companies.

In Witness Whereof, the CNA Companies have caused these presents to be signed by their Vice President and their corporate seals to be hereto affixed on this 21st day of June, 2021.



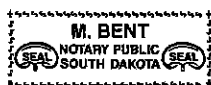
Continental Casualty Company
National Fire Insurance Company of Hartford
American Casualty Company of Reading, Pennsylvania

Paul T. Bruflat

Paul T. Bruflat Vice President

State of South Dakota, County of Minnehaha, ss:

On this 21st day of June, 2021, before me personally came Paul T. Bruflat to me known, who, being by me duly sworn, did depose and say: that he resides in the City of Sioux Falls, State of South Dakota; that he is a Vice President of Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company described in and which executed the above instrument; that he knows the seals of said insurance companies; that the seals affixed to the said instrument are such corporate seals; that they were so affixed pursuant to authority given by the Boards of Directors of said insurance companies and that he signed his name thereto pursuant to like authority, and acknowledges same to be the act and deed of said insurance companies.



My Commission Expires March 2, 2026

M. Bent

M. Bent Notary Public

CERTIFICATE

I, D. Johnson, Assistant Secretary of Continental Casualty Company, an Illinois insurance company, National Fire Insurance Company of Hartford, an Illinois insurance company, and American Casualty Company of Reading, Pennsylvania, a Pennsylvania insurance company do hereby certify that the Power of Attorney herein above set forth is still in force, and further certify that the By-Law and Resolution of the Board of Directors of the insurance companies printed on the reverse hereof is still in force. In testimony whereof I have hereunto subscribed my name and affixed the seal of the said insurance companies this 16th day of February, 2022



Continental Casualty Company
National Fire Insurance Company of Hartford
American Casualty Company of Reading, Pennsylvania

D. Johnson

D. Johnson Assistant Secretary

Form F6853-4/2012

Go to www.cnasurety.com > Owner / Obligee Services > Validate Bond Coverage, if you want to verify bond authenticity.

Authorizing By-Laws and Resolutions

ADOPTED BY THE BOARD OF DIRECTORS OF CONTINENTAL CASUALTY COMPANY:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company at a meeting held on May 12, 1995:

"RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective."

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of Continental Casualty Company.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

"Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the "Authorized Officers") to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, "Electronic Signatures"); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. "

ADOPTED BY THE BOARD OF DIRECTORS OF NATIONAL FIRE INSURANCE COMPANY OF HARTFORD:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company by unanimous written consent dated May 10, 1995:

"RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective."

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of National Fire Insurance Company of Hartford.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

"Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the "Authorized Officers") to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, "Electronic Signatures"); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. "

ADOPTED BY THE BOARD OF DIRECTORS OF AMERICAN CASUALTY COMPANY OF READING, PENNSYLVANIA:

This Power of Attorney is made and executed pursuant to and by authority of the following resolution duly adopted by the Board of Directors of the Company by unanimous written consent dated May 10, 1995:

"RESOLVED: That any Senior or Group Vice President may authorize an officer to sign specific documents, agreements and instruments on behalf of the Company provided that the name of such authorized officer and a description of the documents, agreements or instruments that such officer may sign will be provided in writing by the Senior or Group Vice President to the Secretary of the Company prior to such execution becoming effective."

This Power of Attorney is signed by Paul T. Bruflat, Vice President, who has been authorized pursuant to the above resolution to execute power of attorneys on behalf of American Casualty Company of Reading, Pennsylvania.

This Power of Attorney is signed and sealed by facsimile under and by the authority of the following Resolution adopted by the Board of Directors of the Company by unanimous written consent dated the 25th day of April, 2012:

"Whereas, the bylaws of the Company or specific resolution of the Board of Directors has authorized various officers (the "Authorized Officers") to execute various policies, bonds, undertakings and other obligatory instruments of like nature; and

Whereas, from time to time, the signature of the Authorized Officers, in addition to being provided in original, hard copy format, may be provided via facsimile or otherwise in an electronic format (collectively, "Electronic Signatures"); Now therefore be it resolved: that the Electronic Signature of any Authorized Officer shall be valid and binding on the Company. "

VILLAGE OF DOWNERS GROVE
DEPARTMENT OF PUBLIC WORKS

ADDENDUM NO. 1

FOR

2022 Road Resurfacing Program

BID #CFB-70-0-2022/SG

February 4, 2022

ITEM AND DESCRIPTION:

1. **ADD:**
The following Special Provision should be included with the bid submittal:
 - IDOT LR1030-2, "LOCAL QUALITY ASSURANCE/ QUALITY MANAGEMENT QC/QA" (Effective: January 1, 2022) with the **Nuclear Density Gauge** verification method checked

LR1030-2

State of Illinois
 DEPARTMENT OF TRANSPORTATION
 Bureau of Local Roads & Streets
 SPECIAL PROVISION
 FOR
 LOCAL QUALITY ASSURANCE/ QUALITY MANAGEMENT QC/QA
 Effective: January 1, 2022

Replace the first five paragraphs of Article 1030.06 of the Standard Specifications with the following:

"1030.06 Quality Management Program. The Quality Management Program (QMP) will be Quality Control / Quality Assurance (QC/QA) according to the following."

Delete Article 1030.06(d)(1) of the Standard Specifications.

Revise Article 1030.09(g)(3) of the Standard Specifications to read:

"(3) If core testing is the density verification method, the Contractor shall provide personnel and equipment to collect density verification cores for the Engineer. Core locations will be determined by the Engineer following the document "Hot-Mix Asphalt QC/QA Procedure for Determining Random Density Locations" at density verification intervals defined in Article 1030.09(b). After the Engineer identifies a density verification location and prior to opening to traffic, the Contractor shall cut a 4 in. (100 mm)

Revise Article 1030.09(h)(2) of the Standard Specifications to read:

"(2) After final rolling and prior to paving subsequent lifts, the Engineer will identify the random density verification test locations. Cores or nuclear density gauge testing will be used for density verification. The method used for density verification will be as selected below.

Density Verification Method	
<input type="checkbox"/>	Cores
<input checked="" type="checkbox"/>	Nuclear Density Gauge (Correlated when paving ≥ 3,000 tons per mixture)

Density verification test locations will be determined according to the document "Hot-Mix Asphalt QC/QA Procedure for Determining Random Density Locations". The density testing interval for paving wider than or equal to 3 ft (1 m) will be 0.5 miles (800 m) for lift thicknesses of 3 in. (75 mm) or less and 0.2 miles (320 m) for lift thicknesses greater than 3 in. (75 mm). The density testing interval for paving less than 3 ft (1 m) wide will be 1 mile (1,600 m). If a day's paving will be less than the prescribed density testing interval, the length of the day's paving will be the interval for that day. The density testing interval for mixtures used for patching will be 50 patches with a minimum of one test per mixture per project.

If core testing is the density verification method, the Engineer will witness the Contractor coring, and secure and take possession of all density samples at the

LR1030-2

density verification locations. The Engineer will test the cores collected by the Contractor for density according to Illinois Modified AASHTO T 166 or AASHTO T 275.

If nuclear density gauge testing is the density verification method, the Engineer will conduct nuclear density gauge tests. The Engineer will follow the density testing procedure detailed in the document "Illinois Modified ASTM D 2950, Standard Test Method for Density of Bituminous Concrete In-Place by Nuclear Method".

A density verification test will be the result of a single core or the average of the nuclear density tests at one location. The results of each density test must be within acceptable limits. The Engineer will promptly notify the Contractor of observed deficiencies."

Revise the seventh paragraph and all subsequent paragraphs in Section D. of the document "Hot-Mix Asphalt QC/QA Initial Daily Plant and Random Samples" to read:

"Mixtures shall be sampled from the truck at the plant by the Contractor following the same procedure used to collect QC mixture samples (Section A). This process will be witnessed by the Engineer who will take custody of the verification sample. Each sample bag with a verification mixture sample will be secured by the Engineer using a locking ID tag. Sample boxes containing the verification mixture sample will be sealed/taped by the Engineer using a security ID label."

The Acknowledgement of Receipt of Addendum for this addendum MUST be included in the bid package. Bid packages not including signed Acknowledgement Sheets may be REJECTED.

End of Addendum No. 1
February 4, 2022

**VILLAGE OF DOWNERS GROVE
DEPARTMENT OF PUBLIC WORKS**

ACKNOWLEDGEMENT OF RECEIPT OF ADDENDUM

PROPOSAL/BID: 2022 Road Resurfacing Program

PROPOSAL/BID NUMBER: BID # CFB-70-0-2022/SG

PROPOSAL/BID OPENING: February 16, 2022

ADDENDUM NO.: 1

PROPOSER/BIDDER: K-Five Construction Corp.

ADDRESS: 999 Oakmont Plaza Dr., Westmont, IL

RECEIVED BY: Robert G. Krug
(NAME)

[Handwritten Signature]
(SIGNATURE)

DATE: 2/4/22



**VILLAGE OF DOWNERS GROVE
DEPARTMENT OF PUBLIC WORKS**

ADDENDUM NO. 2

FOR

2022 Road Resurfacing Program

BID #CFB-70-0-2022/SG

February 8, 2022

ITEM AND DESCRIPTION:

1. REPLACE

The quantity of Pay Item #14: Combination Concrete Curb and Gutter Type M-3.12 should be 8,882 feet. The quantity of Pay Item #40: Tree Root Pruning should be 13 each. Schedule of Prices (form BLR 12201) should be replaced with the attached BLR 12201 (Pages 1 through 3A, Printed 02/08/22).

The Acknowledgement of Receipt of Addendum for this addendum **MUST** be included in the bid package. Bid packages not including signed Acknowledgement Sheets may be **REJECTED**.

**End of Addendum No. 2
February 8, 2022**

**VILLAGE OF DOWNERS GROVE
DEPARTMENT OF PUBLIC WORKS**

ACKNOWLEDGEMENT OF RECEIPT OF ADDENDUM

PROPOSAL/BID: 2022 Road Resurfacing Program

PROPOSAL/BID NUMBER: BID # CFB-70-0-2022/SG

PROPOSAL/BID OPENING: February 16, 2022

ADDENDUM NO.: 2

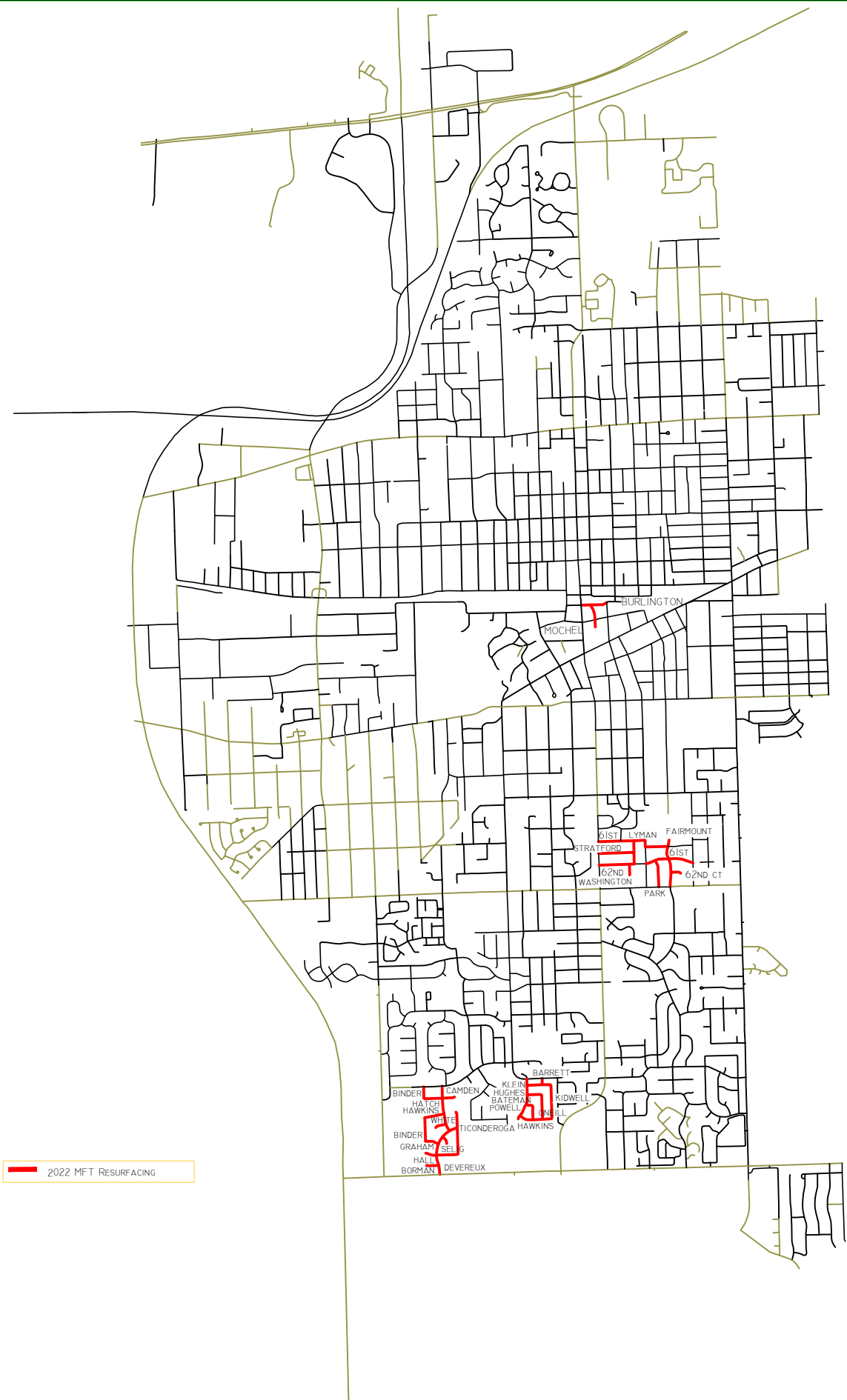
PROPOSER/BIDDER: K-Five Construction Corp.

ADDRESS: 999 Oakmont Plaza Dr., Westmont, IL 60559

RECEIVED BY: Nicholas J Elmer
(NAME)

[Handwritten Signature]
(SIGNATURE)

DATE: 2/8/22



2022 MFT MAP- 5.04 MILES

2022 ST-004B RESURFACING PROJECT

SCALE
NTS 
NORTH

DATE: XX/XX/XX

DRWN BY: AAA

CHKD BY: AAA

DRAWING NO.

EXHIBIT 1

2022 RESURFACING PROJECT (B)
LIST OF STREETS TO BE REPAVED (02/23/22)

STREET	FROM	TO
HUGHES AVE	ONEILL RD	POWELL ST
BARRETT ST	KLEIN AVE	71ST ST
KLEIN AVE	POWELL ST	KIDWELL RD
ONEILL RD	HUGHES AVE	HAWKINS AVE
BATEMAN ST	POWELL ST	HAWKINS AVE
HAWKINS AVE	KIDWELL RD	POWELL ST
KIDWELL RD	KLEIN AVE	HAWKINS AVE
POWELL ST	71ST ST	SW END

HATCH ST	CAMDEN RD	BINDER RD
HATCH PL	EAST END	CAMDEN RD
BINDER RD	71ST ST	HATCH ST
BINDER RD	HAWKINS AVE	GRAHAM AVE
HAWKINS AVE	CAMDEN RD	BINDER RD
GRAHAM AVE	DEVEREUX RD	BINDER RD
TICONDEROGA RD	DEVEREUX RD	HALL ST
TICONDEROGA PL	NORTH END	DEVEREUX RD
HALL PL	TICONDEROGA RD	WEST END
SELIG PL	DEVEREUX RD	SE END
DEVEREUX RD	CAMDEN RD	75TH ST
CAMDEN RD	71ST ST	DEVEREUX RD
BORMAN PL	DEVEREUX RD	WEST END
WHITE PL	CAMDEN RD	WEST END

61ST ST	FAIRMOUNT AVE	MAIN ST
62ND ST	BLODGETT AVE	LYMAN AVE
62ND ST	WASHINGTON ST	MAIN ST
STRATFORD LN	WASHINGTON ST	MAIN ST
WASHINGTON ST	61ST ST	SOUTH END
LYMAN AVE	61ST ST	61ST ST
FAIRMOUNT AVE	63RD ST	N. END
62ND CT	FAIRMOUNT AVE	CUL DE SAC
PARK AVE	63RD ST	62ND ST

BURLINGTON AVE	MAIN ST	WASHINGTON ST
MOCHEL DR	BURLINGTON AVE	S. END



**Resolution for Improvement
Under the Illinois Highway Code**

RBI



Is this project a bondable capital improvement?
 Yes No

Resolution Type	Resolution Number	Section Number
Original	2022-11	22-00117-00-RS

BE IT RESOLVED, by the Council of the Village
 of Downers Grove Illinois that the following described street(s)/road(s)/structure be improved under
 the Illinois Highway Code. Work shall be done by Contract

For Roadway/Street Improvements:

Name of Street(s)/Road(s)	Length (miles)	Route	From	To
Neighborhood Resurfacing	4.99	Various	NA	NA

For Structures:

Name of Street(s)/Road(s)	Existing Structure No.	Route	Location	Feature Crossed
(see attached street list)				

BE IT FURTHER RESOLVED,

1. That the proposed improvement shall consist of

Pavement grinding and resurfacing with HMA level binder course and HMA surface course, curb and sidewalk removal and replacement, restoration. \$1,590,000 is allocated to Rebuild Illinois.

2. That there is hereby appropriated the sum of one million, five hundred and ninety thousand dollars

_____ Dollars (\$1,590,000.00) for the improvement of said section from the Local Public Agency's allotment of Motor Fuel Tax funds.

BE IT FURTHER RESOLVED, that the Clerk is hereby directed to transmit four (4) certified originals of this resolution to the district office of the Department of Transportation.

I, Rosa Berardi Village Clerk in and for said Village
Name of Clerk Local Public Agency Type Local Public Agency Type

of Downers Grove in the State aforesaid, and keeper of the records and files thereof, as provided by statute, do hereby certify the foregoing to be a true, perfect and complete original of a resolution adopted by

Council of Downers Grove at a meeting held on February 01, 2022
Governing Body Type Name of Local Public Agency Date

IN TESTIMONY WHEREOF, I have hereunto set my hand and seal this 1st day of February, 2022.
Day Month Year

(SEAL)

Clerk Signature Rosa Berardi Date 2-1-2022

Approved

Regional Engineer
 Department of Transportation
 _____ Date _____



Resolution for Improvement Under the Illinois Highway Code



Is this project a bondable capital improvement?

Yes No

Table with Resolution Type (Original), Resolution Number (2022-12), and Section Number (22-00117-00-RS)

BE IT RESOLVED, by the Council of the Village of Downers Grove

Name of Local Public Agency: Downers Grove, Illinois that the following described street(s)/road(s)/structure be improved under the Illinois Highway Code. Work shall be done by Contract

For Roadway/Street Improvements:

Table with columns: Name of Street(s)/Road(s), Length (miles), Route, From, To. Row: Neighborhood Resurfacing, 4.99, Various, NA, NA

For Structures:

Table with columns: Name of Street(s)/Road(s), Existing Structure No., Route, Location, Feature Crossed. Row: (see attached street list)

BE IT FURTHER RESOLVED,

1. That the proposed improvement shall consist of

Pavement grinding and resurfacing with HMA level binder course and HMA surface course, curb and sidewalk removal and replacement, restoration. \$1,060,000 is allocated to MFT funding.

2. That there is hereby appropriated the sum of one million, sixty thousand dollars

Dollars (\$1,060,000.00) for the improvement of said section from the Local Public Agency's allotment of Motor Fuel Tax funds.

BE IT FURTHER RESOLVED, that the Clerk is hereby directed to transmit four (4) certified originals of this resolution to the district office of the Department of Transportation.

I, Rosa Berardi, Village Clerk in and for said Village of Downers Grove

statute, do hereby certify the foregoing to be a true, perfect and complete original of a resolution adopted by

Council of Downers Grove at a meeting held on February 01, 2022

IN TESTIMONY WHEREOF, I have hereunto set my hand and seal this 1st day of February, 2022

(SEAL)



Clerk Signature: Rosa Berardi, Date: 2/1/2022

Approved

Regional Engineer, Department of Transportation, Date



Village of Downers Grove

Contractor Evaluation

Contractor: K-Five Construction Corporation

Project: ST-036-17 Finley Rd Resurfacing LAFO- Butterfield to Village Limits; ST-038-17 Dunham Rd Resurfacing LAFO- 63rd to 55th

Primary Contact: Ryan Scott Phone: (630) 768-8457

Time Period: May 2017 – December 2017

On Schedule (allowing for uncontrollable circumstances) Yes No

Provide details if early or late completion: _____

Change Orders (attach information if needed): Finley: 2 balancing authorizations, 1 authorization to switch sodding to seed & blanket due to temperature specs. Dunham: 2 balancing authorizations, one authorization to add removal of agg shoulders pay item.

Difficulties / Positives: Quick responses in communication, kept Village updated. Very cooperative to work with. Very smooth projects.

Interaction with public:

Excellent Good Average Poor

(Attach information on any complaints or compliments)

General Level of Satisfaction with work:

Well Satisfied Satisfied Not Satisfied

Reviewers: Stephanie Graves, PE

Date: 03/30/18